

# The North Shore Neighbourhood Plan



embracing community diversity and sustainability

# The North Shore Neighbourhood Plan

June 2008

**The City of Kamloops  
Development and Engineering Services Department  
Community Planning Division**

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# Executive Summary

# Executive Summary

## EXECUTIVE SUMMARY

The North Shore Neighbourhood Plan was initiated by the Development and Engineering Services Department in February 2005 to provide a comprehensive land use plan to direct and guide the growth and evolution of the North Shore over the next five to ten years. Previous to this Plan, several smaller revitalization and land use plans had been prepared but nothing on a scale that encompassed the whole of the North Shore, nor that collectively addressed issues related to density, design, transportation, land uses and future development scenarios. Through the processing and preparation of the North Shore Neighbourhood Plan, several major goals surfaced as key elements to focus on and direct policy to, including:

- increasing density within key areas of the North Shore, specifically along the Tranquille Commercial District and within the North Shore Towne Centre;
- raising the profile of the North Shore and supporting redevelopment and development opportunities;
- setting a new benchmark for development within the City of Kamloops that focuses on sustainability, urban design and environmental responsibility; and
- identifying new ways to support and encourage complex development by creating new incentive initiatives and by establishing performance-based development guidelines and zoning regulations.

The North Shore Neighbourhood Plan is broken up into four main sections: General Policies, Land Use Policies, Implementation and supporting Appendices.

The General Policies Section was established to apply to the whole of the North Shore and focus on providing policy direction related to Growth Management, Community Improvements, Form and Identity, Heritage Conservation, Public Safety, Transportation, Utilities, Recreation and Environmentally Responsible Development.

The Land Use Policies Section is divided up into seventeen distinct areas and focuses policy more specific to the issues, capacities and future direction for each area.

The Implementation Section focuses on how the Plan can be executed and sets out the incentives that were established to help development take place on the North Shore. Implementation of policies within this Plan is intended to span approximately five to ten years. For some of these policies, additional studies, analysis, planning processes and approvals from City Council will be needed prior to determining their feasibility and appropriateness for the community and for the operations of the City.

The supporting Appendices, of which there are six, are intended to provide additional information and assistance to the Plan through the use of Development Permit Guidelines, a Development Checklist, a list of all Capital Projects and Action Item Priorities, a new Zoning By-law that directly supports and implements the policies in the Plan, an example of how to use Crime Prevention Through Environmental Design (CPTED) in development, and a Procedures Guide to help direct the community through the development process.

The North Shore Neighbourhood Plan strives to provide innovative solutions to the types of issues that any urban environment grapples with. This Plan is intended to support the unique environment that makes up all of the neighbourhoods within the North Shore and was prepared with a high level of community input and support. The North Shore Neighbourhood Plan represents a new benchmark for high quality, sustainable and community-appropriate development, not only for the North Shore but for the entire city of Kamloops and is supported through the use of innovative features such as a Development Checklist, a Development Incentives Matrix and performance-based zoning. These three implementation techniques place Kamloops in a leading role in supporting the development community to achieve environmentally sustainable and community-oriented development.

## Acknowledgements

# Acknowledgements

The creation of The North Shore Neighbourhood Plan and its corresponding regulatory documents is the end result of an intensive planning process spanning over three years. The project area comprises a diverse and vibrant collection of neighbourhoods originally functioning as one independent town and village prior to amalgamation with Kamloops in 1967 and 1973.

The residents and businesses of the North Shore care deeply about their community. This was reflected in the high level of participation in all open houses, surveys and in the energy and enthusiasm expressed by a group of volunteers comprising the North Shore Advisory Committee.

The presentation of this Plan has taken into consideration many divergent viewpoints regarding how the North Shore should develop and evolve over time. These viewpoints were then combined with some of the most innovative, creative and community-oriented planning policies endorsed today, to create an end product that will aid in showcasing the North Shore as the most diverse, dynamic and environmentally responsible neighbourhood within Kamloops.

The policies and regulations developed as part of this planning process also directly reflect the Corporate Strategy of the City of Kamloops by providing good government, creating opportunities for development to be environmentally responsible, establishing high-quality and long-lasting infrastructure and developing the opportunity for the North Shore to showcase what a strong, diverse and sustainable community it is and can be.

As the first comprehensive land use and neighbourhood plan of its kind prepared for the North Shore, it is anticipated that this Plan will serve as an important first step and building block in enhancing the North Shore as a place to live, an environment within which to play and an area where businesses thrive and are supported by the community.

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# Maps, Figures and Tables

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# Corporate Strategy

# Corporate Strategy

In support of the Quest For Quality Initiative launched in 2002, the City of Kamloops has created a Corporate Strategic Plan to identify ways in which the best possible service can be provided to residents, businesses and visitors. A commitment has been made to foster the provision of municipal services based on collaboration, accountability and progressiveness to create a healthy and balanced community.

Principles identified as a means to achieving this goal on a corporate level include: trust, health, innovation, openness and pride. Concurrent with those principles, the City has also identified five key objectives it wishes to achieve in striving for quality in municipal government. These objectives are:

- Tournament Capital - to fulfill the challenge of being the Tournament Capital of Canada.
- Infrastructure - to invest in the long-term financial stability of the City's assets and to improve the overall infrastructure standards of the City.
- Economy - to diversify and strengthen our economy.
- Livability - to ensure Kamloops maintains a high quality of life while meeting the challenges of a growing community.
- Good Government - to be nationally recognized for "Excellence in Public Service".

The North Shore Neighbourhood Plan has strived to meet this Corporate Strategy by creating policy and supporting regulations that are not only embraced by the community, but that also directly correlate to the principles and objectives laid out in the 2006 - 2008 City of Kamloops Corporate Strategic Plan.

Table 1: Corporate Strategy

Corporate Strategic Plan	North Shore Neighbourhood Plan
Principles	
TRUST	<ul style="list-style-type: none"> <li>- including a Capital Projects Priorities list in the Plan to show commitment to the policies and the community.</li> <li>- including new zoning regulations to create opportunity for the private sector to benefit from the new policies created.</li> </ul>
HEALTH	<ul style="list-style-type: none"> <li>- including policies and design guidelines that promote a healthy lifestyle in the community including creating inviting and safe environments for pedestrians and cyclists.</li> </ul>
INNOVATION	<ul style="list-style-type: none"> <li>- including a new zoning section that supports innovative, creative and progressive regulations in government and in the development industry.</li> </ul>
OPENNESS	<ul style="list-style-type: none"> <li>- running a planning process that involves constant feedback from the community including open houses, opinion surveys and the use of a volunteer advisory committee.</li> </ul>
PRIDE	<ul style="list-style-type: none"> <li>- sharing the pride of the North Shore with residents and business owners by creating a progressive and innovative Neighbourhood Plan and by creating new supportive zoning regulations.</li> </ul>

(cont...)

# Corporate Strategy

Table 1: Corporate Strategy (cont.)

Corporate Strategic Plan	North Shore Neighbourhood Plan
<u>Objectives</u>	
TOURNAMENT CAPITAL	- providing opportunity in the Neighbourhood Plan to support the Tournament Capital program and creating economic diversity on the North Shore as a spinoff from that program.
INFRASTRUCTURE	- providing the opportunity to install infrastructure where there currently is none but in an environmentally friendly and cohesive manner that is cost-effective for both the City, development community, and property owner to install and maintain.
ECONOMY	- creating new zoning regulations that offer flexibility in uses and land development to promote economic diversity and stability.
LIVABILITY	- creating policy and regulations that help connect neighbourhoods, provide for pedestrian environments and celebrate the diverse identity of the North Shore.
GOOD GOVERNMENT	- creating flexible policies and new zoning regulations that allow for development appropriate to the community, that is environmentally sustainable and that is progressive and innovative in design.



# Corporate Strategy

## Background

# Background

## 1.0 BACKGROUND

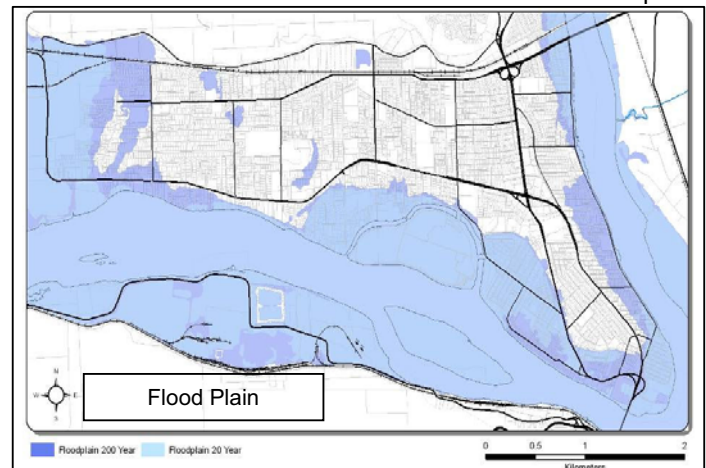
ND Lea Consultants Ltd. was retained in early 2006 to prepare a background report for the North Shore. The North Shore Plan Background Report identifies both opportunities and constraints for the area and was intended to create a context from which policy could be formed to guide and influence the future shape and form of development within this part of Kamloops. This background section is an excerpt directly from the North Shore Plan Background Report. Completed in early 2007, the document provides a snap-shot in time of various aspects of the North Shore, some of which have since changed (e.g. property values). Hard copies of the document can be obtained from the Development and Engineering Services Department or from the City's webpage at: [www.kamloops.ca/communityplanning](http://www.kamloops.ca/communityplanning).

### 1.0.1 Uses and Activities

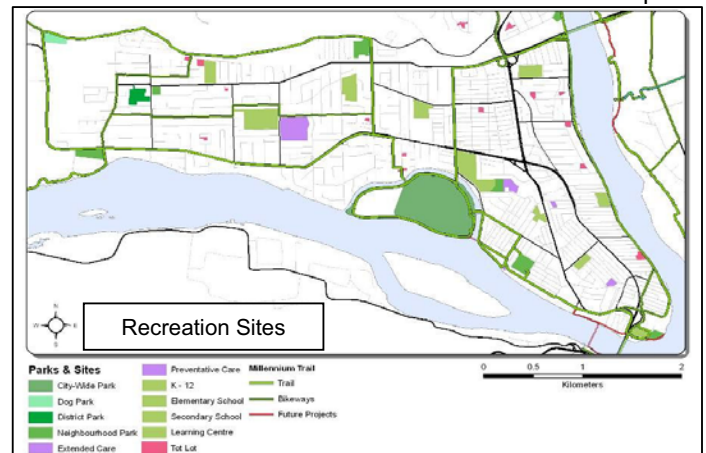
In order to gain an understanding of the types of activities that take place on the North Shore, an evaluation of land uses, activity generators, social spaces, and environmental constraints were examined. A solid review of all indicators revealed the following characteristics about the North Shore:

- significant portions of both North Kamloops and Brocklehurst are within a flood plain area;
- the majority of the developable land within the North Shore is not adjacent to an environmentally sensitive area;
- a majority of property values on the North Shore fall within an affordable range compared to other parts of the city;
- compared to North Kamloops, portions of Brocklehurst are not serviced by as much street lighting;
- the land use pattern is well established with a lack of any large undeveloped areas;
- stormwater structures are largely characterized by swale and gravel pit systems;
- many areas of both Brocklehurst and North Kamloops lack a continuous sidewalk system;
- the water system is currently operating at full capacity and well above city averages;
- current zoning densities permit a doubling in the population density for the North Shore; and
- overall, there is a good distribution of green space throughout the North Shore with particular emphasis on the McArthur Island facility and the Rivers Trail network.

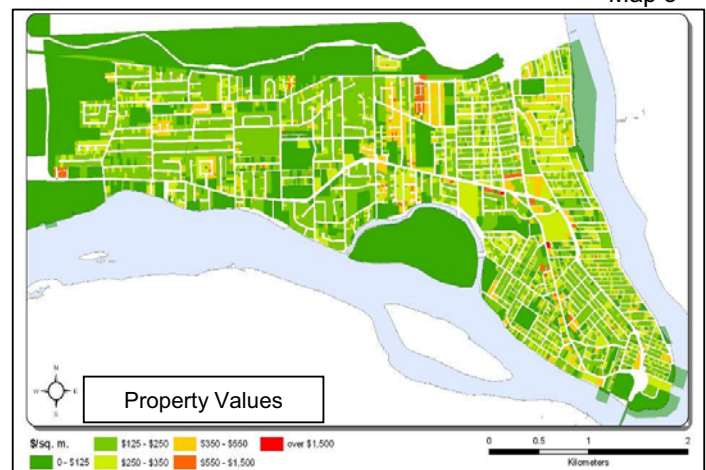
Map 1



Map 2



Map 3



Source: North Shore Background Report

# Background

## 1.0 BACKGROUND (cont.)

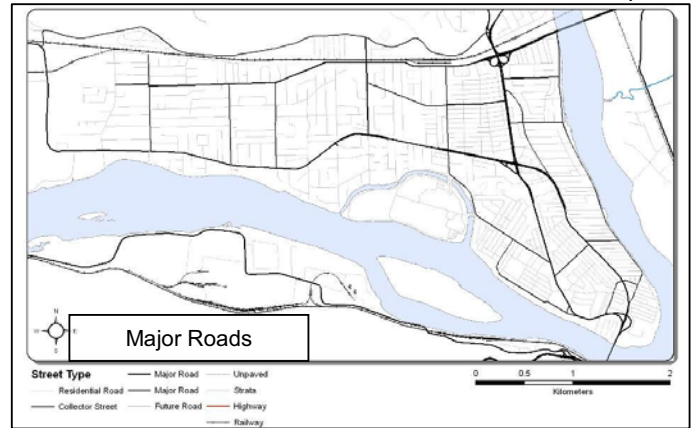
### 1.0.2 Access and Linkages

The North Shore comprises several residential neighbourhoods and commercial and industrial areas that have developed and evolved over the past 40 years (since amalgamation with the City of Kamloops). The road system is well established and largely unchanged and not only services cars, trucks and transit vehicles but also pedestrian and cycle users as well. The North Shore is also serviced by two bridges: the Overlanders Bridge and the Halston connector.

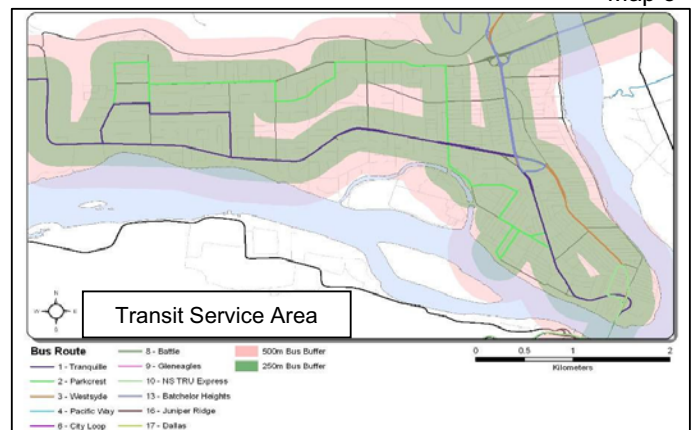
The North Shore Background Report provides an analysis on the existing access and linkages situation and provides a review of critical points that may be affected as the North Shore continues to grow and evolve:

- there is a strong orientation toward heavy use of the Tranquille Road and Fortune Drive arterials but there is limited opportunity to travel without using these two road systems;
- the North Shore road network has the least capacity compared to any other in the city to handle significant increases in traffic;
- transit coverage exists for all neighbourhoods of the North Shore but service with respect to times and days of service could use improvements at certain times of the day and week; and
- existing pedestrian infrastructure (e.g. sidewalks) is dated and is not consistent throughout the whole of the North Shore providing accessibility issues for those persons with disabilities and also for pedestrians wishing to make linkages across Fortune Drive and Tranquille Road.

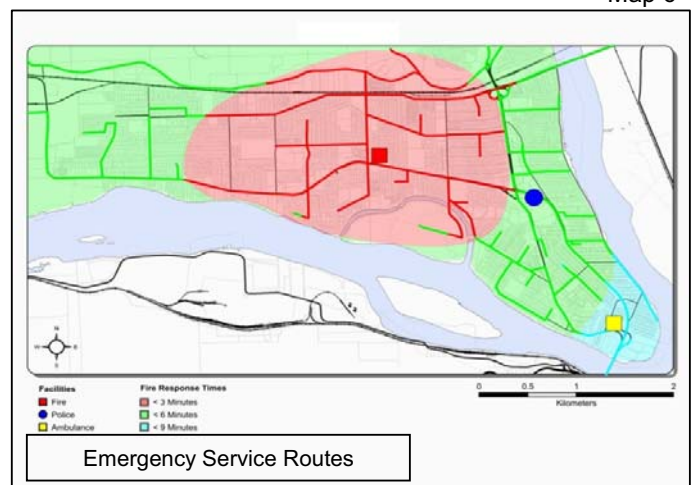
Map 4



Map 5



Map 6



Source: North Shore Background Report

# Background

## 1.0 BACKGROUND (cont.)

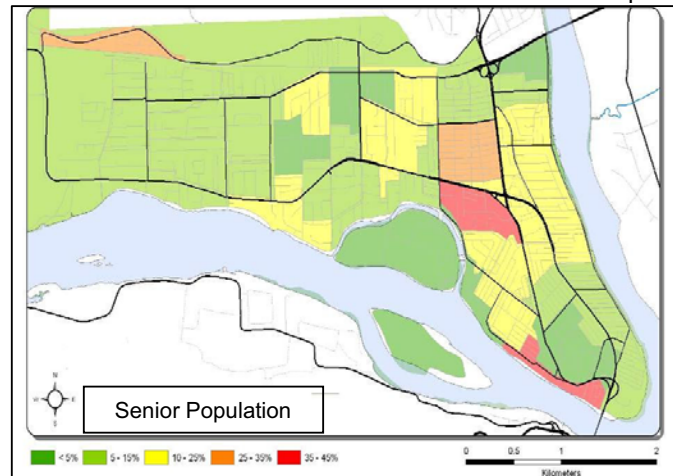
### 1.0.3 Social Connections

The North Shore is a community comprising a strong, complex and cohesive network of social connections. Representing the highest ethnically diverse community within Kamloops, the North Shore also boasts long-term residents whom have been raised and have retired within the same neighbourhood they were born.

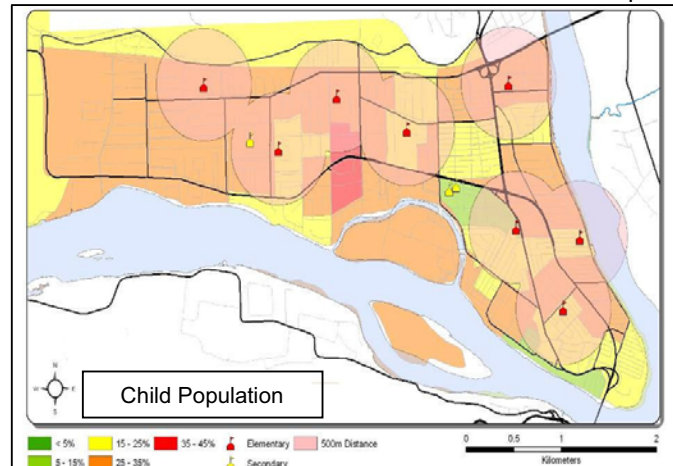
The North Shore Background Report provides a brief overview of some of the social connections that exist on the North Shore as well as a snapshot of the demographic representation of the community from the 2001 Census information:

- the old Village district of Tranquille Road is the predominant focus of activity for the North Shore in terms of social connections;
- there is a distinct lack of community gathering opportunities on the North Shore that typically help bring communities together for greater cultural involvement;
- there is a high degree of pedestrian activity that is also fragmented due to the lack of pedestrian linkages between major activity centres throughout the North Shore;
- there is a deficiency of both men and women in the 45-55 age category or those considered to be the top wage earners;
- there is a high percentage of female population, particularly located in high density areas;
- there are areas of Brocklehurst that do not offer opportunities for walking access to elementary schools;
- the average income for the North Shore is below the average for the rest of the city;
- there is higher than average level of rental accommodation in North Kamloops compared to Brocklehurst;
- a vast majority of local streets throughout the North Shore provide low-speed, safe environments for children to use;
- Tranquille Road, Fortune Drive, 8th Street, Parkcrest Avenue and the CN rail tracks act as significant barriers for the movement of pedestrians and especially children throughout the North Shore; and
- there is a high level of seniors population compared to the rest of Kamloops.

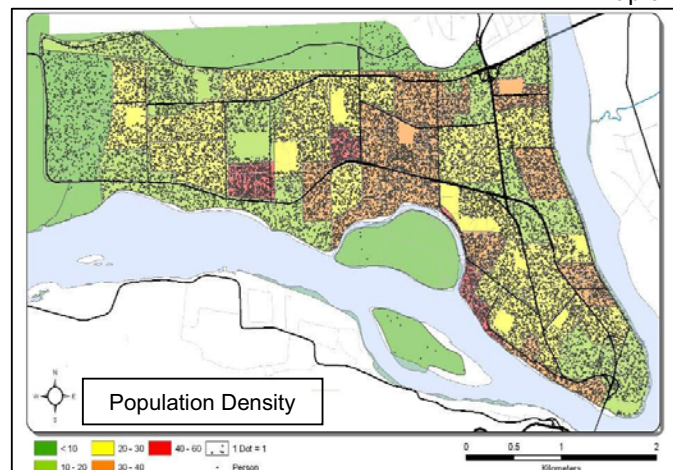
Map 7



Map 8



Map 9



Source: North Shore Background Report



# Background

## 1.0 BACKGROUND (cont.)

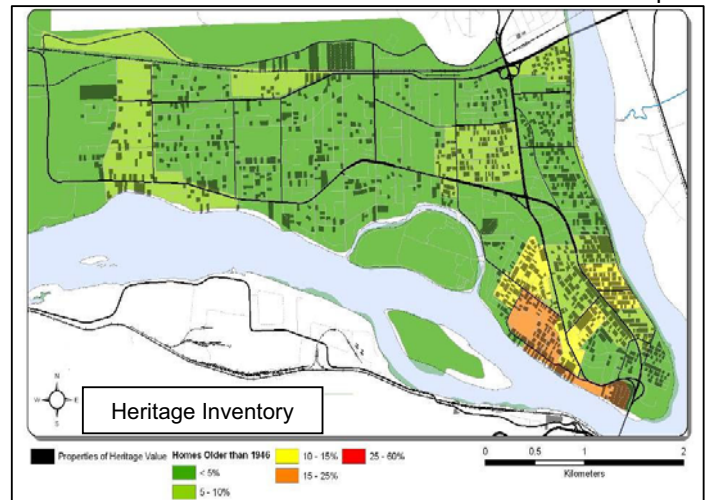
### 1.0.4 Image and Design

The North Shore has struggled with a negative image for many years. Given its existing, older and smaller housing stock, and its proximity to the Downtown and other South Shore commercial areas, the North Shore has not typically benefited from significant new commercial or residential development over the past 20 years. This has also been hindered by an area that is largely built out and is also highly constrained by existing road patterns, lot sizes and limited access points to the south.

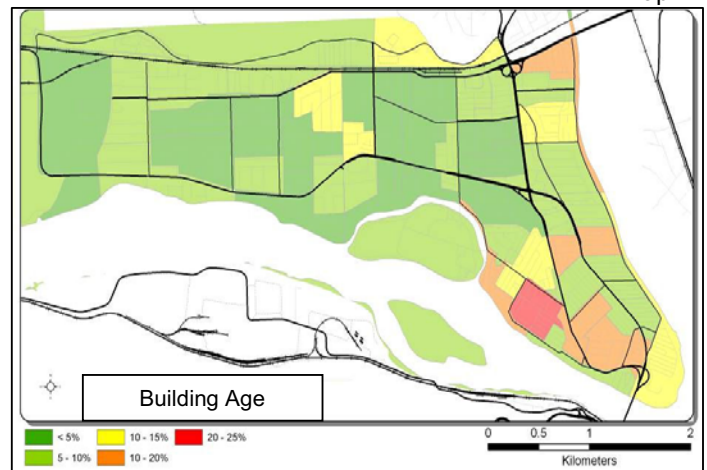
The North Shore Background Report examined several aspects related to the image and design of the North Shore. The findings provide a deeper understanding of the community and its overall configuration:

- statistically, the North Shore has lower than average property values, incomes and education levels which tends to manifest as a negative image or perception of the neighbourhood;
- the North Shore is an intensely complex community fuelled by many divergent socio-economic factors;
- school children identify school grounds, Tranquille Road, alleyways, Northhills Mall and the Brock Shopping Centre as being the top five most undesirable places to be within the North Shore;
- after reviewing RCMP calls for service, significant crime areas are limited to high activity commercial areas, a trend that is consistent with any highly urbanized environment;
- negative activities on the North Shore are reported and treated in a more prominent fashion than any other neighbourhood in the city;
- the North Shore has a rich past reflective of Kamloops' early Native populations, fur trading activities, rural farming and orchard production and a sophisticated irrigation system still visible today;
- the McDonald Park neighbourhood has the highest concentration of the oldest housing stock on the North Shore; and
- the adjacent river system, flat land and enviable weather conditions are three features that attract and keep residents living on the North Shore.

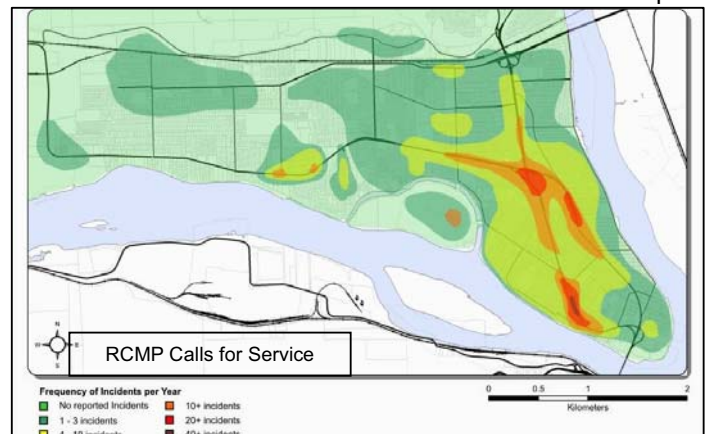
Map 10



Map 11



Map 12



Source: North Shore Background Report

# Background

## 1.0 BACKGROUND (cont.)

### 1.0.5 Report Summary

Research conducted by ND Lea Consultants Ltd. in preparing the North Shore Background Report is vital to understanding the past of the North Shore and current situations that residents, businesses and visitors are faced with. The Background Report identifies key issues that need to be addressed, if not in the North Shore Neighbourhood Plan, then through other planning processes. Some of these issues include:

**Water Supply** - the North Shore's water system is currently operating at near capacity; additional development and the ability for the North Shore and other adjacent areas to increase development will be limited until this issue is addressed.

**Infill Development** - infill will make up a majority of the new development that takes place within North Kamloops and within some areas of Brocklehurst. Vacant infill sites should be catalogued.

**Flood Plain** - about one third of the North Shore is located within a flood plain which restricts and limits development type and location; specific regulations are needed to address this development restraint.

**Sidewalks** - the North Shore was originally built with very different infrastructure standards compared to the current day standards; streetscape improvements and pedestrian safety issues are key elements that need to be addressed.

**Cyclists and Pedestrians** - the North Shore's flat topography is attractive for cyclists and pedestrians of all ages and user groups; additional work is required to ensure proper linkages exist between neighbourhoods that are safe and that provide a wide range of choice of travel routes.

**Image and Design** - the main obstacle for the North Shore's image rests in community care and maintenance, particularly of public space; however, steady and consistent public and private investment is needed for roads, lighting, building maintenance and beautification to help bolster the neighbourhood and civic pride that currently does exist.

The North Shore Background Report is available on the City of Kamloops website at: [www.kamloops.ca/communityplanning](http://www.kamloops.ca/communityplanning).

Source: North Shore Background Report



Entrance to McDonald Park; Photo: Carla Stewart



North Shore Entry Corridor; Photo: Carla Stewart



North Shore Village District; Photo: Carla Stewart

# Background

## 1.1 ACRONYMS AND DEFINITIONS

### Acronyms

CPTED	- Crime Prevention Through Environmental Design	SOV	- Single Occupancy Vehicle
DCC	- Development Cost Charges	TDM	- Transportation Demand Management
LEED	- Leadership in Energy and Environmental Design	TOD	- Transit-Oriented Development
NSBIA	- North Shore Business Improvement Association	TRU	- Thompson Rivers University
OCP	- Official Community Plan		

### Definitions

<b>Action Items</b>	documented tasks or activities that should take place. Action items are included throughout the North Shore Neighbourhood Plan within each section they apply to and are catalogued in one table, with priorities, as part of Appendix "C" of this Plan.
<b>CPTED</b>	Crime Prevention Through Environmental Design is the design and use of the built environment which can lead to a reduction in the fear and incidence of crime in an area.
<b>Density Transfer</b>	in general terms, means the transfer of allowable density (the number of units allowed on one site) from one parcel of land to another.
<b>Development Cost Charges</b>	are levied on new projects to help fund the cost of sewer, water, storm drainage, road and parkland services needed to accommodate growth. The amount of the Development Cost Charge (DCC) rates will vary from municipality to municipality and will depend on the condition and capacity of existing infrastructure, the location of new development, the type of land use and the characteristics of the development project.
<b>Eco-roof</b>	see "Green Roof".
<b>Goals</b>	are identified as elements that the community wants to achieve. Are used to measure development proposals against to determine if they are on line with the identified and perceived future.
<b>Green Development</b>	development that includes consideration of community-wide or regional environmental implications, as well as site-specific green building concepts, including such things as green buildings, green/eco-roofs, eco-friendly stormwater systems, on-site recycling and composting, grey water systems, rain harvesting and reuse of building materials.
<b>Green Roof</b>	located on a building (includes those with pitched roofs) that is partially or completely covered with vegetation, soil and/or growing medium, planted over a waterproof membrane. Can also include extensive coverage of roofs by container gardens or planters. Includes eco-roofs.
<b>Green Streets</b>	are streets that do not just convey unfiltered rain water into stormwater systems but are designed to intercept and clean rain water, using a natural system, before it enters into nearby water systems.
<b>Grey Water Recycling</b>	grey water is made up of water that has already been 'used' but that is clean enough to be 're-used' for other purposes. Typically made up of bath, shower, laundry and bathroom sink water, grey water can be used to irrigate landscaped areas rather than using treated drinking water. Grey water does not include water used from toilets and also from the kitchen sink or dishwasher because of food particles.
<b>LEED</b>	a standard used as a benchmark for the design, construction and operation of high performance green buildings. Establishes building criteria for: sustainable site development, saving water use, having energy efficiencies, using appropriate materials in construction and creating a high level of indoor environmental quality. LEED has been endorsed by the Canada Green Building Council and is also expanding its criteria to a neighbourhood level to promote healthy, green and sustainable development.



# Background

## 1.1 ACRONYMS AND DEFINITIONS (cont.)

<b>Mixed-use</b>	combining commercial and residential components in a single property.
<b>Modulation</b>	in the Design Guidelines, modulation is a stepping back or projecting forward of portions of a building face within specified intervals of building width and depth, as a means of breaking up the apparent bulk of a structure's continuous exterior walls.
<b>Non-Market</b>	any form of housing which meets Canadian Mortgage and Housing Corporation's (CMHC) definition of 'affordable', including, without limitations, rental housing, homeownership, assisted/supportive housing and seniors housing.
<b>Official Community Plan</b>	a document that is developed to outline a community's long-term vision. The policies and objectives in the Official Community Plan (OCP) help guide decisions on planning and land use management and represents one of the most important policy documents for Kamloops.
<b>Park and Share</b>	areas of parking that are strategically located to allow several businesses to use at the same time to help reduce the need to have individual parking lots on every commercial property. A sustainable development concept.
<b>Pedestrian-Oriented Space</b>	an area which promotes visual and pedestrian access onto the site and which provides pedestrian-oriented amenities and landscaping to enhance the public's use of the space for passive activities.
<b>Pedestrian-Oriented Use</b>	a pedestrian-oriented business is a commercial enterprise whose customers commonly arrive to the business by foot, or whose signage, advertising, window displays and entry ways are oriented toward pedestrian traffic.
<b>Policies</b>	statements that guide decision making by establishing preferences for the future. Policies in this Plan are consistent with the vision, goals and principles supported by the North Shore community.
<b>Principles</b>	are identified as rules of action or conduct used when examining development proposals. Should be taken into consideration and used as a measure to determine benefits to a community.
<b>Rain Harvesting</b>	the collection and storage of rain from roofs and other hard surfaces for future use. Usually stored in rain barrels or water storage tanks, the water is available to use during times when it is not raining.
<b>Scale, Human</b>	the perceived size of a building relative to a human being. A building is considered to have 'good human scale' if there is an expression of human activity or use that indicates the building size.
<b>Single Vehicle Occupancy</b>	an automobile whose only occupant is the driver.
<b>Streetscape</b>	the visual character of a street as determined by various elements such as structures, greenery, open space, view, etc.
<b>Sustainable Development</b>	balancing human need with environmental protection so that human and environmental needs can be met in the present and into the indefinite future.
<b>Transportation Demand Management</b>	a form of community and transportation planning that creates opportunities to reduce the use of single-occupancy vehicle trips by encouraging the use of transit, carpooling, biking and/or walking.
<b>Transit-Oriented Development</b>	a concept where development centres around high quality public transportation systems, to eliminate the need for a complete dependence on an automobile and to create communities that are compact, diverse and walkable.
<b>Vision</b>	a statement or set of statements depicting a concept desired into the future. Establishes a visual idea of what a community wants to become.

# Introduction



# Introduction

## 2.0 BACKGROUND

The City of Kamloops was created through the amalgamation of several townships, districts and outlying areas to form the present-day municipal boundaries. Of those several amalgamations, North Kamloops joined Kamloops in 1967 and the District of Brocklehurst followed in 1973. A planning exercise was undertaken in 1974 to establish future growth plans for the newly created city and since that time, various neighbourhoods and areas have undergone supplemental planning exercises. Plans have been prepared for Dallas, Barnhartvale, Aberdeen, Westsyde and Batchelor Heights to direct growth and future development patterns. Although some planning work has been done for the Airport Lands, McDonald Park, Tranquille Road and 8th Street, North Kamloops and Brocklehurst have gone through nearly 40 years without a comprehensive development plan that collectively addresses land uses, densities and future direction. In 2005, the newly adopted Official Community Plan identified the preparation of a neighbourhood plan for the North Shore as a priority. The planning process subsequently began in early 2005 and was finalized with the preparation of the North Shore Neighbourhood Plan three years later, in the summer of 2008.

## 2.1 VISION, GOALS AND PRINCIPLES

### 2.1.1 Vision

Through public input and consultation, an overall Vision for the future evolution of the North Shore has been established:

The North Shore will continue to be a **unique and diverse community** that complements its surroundings, contributes to the healthy growth of Kamloops and builds on its past.

The North Shore will be a **model of responsible development** that seeks to respect the natural environment, connects to its neighbours, provides for housing choice and reuses its existing built and natural assets where possible.

The North Shore will continue to be an **integrated community** that reflects all its cultural identities, embraces and models creative solutions to social issues and development challenges, retains and enhances a strong and vital economy, accommodates growth without exploiting the environment and retains its small-town character.



The North Shore; Photo: David Wise

# Introduction

## 2.1 VISION, GOALS AND PRINCIPLES (cont.)

### 2.1.2 Goals

The process of establishing goals is a means to measure the success of a community in its efforts to support its broader vision for the future. The following goals have been identified for the North Shore:

**Innovation** - explore new and proven urban forms that create a more pedestrian, compact, green and complete neighbourhood that is more efficient, healthy, safe and livable.

**Environmentally Sensitive** - respect, conserve, and enhance natural assets of the community.

**Diversity** - provide for a range of types and styles of homes, local services and associated amenities that include all age groups and cultures and that compliment adjoining land uses.

**Adaptive Reuse** - use the significant natural, built and existing resources of a neighbourhood in redevelopment scenarios to create an exceptional and unique community.

**Value** - foster the creation of an outstanding neighbourhood that adds value to the greater community.

**Legacy** - re-create a complete community reflective of its past where residents and businesses can live, work and interact within their neighbourhoods without relying on other areas of the city.



Brocklehurst; Photo: David Wise



Scenic Views from the North Shore; Photo: David Wise

# Introduction

## 2.1 VISION, GOALS AND PRINCIPLES (cont.)

### 2.1.3 Principles

Identifying planning principles is a key factor to promoting sustainable development in that the principles identified here are a reflection of the long-term interests of the North Shore's residents, workers, business owners and visitors. These principles should be taken into consideration when examining development proposals and used as a measure to determine if there are benefits to the community and if new development is appropriate as proposed.

1. **Create a Diverse and Complete Community** - by providing a range of activities and land uses that enable residents to live, work and play within a convenient walking, cycling or transit-riding distance. Residents can choose to live, work, shop and play in close proximity to each other. People can access daily activities, transit is viable and local businesses are supported.
2. **Reinforce the Neighbourhood Heart** - by enhancing the Tranquille Commercial District and the North Shore Towne Centre as the recreational, cultural and economic hearts of the community.
3. **Protect the Distinctive Character** - by conserving and enhancing each neighbourhood's existing and unique elements, including road patterns, housing styles, open space networks and landscaped areas.
4. **Protect Housing Choice Availability** - by providing a variety of housing forms and sizes while also providing different ownership and rental opportunities, that collectively support a range of age groups, family types, lifestyles and income levels.
5. **Design for Safety and Security** - by ensuring CPTED (Crime Prevention Through Environmental Design) principles are included in the planning framework. This includes examining lighting, traffic calming measures, signage and building orientation and design for all development.
6. **Encourage Environmental Stewardship** - by providing the necessary development standards to ensure the protection of environmentally sensitive areas and by promoting continued environmental responsibility and lifestyles. Ensuring that development respects natural landscape features and has high aesthetic, environmental and financial value.
7. **Create Value by Design** - by encouraging innovative, high quality urban design that fits the existing and future built form, enhances streetscapes and refines the open space network.
8. **Provide for Adaptability** - by ensuring that the North Shore, as it grows and changes, can renew and adapt itself effectively to new social and economic conditions, programs, policies and technologies.
9. **Utilize Smarter and Cost-Effective Infrastructure and Green Buildings** - by encouraging the use of green technology and other similar systems that can save both money and the environment.

# Introduction

## 2.1 VISION, GOALS AND PRINCIPLES (cont.)

A community's future is defined by its vision and the goals and principles that are established to support the vision's direction. The following table illustrates the connection between the Vision, Goals and Principles established for the North Shore. These identifiers are used to measure whether development scenarios are appropriate and compatible with the direction for growth that is identified and supported by the community.

Table 2: Vision, Goals and Principles

Vision	
<u>Goals</u>	<u>Principles</u>
Adaptive Reuse	<ul style="list-style-type: none"> <li>- Reinforce the Neighbourhood Heart</li> <li>- Protect Housing Choice</li> <li>- Provide for Adaptability</li> </ul>
Environmental Sensitivity	<ul style="list-style-type: none"> <li>- Encourage Environmental Stewardship</li> <li>- Utilize Smarter and Cheaper Infrastructure and Green Buildings</li> </ul>
Diversity	<ul style="list-style-type: none"> <li>- Create a Diverse and Complete Community</li> <li>- Protect Distinctive Character</li> <li>- Protect Housing Choice Availability</li> </ul>
Innovation	<ul style="list-style-type: none"> <li>- Design for Safety and Security</li> <li>- Create Value by Design</li> <li>- Utilize Smarter and Cheaper Infrastructure and Green Buildings</li> </ul>
Value	<ul style="list-style-type: none"> <li>- Design for Safety and Security</li> <li>- Create Value by Design</li> <li>- Utilize Smarter and Cheaper Infrastructure and Green Buildings</li> </ul>
Legacy	<ul style="list-style-type: none"> <li>- Reinforce the Neighbourhood Heart</li> <li>- Protect Distinctive Character</li> <li>- Create Value by Design</li> </ul>



# Introduction

## 2.2 PURPOSE OF PLAN

The purpose of the North Shore Neighbourhood Plan is to provide a comprehensive land use plan that will direct and guide the growth and evolution of the North Shore over the next 5 - 10 years. The vision, goals, principles and corresponding policies listed in this document provide direction to City Council, City staff, residents, business owners, developers and visitors to ensure that balances between all elements of the community can be achieved. The North Shore Neighbourhood Plan will serve as the community's first fully comprehensive land use plan.

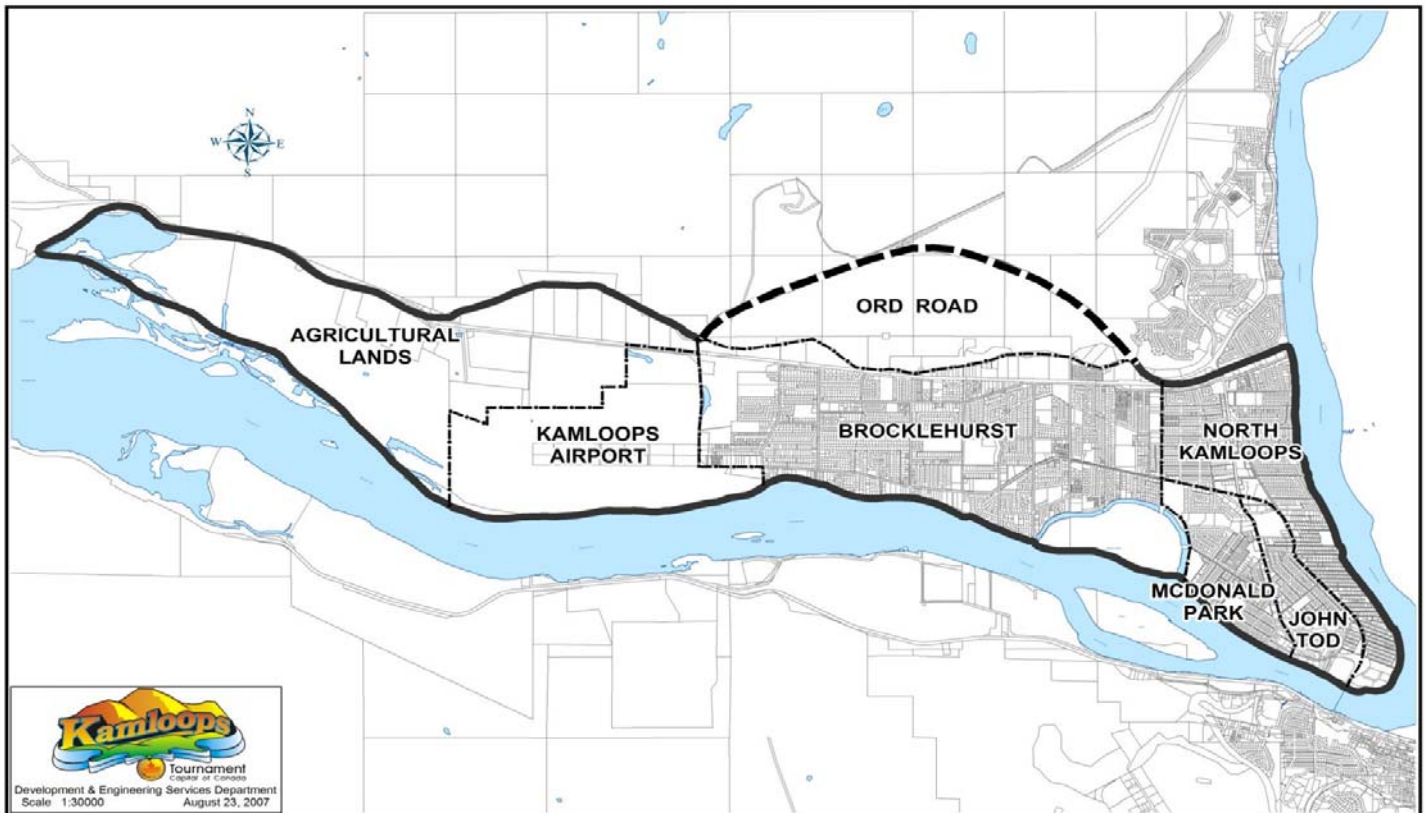
## 2.3 PROJECT BOUNDARY

The North Shore Neighbourhood Plan comprises an area of the city housing nearly one third of the total population of Kamloops.

Sub-neighbourhoods included in this Plan are:

- North Kamloops
- John Tod
- McDonald Park
- Brocklehurst
- Ord Road
- Kamloops Airport
- Agricultural Lands

Map 13: North Shore Project Area



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Dashed line represents a  
potentially adjustable boundary.

# Introduction

## 2.4 PROCESS

The North Shore Neighbourhood Plan was prepared through an extensive process of community consultation, including assistance provided by the North Shore Advisory Committee, technical support from City staff, a comprehensive public opinion survey and frequent meetings with major stakeholders in the community, including but not limited to, the North Shore Business Improvement Association (NSBIA), social service agencies, the RCMP, the Grasslands Conservation Council and Interior Community Services.

The North Shore Advisory Committee was composed of members of the NSBIA as well as Citizens at Large and were originally chosen by a consultant. Technical advice was provided by City staff and consultation work was provided by Urban Systems Ltd. for the preparation of the North Shore Strategy document and by ND Lea Consultants Ltd. for the preparation of the North Shore Background Report and for assistance with plan graphics. Stakeholder input remained specific to each agency's mandate.

The North Shore Neighbourhood Plan process is outlined below:

Table 3: North Shore Neighbourhood Plan Process

February 2005	Urban Systems Ltd. retained to prepare a strategy for the preparation of a North Shore Neighbourhood Plan.
July 2005	Key issues identified by Urban Systems Ltd.; plan process established.
August 2005	North Shore Advisory Committee formed; first meeting of Committee.
October/ November 2005	North Shore Advisory Committee reviews the identified issues and finalizes list.
January/ February 2006	ND Lea Consultants Ltd. retained to prepare a background report for the North Shore and to provide graphics for the land use plan.
March 2006	1,500 public opinion surveys sent out to residents and businesses; survey launched on City web page; first public open house held at Henry Grube Centre.
April 2006	Public opinion survey ends; results are provided to the public.
May/June 2006	Workshop meetings held with North Shore Advisory Committee to review potential development scenarios for each neighbourhood of the North Shore; meetings held with various stakeholders.
June/ August 2006	Preparation begins of the first draft of new zoning regulations and new policy statements.
September 2006	Technical workshop held with City staff to discuss redevelopment options; review of background report conducted.
October/ November 2006	Workshop held with North Shore Advisory Committee; second public open house held; on-going preparation of new policy and regulations.

cont....

# Introduction

## 2.4 PROCESS (cont.)

Table 3: North Shore Neighbourhood Plan Process (cont.)

December 2006/ March 2007	Review of draft policy and regulations; preparation of new Development Permit Guidelines.
March/June 2007	Preparation of supporting graphics for land use plan; stakeholder meetings ongoing; review of policy and regulations with North Shore Advisory Committee and other stakeholders; third open house held.
October/December 2007	Refinement of regulations and policies. Finalize Development Incentives Matrix and Development Checklist.
February/April 2008	Final draft distributed to stakeholders; two workshops held with City Council to identify major elements of the Plan and to discuss the North Shore Incentive Program.
May/June 2008	Public Hearing; adoption of new by-laws.



# Introduction

## 2.5 COMMUNITY INPUT

### 2.5.1 North Shore Planning Committee

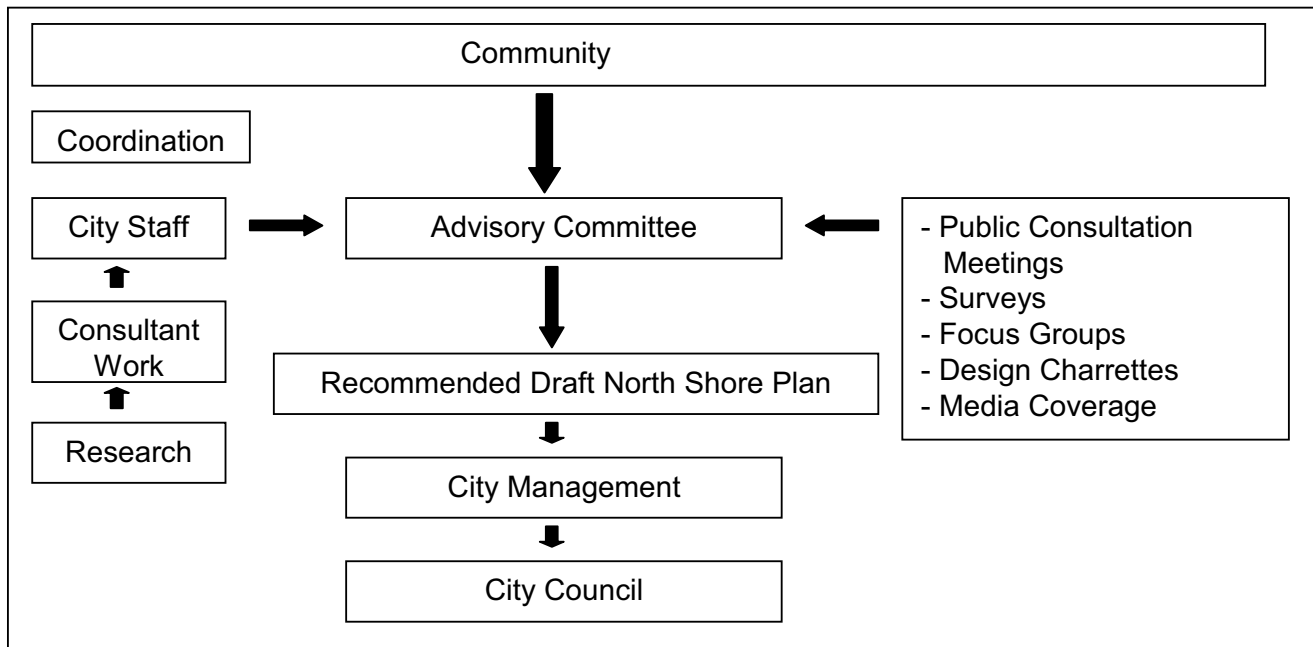
Urban Systems Ltd. was retained in February 2005 to prepare a strategy for the preparation of a North Shore Neighbourhood Plan. A component of the strategy was to form a North Shore Advisory Committee that would provide guidance to City staff throughout the creation of the Plan. Urban Systems Ltd. established the committee structure comprising three Citizens at Large, three members of the North Shore Business Improvement Association, one North Shore real estate representative and one TRU (Thompson Rivers University) representative. With the addition of one member of City Council, the Committee was finalized at ten people.

The North Shore Advisory Committee met extensively during the first phase of public consultation ending with a series of workshops in June 2006. During the second phase of public consultation, in the latter part of 2006, the Committee was instrumental in providing feedback on various redevelopment scenarios and in representing the interests of the community in determining an appropriate direction for the Plan and its land use policies.



North Shore Advisory Committee Meeting;  
Photo: David Wise

Figure 1: Community Input Process



# Introduction

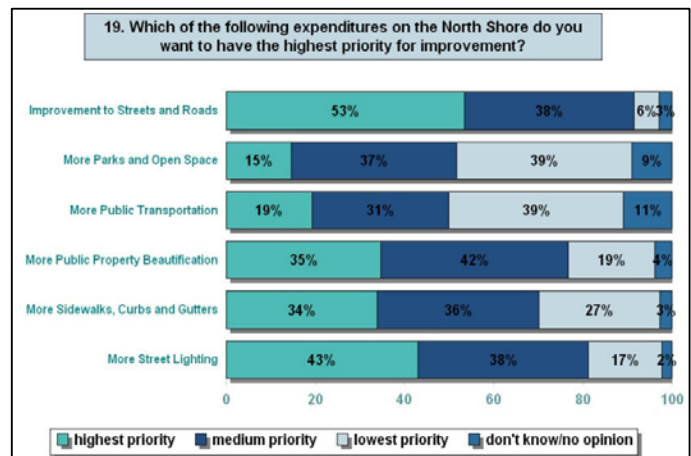
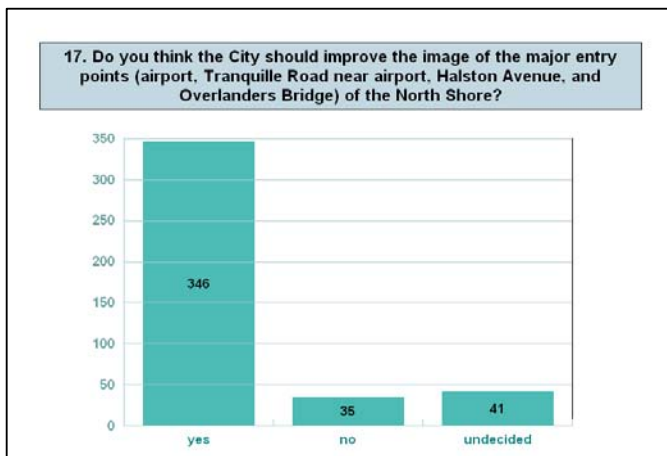
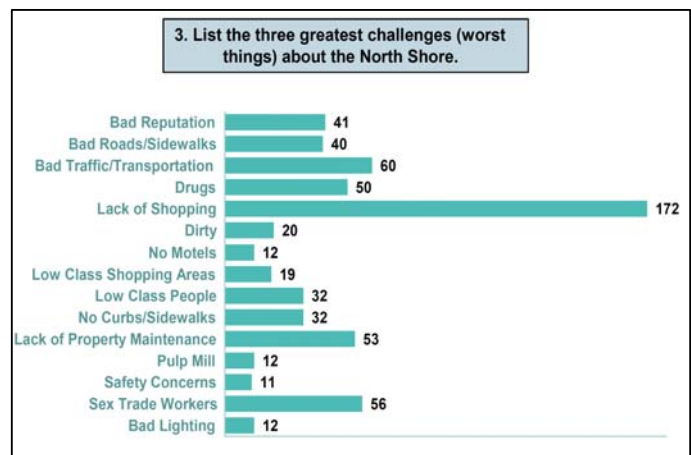
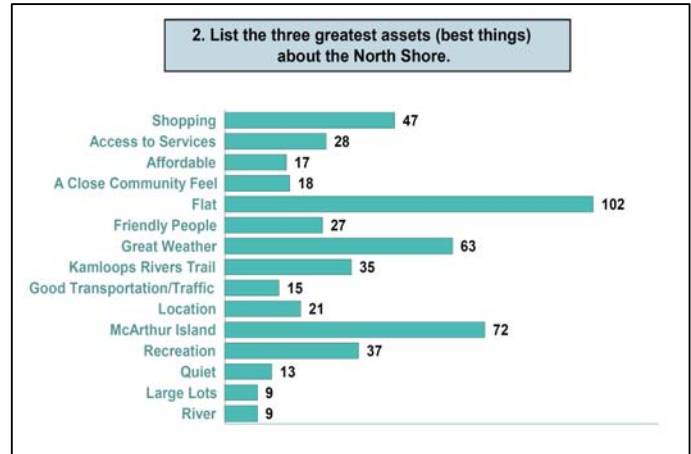
## 2.5 COMMUNITY INPUT (cont.)

### 2.5.2 Public Opinion Questionnaire

In March 2006, questionnaires were sent out to 1,500 random residents and businesses on the North Shore. The questionnaire was also launched on the City's web page and was available for pick up by the public. The questionnaire ran for a two-week period providing the North Shore community an opportunity to identify issues affecting them and to provide direction for a preferred future. Over 33 per cent of the questionnaires were returned indicating a high level of participation. Results of the questionnaire were made available on the City's web page and were sent to the North Shore Advisory Committee, stakeholder groups and media personnel.

The results of the survey provided a comprehensive understanding of the issues faced by North Shore residents and businesses and were used to establish strategies and redevelopment scenarios for specific areas as well as the creation of draft policy statements for the land use plan.

The four charts provided here are examples of some of the results tabulated from the returned questionnaires. The full set of results can be viewed on the City's web page: [www.kamloops.ca/communityplanning](http://www.kamloops.ca/communityplanning).



# Introduction

## 2.5 COMMUNITY INPUT (cont.)

### 2.5.3 Open Houses

The first public open house for the North Shore Neighbourhood Plan was held on March 22, 2006 at the Henry Grube Centre. The open house was the first opportunity for residents and business owners to become acquainted with the planning process, to provide feedback on various development scenarios and to fill out the North Shore Plan Questionnaire.

Two other open houses were held throughout the preparation of the North Shore Neighbourhood Plan: in November 2006 to gain feedback about potential solutions identified in response to the issues raised from the first open house and in June 2007 to gain feedback about development permit guidelines and design concepts.

#### March 2006 Open House



Open House Attendance; Photos: Carla Stewart



# Introduction

## 2.5 COMMUNITY INPUT (cont.)

### 2.5.3 Open Houses (cont.)

#### November 2006 Open House



#### June 2007 Open House



Open House Attendance; Photos: Carla Stewart

# Introduction

## 2.6 KAMPLAN: OFFICIAL COMMUNITY PLAN (OCP)

The preparation of the North Shore Neighbourhood Plan has focused on policy and future direction that is not only supported by the community but that is also consistent with the preferred future established in KAMPLAN: The Official Community Plan for Kamloops 2004. Several supporting plans identified in the OCP were reviewed and used in conjunction with the policies established for the North Shore Neighbourhood Plan. These documents include:

- Kamloops Pedestrian Master Plan
- Kamloops Airport Lands Marketing Strategy
- Kamloops Parks and Recreation Master Plan
- Kamloops Airport Land Use and Development Plan
- North Shore Revitalization Strategy and Concept Plan
- TravelSmart
- McDonald Park Neighbourhood Plan
- Kamloops Cultural Strategic Plan
- 8th Street Corridor Plan
- North Shore Development Opportunity Assessment
- Tranquille Development Permit Facade Design Guidelines
- Kamloops Bicycle Master Plan
- Kamloops Transit Plan, 2000 - 2020 (draft) to 2007
- City of Kamloops Social Plan
- John Tod Neighbourhood Community Development Project

Several goals of the Official Community Plan are also met from the policies and direction set in the North Shore Neighbourhood Plan:

Table 4: Official Community Plan Goals

Official Community Plan Goal	OCP Goal Addressed in North Shore Neighbourhood Plan
REGIONAL GROWTH STRATEGY - to create a balance among human settlement, economic development and environmental conservation such that development does not significantly limit the options of future generations.	✓
COMMUNITY GROWTH - to develop in a manner which will lead to a more compact, sustainable and efficient land use form while maintaining and enhancing the community's livability.	✓
NEIGHBOURHOOD GROWTH - to maintain and develop integrated, sustainable and livable neighbourhoods; to develop more compact and cost-effective neighbourhoods.	✓
SPECIAL DEVELOPMENT AREAS - to reserve sufficient lands to accommodate growth to the year 2036 or a population horizon of 120,000.	✓
RESIDENTIAL DEVELOPMENT - to encourage infill, intensification and full utilization of existing service capacity prior to peripheral expansion; to ensure a broad range and choice of housing types and locations capable of meeting the needs of residents of various age groups, family types, lifestyles and income levels; to ensure that residential development proceeds in an orderly, cost-effective manner; to retain established neighbourhood character, amenities and quality when encouraging more diversity; to encourage innovation, flexibility and quality in the provision of housing; to encourage suitable housing opportunities and convenient community services for residents having special housing requirements; to encourage suitable affordable housing options for Thompson Rivers University students.	✓
AFFORDABLE HOUSING - to continue to be a leader in providing innovative solutions that increase the supply of affordable housing to meet the needs of Kamloops' residents.	✓

(cont...)

# Introduction

## 2.6 KAMPLAN: OFFICIAL COMMUNITY PLAN (OCP) (cont.)

Table 4: Official Community Plan Goals (cont.)

Official Community Plan Goal	OCP Goal Addressed by North Shore Neighbourhood Plan
COMMERCIAL DEVELOPMENT - to encourage efficient utilization of existing serviced and designated lands prior to outward expansion; to support the role and function of the City Centre, Tranquille Commercial District and North Shore Towne Centre as the primary commercial and cultural centres; to encourage a high standard of development to enhance the urban environment; to establish an orderly framework for guiding commercial activity; to encourage innovation and quality in design and development; to provide sufficient vocational opportunities for commercial enterprises; and to establish a strong, diversified commercial base to provide employment and a high level of service to residents and visitors.	✓
INDUSTRIAL DEVELOPMENT - to encourage industrial development as a means of expanding and diversifying the City's economy and tax base; to encourage a high standard of development to enhance and protect the urban and natural environment; and to encourage efficient utilization of existing services and serviced land.	✓
AGRICULTURAL/RESOURCE LANDS - to preserve and protect environmentally sensitive and unique natural areas; to ensure that agricultural lands are preserved for agricultural purposes and to enhance the viability of agricultural operations within the City; to discourage urban development into the Agricultural Land Reserve; and to retain public access to Crown lands for recreation and other public use purposes.	✓
SAND/GRAVEL EXTRACTION - to designate areas of existing and future sand and gravel extraction; to minimize land use conflicts between sand and gravel extraction operations and neighbouring properties.	DID NOT ADDRESS
PARKS AND RECREATION - to ensure that all citizens of Kamloops have adequate opportunities for personal growth in their leisure, thereby enriching the life of individuals and the community; to protect, preserve and enhance the environment for the enrichment of the community and the enjoyment of all people; to ensure that public parkland of all types is distributed throughout the community in an equitable manner to meet the community's needs; to develop and maintain an aesthetically appealing environment; and to support the development of a strong community image.	✓
COMMUNITY FACILITIES - to ensure that community and social services are available to meet the needs of existing and future residents; to designate sufficient land to accommodate the needs of community and social service facilities; to maintain service levels, improve where feasible and encourage maximum utilization of existing service systems prior to expansion; and to encourage sensitive integration of small-scale care facilities within neighbourhoods.	✓
HERITAGE - to preserve, enhance and promote the community's cultural heritage for the benefit of residents and visitors alike.	✓
POLICE AND FIRE PROTECTION - to provide and maintain police and fire protection service levels in accordance with the growing and changing needs of the community.	DID NOT ADDRESS

(cont...)

# Introduction

## 2.6 KAMPLAN: OFFICIAL COMMUNITY PLAN (OCP) (cont.)

Table 4: Official Community Plan Goals (cont.)

Official Community Plan Goal	OCP Goal Addressed by North Shore Neighbourhood Plan
SOCIAL PLANNING - to encourage and assist in the development of social action strategies that strengthen and enhance the well-being of our community; and to recognize the City's limited revenue base when determining an appropriate level of involvement in addressing social issues.	✓
ECONOMIC DEVELOPMENT - to develop a strong, diversified and sustainable economy that will provide expanded opportunities for employment, support community growth and foster community pride; and to encourage economic development while maintaining an equitable balance between business/industry and residential taxpayers.	✓
GENERAL SERVICING STRATEGY - to provide a high level of service in a cost-effective and efficient manner, balancing demands with affordability.	✓
NATURAL ENVIRONMENT - to protect and enhance the quality of the natural environment; and to protect and enhance fish and wildlife habitats in balance with flood protection and recreational access to riverbanks and open space areas.	✓
URBAN ENVIRONMENT - to develop and maintain an aesthetically appealing and environmentally sensitive urban environment to enhance the quality of life for residents and visitors alike.	✓
HAZARD LANDS - to ensure public safety and protection from property damage from areas exhibiting hazardous conditions.	✓
ENVIRONMENTALLY SENSITIVE AREAS - to preserve and protect environmentally sensitive and unique natural areas; and to protect and enhance fish and wildlife habitats in balance with urban development and human use and enjoyment of open space.	✓



The North Thompson River; Photo: Carla Stewart

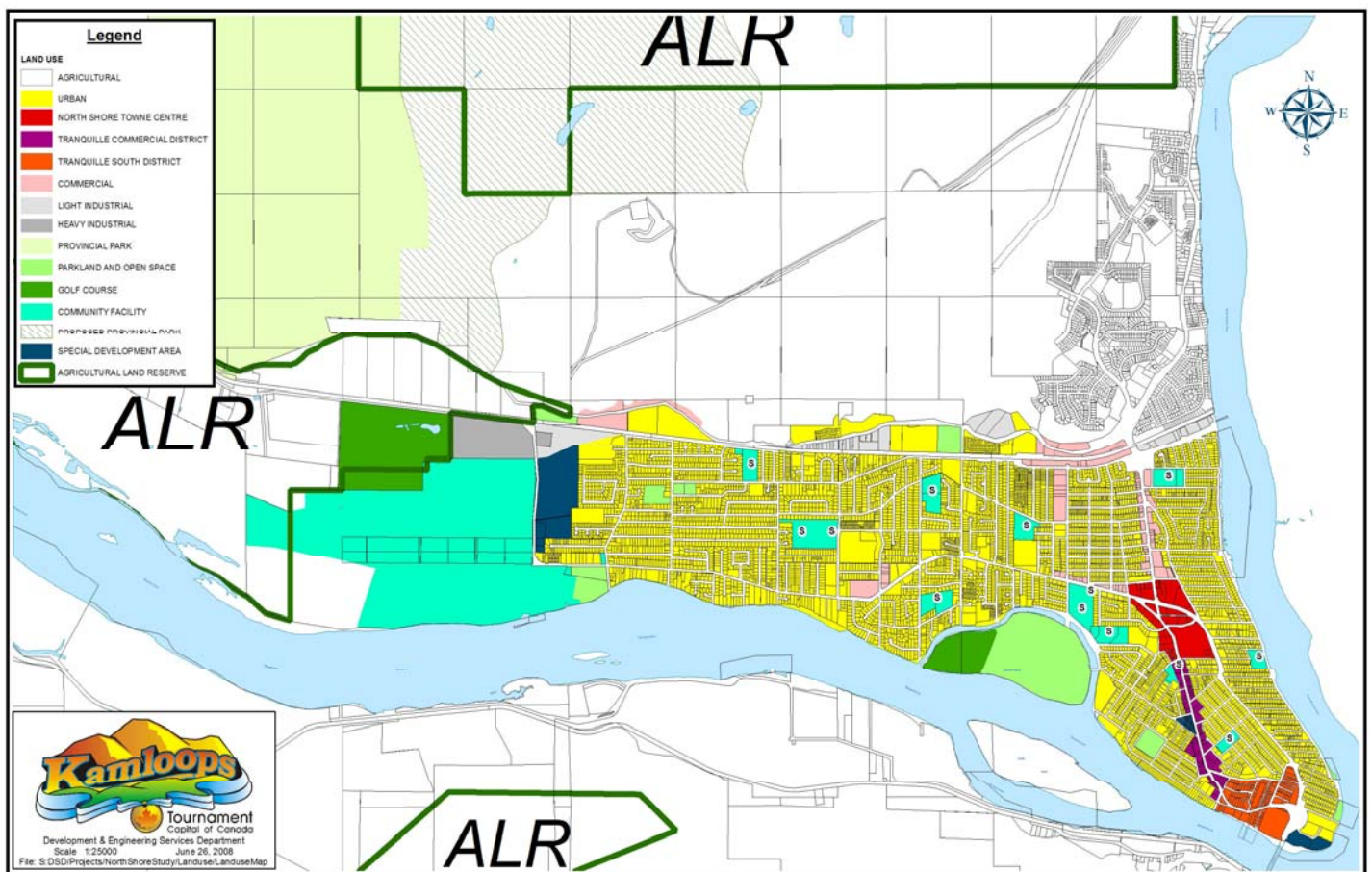


# Introduction

## 2.7 LAND USE DESIGNATIONS

The North Shore comprises several different categories of land uses, each represented with KAMPLAN: The Official Community Plan. Land uses are depicted here to give a general understanding of how the North Shore is developed and to give a direction for how future development and evolution is anticipated to take place. This map was used in the preparation of policy and regulations for the North Shore Neighbourhood Plan.

Map 14: General Land Use Map



## General Policies

# General Policies

## 3.0 GROWTH MANAGEMENT

The North Shore, Kamloops and the Thompson-Nicola Regional District have recently experienced significant economic growth and the corresponding development and expansion pressures that are ultimately a result of a vibrant market. Dealing with development pressures in today's urban environment requires creativity and ingenuity resulting in more dense but vibrant communities. The North Shore currently comprises neighbourhoods that are already well established and reasonably dense. In order to continue to accommodate the city's population growth, density increases, infill development and upgrading existing housing stock will continue to be the dominant means by which residential and commercial uses are accommodated.

The North Shore has several natural and man-made boundaries that serve to contain growth. The North Thompson and Thompson Rivers, Kamloops Airport, the CN rail line, and, to a certain extent, the grasslands and steep slopes of the hillsides north of Ord Road all serve as boundaries for North Shore development. In order therefore, for the North Shore to accommodate approximately 21 per cent of the city's future growth to the year 2021, as identified in the Official Community Plan, development will need to be focused toward infill and intensification. Sustainable development standards, environmental design elements and creative and innovative building styles will all be necessary to support smart growth principles while integrating new development into existing and long-established North Shore neighbourhoods.

### **ACTION ITEMS:**

- a. Develop density transfer opportunities for residential and mixed-use developments within the Tranquille Commercial District and North Shore Towne Centre.
- b. Establish a new Lac Du Bois Grasslands Park boundary, in conjunction with identifying an urban growth boundary, for the northwestern limits of the City of Kamloops.

### **SUPPORTING REGULATIONS:**

- a. North Shore Zoning Code

### **POLICY STATEMENTS:**

1. The City will encourage more residents to live within the Tranquille Commercial District and the North Shore Towne Centre.
2. The City will encourage the concentration of higher density residential development to take place adjacent to commercial centres and along transit routes.
3. The City will encourage the use of existing roads and infrastructure in supporting new residential development.
4. The City will encourage use of density transfers to concentrate residential development in or adjacent to the Tranquille Commercial District and North Shore Towne Centre.
5. The City will support reductions in parking and building setbacks where appropriate, and where residential developments are located adjacent to transit routes and near, adjacent to or within the Tranquille Commercial District and/or North Shore Towne Centre.
6. The City will encourage the extension of the Lac du Bois Grasslands Park to enable further protection of this Environmentally Sensitive Area.
7. The City will discourage the downzoning of existing multiple-family properties to a single family or two family zoning.
8. The City will continue to support the recommendations of TravelSmart in conjunction with population growth and the timing of any additional infrastructure improvements necessary to accommodate future growth needs of the North Shore and the City as a whole.

# General Policies

## 3.1 ECONOMIC GROWTH

The economic potential of the North Shore is affected in large measure by its advantageous location to the Kamloops airport, its close proximity to three major highway systems, its access to a major rail line, its flat, serviced land and its typically lower and affordable land values. The North Shore also offers a wide range of housing options, affordable accommodation, world-renowned sporting and recreation facilities and excellent weather conditions found within Kamloops.

The North Shore boasts the availability of light to heavy industrial land, the airport, a major economic generator, and retail, service and mixed commercial areas within a location that is easily accessible to the downtown and to the Sahali Town Centre by two bridges and an arterial road network system that accommodates all vehicle types including dangerous goods trucking.

### **ACTION ITEMS:**

- a. Prepare a strategy plan with the North Shore Business Improvement Association, with input from Venture Kamloops, to help increase awareness of the North Shore's business opportunities and to promote the benefits of locating businesses on the North Shore.
- b. Prepare a revitalization strategy for the North Shore Towne Centre, Tranquille Commercial District and the Tranquille South area, that acts as a showcase for the North Shore's main commercial areas.
- c. Prepare a land inventory identifying development opportunities within the commercial and industrial areas of the North Shore.

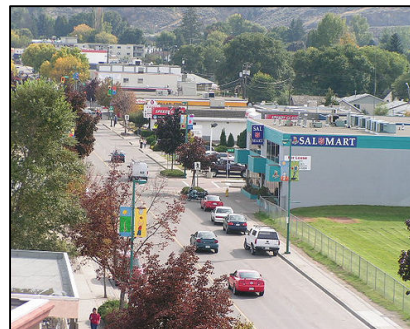
### **SUPPORTING REGULATIONS:**

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

The North Shore is also highly under-utilized from a commercial point of view. With one third of the city's population living in this neighbourhood, many residents are forced to other parts of the city to obtain access to clothing stores or a basic selection of consumer goods. Through the public consultation phases of this Plan, many residents expressed a deep desire to be able to conduct all their shopping needs on the North Shore. This desire creates many opportunities for businesses to serve a population that shows loyalty to its community and that has indicated they would prefer to shop within their own neighbourhood.



Northhills Shopping Mall; Photo: Carla Stewart



Tranquille Commercial District; Photo: Carla Stewart



Industrial Tank Farm; Photo: Carla Stewart

# General Policies

## 3.1 ECONOMIC GROWTH (cont.)

### POLICY STATEMENTS:

1. The City will encourage the economic focus of the North Shore to be directed and refocused to the Tranquille Commercial District and to the North Shore Towne Centre.
2. The City will encourage, support and promote attractions and events that would bring visitors and business opportunities to the North Shore.
3. The City will encourage employment areas that are diverse, competitive and balanced and that provide employment and shopping opportunities that are accessible by transit networks on the North Shore.
4. The City will encourage the increasing role that tourism and recreation are playing in the North Shore's economy.
5. The City may enter into partnerships with the private sector to promote the development and marketing of the commercial and industrial lands of the North Shore.
6. The City will continue to support the airport in its function and importance in generating economic activity for the North Shore and for Kamloops as a whole.
7. The City will encourage improvements and redevelopment of the Tranquille Commercial District and the North Shore Towne Centre to be focused toward maintaining both these areas as the most comprehensive and diverse focus of the North Shore, including encouraging significant retail, office services, entertainment, government, institutional, residential and pedestrian-oriented commercial functions to locate there.
8. The City will encourage both the North Shore Towne Centre and the Tranquille Commercial District to continue as the major commercial centres of the North Shore and will support new development in other parts of the North Shore when it does not negatively impact the function of these two key areas.
9. The City will actively promote the North Shore as a tourist destination recognizing the potential significance of tourism in connection with the McArthur Island facility and the Tournament Capital program.
10. The City will support the preparation of a comprehensive evaluation of commercial land use needs, including identifying uses that should be retained and/or promoted, creating a comprehensive inventory of commercial businesses and identifying gaps in commercial services and the needs of the community.

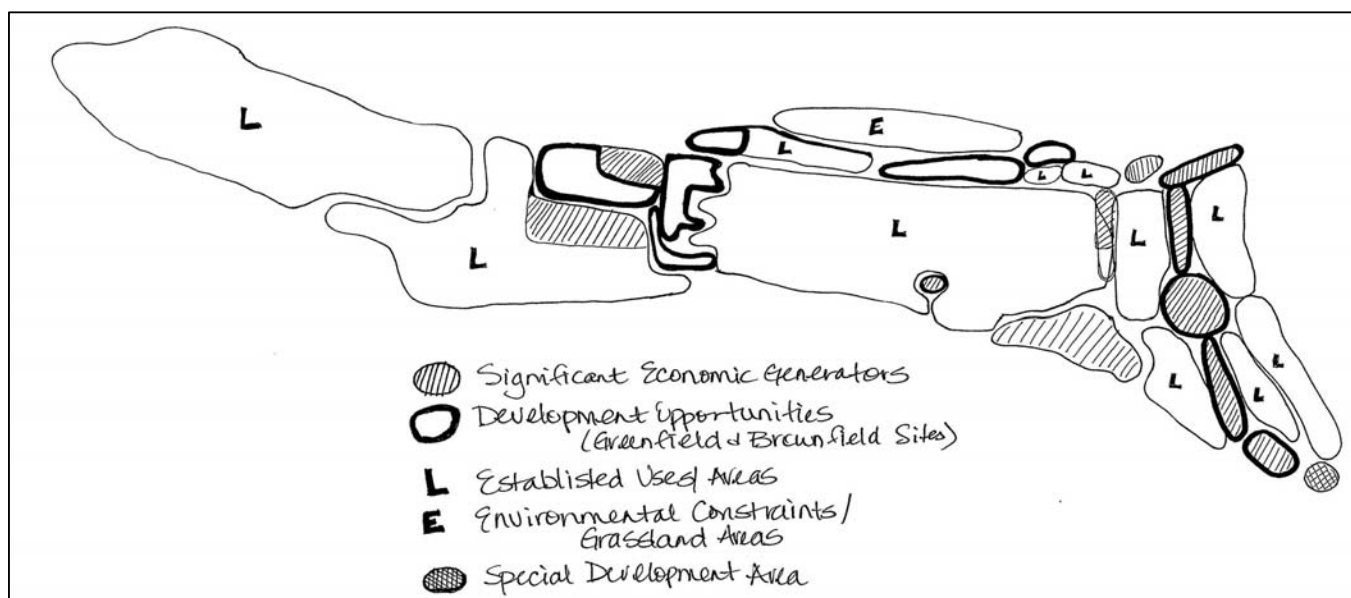


# General Policies

## 3.1 ECONOMIC GROWTH (cont.)

The North Shore is a well established and largely built-out community. As the Economic and Development Opportunities Map illustrates, the main areas where development opportunities are within the North Shore Towne Centre and Tranquille Commercial District which also act as the significant economic generators for the neighbourhood. Industrial land at the airport, residential land at the west end of Brocklehurst and commercial areas along Ord Road are the last remaining significant vacant sites on the North Shore that are able to accommodate development. The map also shows the Henry Grube site as a Special Development Area which would require an extensive review, including public involvement, prior to determining what development, if any, should take place there.

Map 15: Economic and Development Opportunities



Although there are significant areas of the North Shore shown as having established uses, the density increases, under current regulations, can be substantial. Originally zoned at a sustainable two-family density, rather than just a single family density, the North Shore has the potential to nearly double its population. Infill development and new housing projects however, should ensure that sensitive integration takes place. The North Shore has a significant amount of zoning designated at a two-family density. Maintaining this density is important to be consistent with the sustainability goals established by the community. However, much of these two-family zoned areas were constructed with a single family dwelling character that is important to retain and should be reflected in the designs of new development, even where densities are two-family or greater.

Additional economic information on the North Shore is available in the North Shore Background Report: [www.kamloops.ca/communityplanning](http://www.kamloops.ca/communityplanning).



# General Policies

## 3.2 COMMUNITY IMPROVEMENTS

Community pride is often directly connected to the appearance of a neighbourhood, both in its public space and in its private and semi-private holdings. Maintenance, rehabilitation and public and private investment are important elements in establishing and sustaining community pride.

### POLICY STATEMENTS:

1. The City will establish a coordinated approach to community improvements reflecting the problems, priorities and financial resources of the North Shore and the City of Kamloops.
2. The City will encourage and support private efforts to rehabilitate and redevelop property, which will have a positive impact on the stability and growth of the area.
3. The City will encourage and improve existing recreational services and construct new facilities, as required, to meet the needs of residents.
4. The City will support stabilizing and improving the property and business tax base in residential, commercial and other employment areas, by upgrading public property and services to stimulate private investment.
5. The City will support the use of pilot projects as a cost-effective means to determining the success of proposed infrastructure improvements.
6. The City will support the North Shore Business Improvement Association and its programs and efforts toward beautification and toward increasing the economic viability and successes of the North Shore.



Tranquille Sidewalk Improvements;  
Photo: John Popoff



McDonald Park Improvements;  
Photo: Carla Stewart

### ACTION ITEMS:

- a. Acquire property for strategic purposes, involving such matters as parkland, transportation improvements, urban beautification efforts and off-street parking.
- b. Develop a program to encourage, educate and provide incentives for property maintenance and beautification for both residential and commercial properties.
- c. Develop a program for the installation and maintenance of landscaping and beautification treatments within the Tranquille Commercial District, the North Shore Town Centre and the Tranquille South area as a cost share initiative between the City, the North Shore Business Improvement Association and local North Shore businesses.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

# General Policies

## 3.3 COMMUNITY FORM AND IDENTITY

The North Shore community is a unique assortment of neighbourhoods representing various diverse land uses, ethnic and age groups, topographical forms, stock of heritage buildings, agricultural dominance and natural areas and river systems. As part of the community consultation phase, residents and business owners identified that it is important to recognize and protect the positive aspects of the North Shore's form and identity; therefore, ensuring that all new development makes a positive contribution to the quality and character of the North Shore is important.

The City of Kamloops is also in an enviable position of showing leadership in this community by taking proactive steps to set the stage for the kind of development and public spaces that are supported in the Official Community Plan and by residents, businesses and visitors. Ensuring that new development is of a high quality and that improvements achieve multiple purposes such as improving traffic flow, coordinating access management and parking, increasing pedestrianization and enhancing the aesthetic of the public realm are key elements to celebrating and recognizing the North Shore's community form and identity.

### POLICY STATEMENTS:

1. The City will encourage new development to be located and organized to fit within existing neighbourhoods or within a planned context. New development shall frame and support adjacent streets, parks and open spaces to improve safety, pedestrian interest and casual views of these spaces.
2. The City will encourage new development to locate and organize vehicle parking, vehicle access, service areas and utilities to minimize their impact on adjacent properties and to also improve the safety and attractiveness of adjacent streets, parks and open spaces.
3. The City will support the use of performance and flex-based zoning and development permit regulations to encourage new development to be massed to fit harmoniously into existing surroundings and to respect and improve the local scale and character.
4. The City will encourage infill development to consider and reinforce the general physical patterns and character of established neighbourhoods with respect to:
  - a) patterns of streets, blocks, lanes, parks and public building sites;
  - b) general size and configuration of lots, height, massing, scale and type of dwelling unit compatible with that permitted by the North Shore Zoning Code for adjacent residential properties;
  - c) prevailing building types;
  - d) setbacks of buildings from the street or streets;
  - e) prevailing patterns of rear and side yard setbacks and landscaped open space;
  - f) constructing specific landscape or built form features that contribute to the unique character of a neighbourhood; and
  - g) conserving heritage buildings, structures and landscapes.
5. The City will encourage high quality design within North Shore commercial and industrial areas by designating the North Shore as a Development Permit Area and by providing development permit guidelines that respect building form, character and height and that address opportunities for landscaping and green design standards.
6. The City will encourage new residential development that is pedestrian and cycle friendly by discouraging cul-de-sacs and by discouraging the visual prominence of garages by supporting reductions in front yard setbacks for the main front wall of a house to help recess the garage away from the street front.

(cont...)

# General Policies

## 3.3 COMMUNITY FORM AND IDENTITY (cont.)

### POLICY STATEMENTS: (cont)

7. The City will support and encourage quality residential neighbourhoods which provide for safe and convenient pedestrian and cycle movements and convenient access to community facilities and commercial areas.
8. The City will support the protection of existing, stable neighbourhoods from incompatible adjacent land uses, inappropriate infill or inappropriate redevelopment projects.
9. The City will support increases in densities, throughout the North Shore, but particularly along arterial roads and public transportation corridors, to promote a more compact, sustainable and pedestrian-friendly community. Projects for increased density in single and two-family neighbourhoods shall be supported where buildings specifically take into consideration appropriate designs that allow for sensitive integration into the existing form and character of the North Shore neighbourhood.
10. The City will support the use of Community Gardens, both on public and private property, throughout all residential neighbourhoods on the North Shore, provided that parking can be accommodated on site and that the garden is sensitively integrated into the existing community.

Poor integration of new development into an established neighbourhood;  
Photo: Carla Stewart



Poor integration of new development into an established neighbourhood;  
Photo: Carla Stewart



Excellent integration of duplex and new development into existing single family style neighbourhoods  
Photo: Carla Stewart



New duplex development with a single family design integrates well into the existing neighbourhood;  
Photo: Carla Stewart



### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

### ACTION ITEMS:

- a. Establish a "Neighbourhood Pride" program, in conjunction with stakeholders and businesses, to foster a sense of neighbourhood identity and pride and to conduct programs such as Neighbourhood Watch, Adopt-a-Street and other community activities.
- b. Partner with the Kamloops Food Policy Council to explore developing Community Gardens on the North Shore.

# General Policies

## 3.3 COMMUNITY FORM AND IDENTITY (cont.)

Creating safe, inviting, attractive and functional spaces in existing neighbourhoods and commercial areas is a key component to supporting a mature neighbourhood and a healthy community. The illustrations shown here are examples of the type of community form and identity that is supported by the policies in this Plan including the use of rear alleys for accommodating additional, smaller residential units and including designing higher density residential developments so as to connect to a pedestrian form and scale and to provide natural surveillance opportunities to adjacent public spaces.

Before

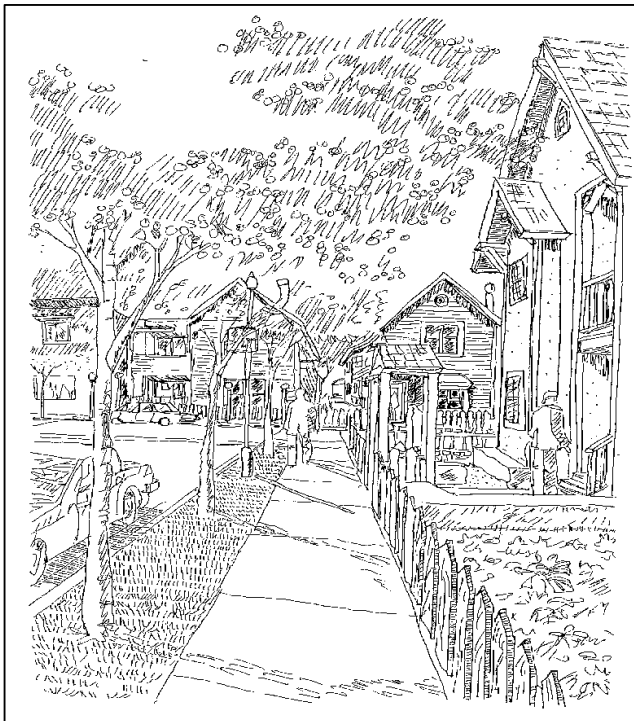


Brock Shopping Centre;  
Photo: ND Lea

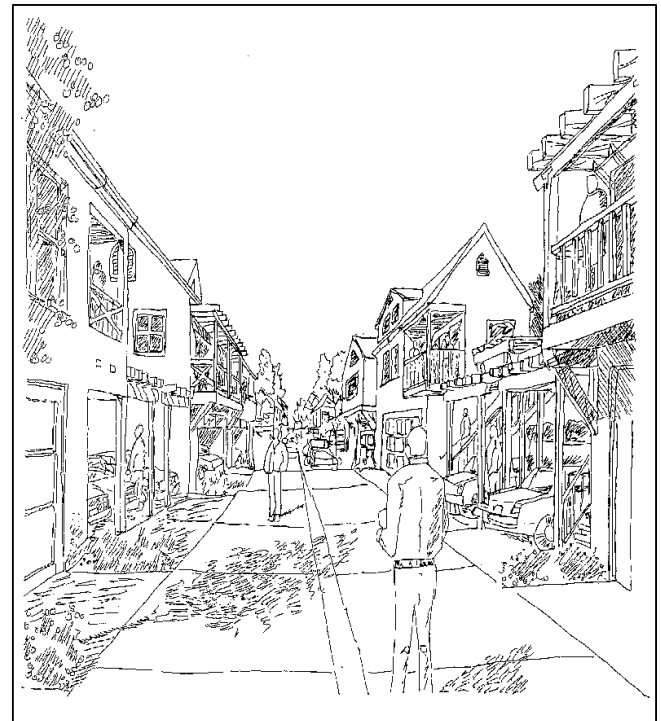
After



Pedestrian-focused, traffic-calming development infill  
example at Brock Shopping Centre;  
Source: ND Lea



Increased density development designed to connect directly to the street for natural surveillance opportunities;  
Original Image Source: *The Next American Metropolis*  
Drawing: Carla Stewart



Rear alley showing buildings constructed with garden suites;  
Original Image Source: *The Next American Metropolis*  
Drawing: Carla Stewart



# General Policies

## 3.4 PUBLIC SAFETY

All communities, whether in a rural or urban setting, experience issues related to crime and public safety. These issues are heightened and increase in frequency where there are greater concentrations of people and where there are more opportunities to take advantage of dense living environments.

Certain areas of the North Shore are experiencing these heightened levels of crime and public safety issues due to the increase in the use of illicit drugs in today's society and the corresponding property crime that is associated with supporting drug use. In order to create an environment where people of all ages feel comfortable, welcome and safe, crime must be addressed at the most basic level: the physical environment. Unfortunately, many developments are inadvertently designed to attract and encourage crime, creating spaces in communities influenced by fear and a sense of helplessness. Proven in many communities throughout North America, the manipulation of the physical environment can provide behavioural effects that will reduce the incidence and fear of crime, thereby improving quality of life.

A well-utilized and highly successful program has been implemented extensively throughout North America as an effective way to prevent crime opportunities. This program is known as CPTED: Crime Prevention Through Environmental Design (pronounced SEPTED). CPTED is a multi-disciplinary approach to crime prevention utilizing police knowledge, municipal staff services, architectural expertise and developers and community groups desiring to build within a neighbourhood. CPTED focuses on examining an environment from a cohesive point of view whereby both the physical and social conditions are examined, on-site and surrounding the site, to identify the influences that create crime targets and crime opportunities. Including CPTED principles at the design stage of development is the key to success and the key to eliminating costly retrofits, or "hard targeting" to an existing building. In order to deal with the identified crime issue, both real and perceived, on the North Shore, CPTED principles will need to become a key component to any and all site and building design.



Semi-public space; creates distinctions and hierarchy of uses; Source: The Planning Centre *Center Views Newsletter*



Uncluttered store windows allow for natural surveillance; Source: The Planning Centre *Center Views Newsletter*



Natural surveillance blocked by flyers and posters; Source: The Planning Centre *Center Views Newsletter*



Semi-private space; creates distinctions and hierarchy of uses; Source: Henrico County



Glass allows for natural surveillance in and out of building; Source: Unknown Internet Site



Dark passageway and low visibility to the other side; Source: Unknown Internet Site

# General Policies

## 3.4 PUBLIC SAFETY (cont.)

### **POLICY STATEMENTS:**

1. The City will require all new development to undertake a CPTED building and site design review.
2. The City will support a proactive approach to reduce and eliminate crime and nuisance behaviour through the manipulation of the built environment before projects are constructed.
3. The City will proactively plan and design safer buildings, facilities and parks using crime prevention methods.
4. The City will foster an enhanced image for the North Shore and Kamloops by incorporating crime prevention principles into all development.
5. The City will encourage the reduction of crime and the reduction of the perception of crime by requiring the implementation of CPTED principles at the concept and design stage of a development.
6. The City will encourage social service agencies to locate in areas directly accessible, either through public transportation, cycle or pedestrian route, by those agency's clientele.
7. The City will support conducting a feasibility study to examine the North Shore Towne Centre, Tranquille Commercial District and Tranquille South public lighting systems and will support identifying improvements for increases in pedestrian visibility and safety.

### **ACTION STATEMENTS:**

- a. Incorporate a CPTED review into the City's development application review process.
- b. Incorporate RCMP input into the City's development application review process.
- c. Conduct a public lighting system feasibility study for the North Shore Towne Centre, Tranquille Commercial District and Tranquille South area to identify improvements and implementation of upgrades.
- d. Develop programs to help educate City Council, City staff, architects, developers and designers on CPTED and/or crime prevention principles.

### **SUPPORTING REGULATIONS:**

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines



# General Policies

## 3.5 ACCESSIBILITY

Healthy communities are those that are accessible for all age groups and for those with physical disabilities. As identified in the public opinion survey, one of the best features of the North Shore is its flat topography. This lack of steep and hilly terrain provides an opportunity for seniors, children and persons with disabilities to travel independently, interacting with their surroundings without the use of a vehicle or without assistance from others.

Gaps in the current infrastructure however, do exist. These range from a lack of sidewalks and a lack of safe pedestrian access routes, to outdated and incomplete infrastructure systems that make pedestrian travel challenging and prohibitive. Stakeholder feedback identified these infrastructure gaps as reasons why accessibility is severely restricted in some areas thereby creating an environment that is exclusive rather than safe and inviting.



Physical barriers to accessibility; Photo: Carla Stewart



Accessible neighbourhood pathway/greenway; Photo: Carla Stewart



Lack of safe and appropriate pathways; Photo: Carla Stewart

### POLICY STATEMENTS:

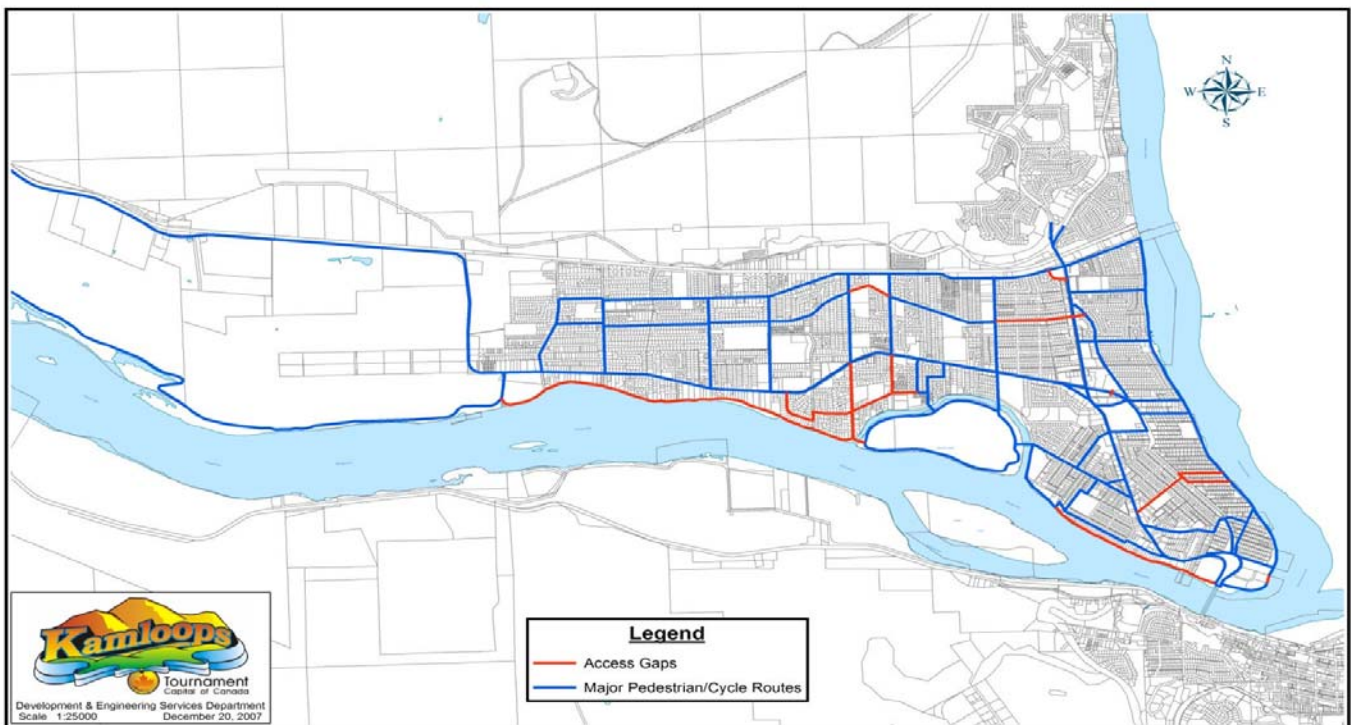
1. The City will support the creation and retention of greenways and pathways that connect neighbourhoods and allow for convenient and alternative ways of travelling that are accessible for pedestrians, cyclists and the disabled.
2. The City will support creating and maintaining a greenway or pathway system throughout the North Shore to connect all residential neighbourhoods and to create designated cycle commuter routes.
3. The City will encourage the creation of a barrier-free environment, including the design and layout of buildings, site development and municipal infrastructure.
4. The City will encourage all new buildings to be accessible to persons with disabilities in accordance with the provisions of the BC Building Code and any such amendments.
5. The City will encourage builders to exceed the minimum standards of accessibility set out in the BC Building Code when designing and constructing new buildings.
6. The City will encourage density transfers, increases in building height and reductions in setbacks and parking requirements when a minimum of 20 per cent accessible residential units are provided for in a multiple-family residential or mixed-use development.

# General Policies

## 3.5 ACCESSIBILITY (cont.)

Gaps in infrastructure on the North Shore are shown on the Accessibility Gaps Map ( see below) in red. In some instances, informal passageways may exist, in others there are physical barriers and/or the presence of private property that prevent access by pedestrians or cyclists. Working toward eliminating these barriers would increase accessibility for all residents and commuters of the North Shore.

Map 16: Accessibility Gaps



### ACTION ITEMS:

- Prepare a priorities list of infrastructure upgrades to work toward improving accessibility between existing neighbourhoods and between residential and commercial areas along major pedestrian traffic routes.
- Develop density transfer opportunities for residential and mixed-use developments within the Tranquille Commercial District and North Shore Towne Centre.

### SUPPORTING REGULATIONS:

- North Shore Zoning Code
- North Shore Development Permit Guidelines

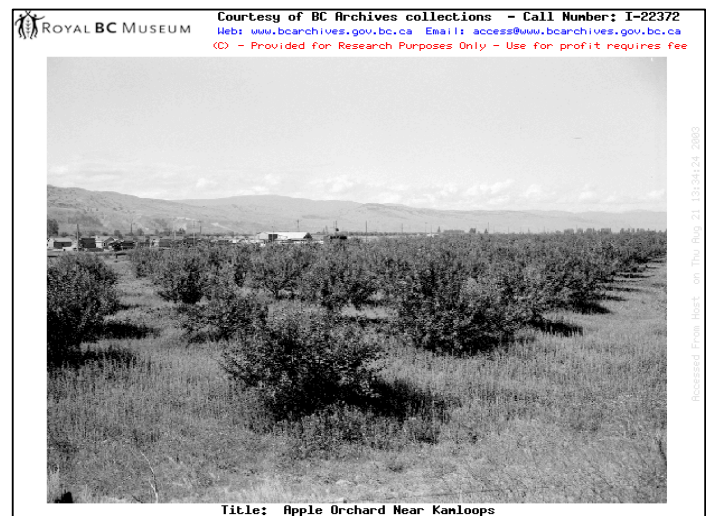
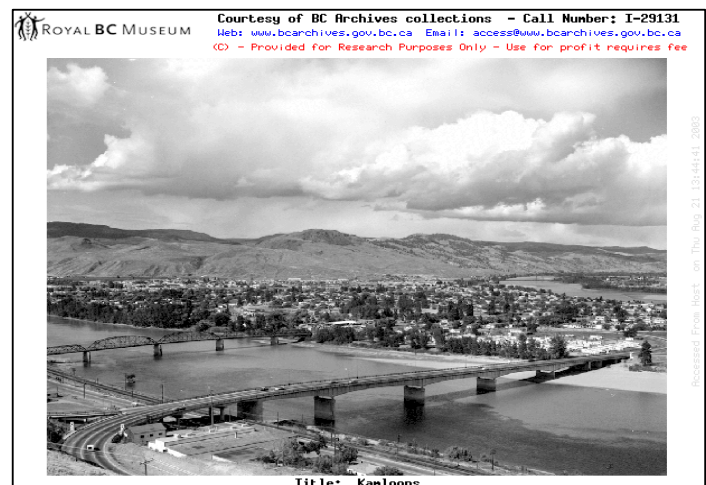
# General Policies

## 3.6 HERITAGE CONSERVATION

Heritage resources comprise man-made, land use and landscaped features that are indicative of past human activities, events and/or achievements. Such resources include houses, barns, social halls, theatres, churches, ceremonial sites, areas of land, bridges, orchards, roadways, monuments, residential areas, irrigation systems, farms and parks to name a few.

Supporting the retention and recognition of the North Shore's built and natural heritage assets in order to preserve a sense of community identity and to support continuity between the past and the present is essential for a vibrant, progressive and healthy community. As part of the public consultation phase of the preparation of this Plan, the residents and businesses of the North Shore were asked whether heritage conservation was important to pursue as part of this Plan. A majority of those surveyed agreed that preserving the North Shore's heritage and past was important for the community and that more should be done to ensure that long-term protection of heritage resources is made a priority.

Conserving historical features, buildings, blocks and neighbourhoods is also an important aspect in maintaining community pride by retaining and celebrating a physical reminder of a community's history. The North Shore boasts many natural historical resources and four areas that were identified as potential Heritage Conservation Areas: the McDonald Park Neighbourhood, the John Tod Neighbourhood, portions of the Schubert Drive Neighbourhood and the North Kamloops West Neighbourhood. The North Shore also has many significant individual heritage resources that should be retained and celebrated, the most recent of which is the Wilson House which provided an opportunity for re-use, rehabilitation and community re-investment and spirit.



Photos: BC Archives



# General Policies

## 3.6 HERITAGE CONSERVATION (cont.)

### POLICY STATEMENTS:

1. The City will seek to ensure that heritage resources are maintained and enhanced within a compatible context. The City will address this objective as part of its consideration of any application for development approval that affects a property, or adjoining property, occupied by a heritage resource.
2. The City will encourage public works, both municipal or provincial, to be undertaken in a sensitive manner with regard to work being done to heritage resources.
3. The City will require approval of City Council, in consultation with the Kamloops Heritage Commission, for the alteration or demolition of a designated heritage building, a property listed on the City of Kamloops heritage register or a building or heritage resource located in a designated heritage conservation district.
4. The City will strive to maintain any heritage assets which it owns to as high a standard as possible in order to demonstrate the City's commitment to heritage preservation.
5. The City will encourage the use of a heritage conservation covenant to ensure the retention of heritage features and structures, as a condition of approval of a proposed development involving a heritage resource.
6. The City will support an inventory of heritage resources on the North Shore in conjunction with consultation from the Kamloops Heritage Commission. This inventory will foster awareness of the North Shore's heritage resources to residents, business owners and visitors.

(cont...)



Historic Wilson House; Photo: Carla Stewart



Historic View of Mount Peter and Mount Paul; Photo: Carla Stewart



The Tom Bones House; Photo: Dan Wallace

# General Policies

## 3.6 HERITAGE CONSERVATION (cont.)

### **POLICY STATEMENTS:** (cont.)

7. The City will analyze the potential for and support where appropriate, the creation of Heritage Conservation Areas in the John Tod, McDonald Park, North Kamloops West and Schubert Drive Neighbourhoods.
8. The City will support the enhancement and upgrading of the Tranquille Commercial District to reflect and celebrate the historical status of the Township and Village of North Kamloops.
9. The City will support integrating the North Shore's heritage and cultural diversity into the development of park sites and recreation facilities, as appropriate.

### **ACTION ITEMS:**

- a. Include heritage conservation policies for the North Shore within the City of Kamloops Heritage Strategic Management Plan.
- b. Engage in a planning process to create, where appropriate, Heritage Conservation Areas for the McDonald Park, John Tod, North Kamloops West and Schubert Drive Neighbourhoods.

### **SUPPORTING REGULATIONS:**

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines
- c. Kamloops Heritage Tax Incentive Program
- d. Kamloops Cultural Strategic Plan



North Shore Pagoda  
Photo: Thompson Rivers University



Demolished North Shore Pagoda  
Photo: Carla Stewart



# General Policies

## 3.7 ALTERNATIVE DEVELOPMENT STANDARDS

Many communities across British Columbia and Canada were developed with the use of environmentally friendly street designs and infrastructure systems. Using green infrastructure before it was known as "green infrastructure", many neighbourhoods were built with the use of ditches, wetlands, swales, gravel driveways and boulevards, rock pits and smaller building footprints contributing to a healthy and sustainable living environment. With the advent of engineered roadways and utility systems, combined with an increase in the use and dependence of the automobile, many new neighbourhoods have been constructed with a new norm of wide, paved surfaces and curb, gutter and sidewalk systems to replace the gravel boulevards.

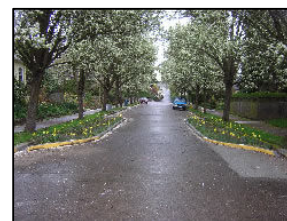
Brocklehurst is a classic example of a neighbourhood that was originally constructed with sustainable and environmentally friendly infrastructure and roads. Although not a perfect system, the boulevard and utility infrastructure on the North Shore is a celebrated first step toward creating truly sustainable and healthy neighbourhoods. This section will provide suggestions to improve the current system in place and to create areas that are safer for pedestrians, that serve as an example for other neighbourhoods of the city to emulate when making decisions on how to become more environmentally friendly and to ultimately contribute to a reduced environmental footprint by creating urban areas that have positive, rather than negative effects on global systems.

### POLICY STATEMENTS:

1. The City will support the use of any Provincial local government infrastructure grant programs to implement green street standards for the North Shore.
2. The City will support connecting public and private natural areas to create a green infrastructure system that supports water infiltration, wildlife habitat and healthy communities.
3. The City will support creating a healthier river system by encouraging green stormwater systems and green street designs.
4. The City will support the use of sustainable infrastructure systems for both residential and commercial development.
5. The City will support commercial and multiple family developments using green infrastructure and stormwater systems as part of the required amenity and landscaping areas.
6. The City will recognize that streets are the most accessible public spaces and will support green streets and pedestrian friendly systems as the guiding design used for neighbourhood residential streets.



Green wall along roadway;  
Source: Unknown Internet Site



Street modified to create green areas;  
Source: Unknown Internet Site



Inner-city Green roof system;  
Source: Unknown Internet Site



On-site stormwater detention;  
Source: Unknown Internet Site



Rain water collection;  
Source: Unknown Internet Site



On-site stormwater system;  
Source: Unknown Internet Site

(cont...)

# General Policies

## 3.7 ALTERNATIVE DEVELOPMENT STANDARDS (cont.)

### POLICY STATEMENTS: (cont.)

7. The City will examine the use of a Clean River Rewards program that helps to reduce stormwater discharge and creates cleaner water, healthy watersheds and livable communities by examining the feasibility of offering utility discounts to those who manage stormwater on site, thereby reducing infrastructure and maintenance costs for the City of Kamloops.
8. The City will support water conservation programs for residential properties as part of the City's EcoSmart program (e.g. the use of rain barrels, water meters and xeriscaping).
9. The City will support and encourage leading edge, environmentally friendly, cost-effective stormwater and development standard installations for the North Shore as a leader in sustainable development practices.
10. The City will encourage sustainable forms of site development that use on-site stormwater treatments, on-site recycling and composting and grey water systems.

### ACTION ITEMS:

- a. Investigate establishing an 'Adopt-a-Trail' Program to engage community involvement in the development and maintenance of current and future trails.
- b. Create a Green Infrastructure Best Management Practices Guide for the North Shore providing specifics on green streets designs, eco-stormwater systems and green roof systems.
- c. Develop a program to help landowners retrofit existing buildings to become sustainable by using green infrastructure systems.
- d. Examine the feasibility of preparing an integrated stormwater management system that takes into account all sustainable forms of infrastructure development, establishing a connected and comprehensive system for the North Shore.



Green Street Design Standards; Source: Unknown internet site

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines



Green Stormwater Infrastructure Treatments and Green Roof; Source: Unknown Internet Site



# General Policies

## 3.7 ALTERNATIVE DEVELOPMENT STANDARDS (cont.)

Many examples of green streets, green stormwater systems and green roof designs are available throughout British Columbia, Canada, the United States and Europe. Those systems were implemented and installed after conducting technical reviews and developing an understanding of all the issues that are specific to an individual community. In the case of the North Shore, a technical advisory committee for Green Streets was formed to first identify the operational issues that would need to be addressed if green street standards were implemented and second, to establish design parameters for the North Shore. Criteria for new development and for retrofitting existing streets was established by this committee as a start to supporting the policies in this Plan, with the understanding that the criteria and design specifications would need to be refined and adjusted at a later date. The designs shown here are examples of what a green street on the North Shore could look like.



Before and after images of a typical street in Brocklehurst; Source: ND Lea Consultants Ltd.



# General Policies

## 3.8 ENVIRONMENTALLY SENSITIVE AREAS

### 3.8.1 Flood Plain Areas

The North Shore is situated on the banks of two primary water systems: the North Thompson River and the Thompson River. Extensive access to these two rivers is available from the Schubert Drive and McDonald Park Neighbourhoods, along the airport and agricultural lands, and off of McArthur Island. These two rivers, and their active riparian areas, are currently protected from potentially harmful development through the provincial Riparian Area Regulations, the subsequent City of Kamloops Riparian Development Permit Area in the Official Community Plan and by statutes established by Fisheries and Oceans Canada.

Protection of human habitat from flood waters is also a concern along both riverbanks and typically requires human intervention every 20 and 200 years. A majority of both the Schubert Drive and McDonald Park Neighbourhoods are within the 20 year flood plain level and at least one third of all of the North Shore is located within the 200 year flood plain. At times of flooding, both from overland water and from seepage underground during extended periods of high water levels, protecting the significant infrastructure systems of the North Shore requires a considerable effort and places many people at risk from losing habitable space in the form of basement suites or homes that were built below the flood plain level.

Most of the areas of the North Shore were built prior to the implementation of any development restrictions preventing habitable space from being constructed within a flood plain level. New development is currently required to meet an established flood plain level but there are hundreds of existing dwellings that have habitable space, particularly basement suites, within flood prone areas. In an attempt to provide safe, affordable and adequate housing alternatives for all residents of the North Shore, the use of basement suites below established flood plain levels are not supported, even where the density is permitted through zoning. In areas where basement suites fall within a designated flood plain area, the use of a garden suite, in conjunction with a garage and where lane access is available, is another form of housing that is acceptable to use.

#### POLICY STATEMENTS:

1. The City will support alternative forms of housing in flood prone areas by permitting garden suites above garages and the conversion of garages into one residential unit, where those garages and garden suites are adjacent to a rear alley and where the garden suite is not subdivided into a separate legal parcel.
2. The City will not support the locating of secondary suites below the 200 year flood plain level.
3. The City will support restricting residential dwelling types on lands that are impacted by flooding.
4. The City will encourage property owners to naturalize those portions of their yards that abut environmentally sensitive areas, including riverbanks and sloughs, as approved through a Riparian Area Assessment.

#### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines



North Thompson River;  
Photo: Carla Stewart



McArthur Island Slough;  
Photo: Carla Stewart



# General Policies

## 3.8 ENVIRONMENTALLY SENSITIVE AREAS (cont.)

### 3.8.2 Grasslands

Grasslands are one of the most significant and threatened ecosystems found within British Columbia, particularly in the Interior of the province where Kamloops plays a vital role in its preservation. Easy to develop and less "valued" compared to West Coast old growth forests, grassland ecosystems and their continued longevity are critical to the health and sustainability of Kamloops' ecosystem. Within the North Shore Neighbourhood Plan project area lays substantial areas of grasslands, notably the Lac Du Bois grasslands to the north of Ord Road, the Airport Lands and the Agricultural Lands west of the airport. Portions of these lands also fall within the Lac Du Bois Provincial Park system with the rest owned by the Crown or private individuals.

The grasslands areas west of Batchelor Heights potentially represent a significant future development area for the city as a whole. TravelSmart, a land use and transportation management document completed in 1999, examined Kamloops' growth areas and the corresponding capital improvements that would be required to accommodate growth up to the 120,000 population mark. TravelSmart did not however, identify the Lac Du Bois area as a future growth site for Kamloops within that 120,000 population calculation. Therefore, from a growth management perspective, development of the Lac Du Bois grasslands has only been seen as a very long-term option in the future and evolution of Kamloops. However, based on the background information prepared as part of the North Shore Neighbourhood Plan, and on preliminary estimates from the Traffic and Transportation Section, it has been identified that the preferred land uses and rate of growth estimated and identified in TravelSmart have now been exceeded and that the TravelSmart model is required to be updated to be consistent with Kamloops' existing conditions, demands and service levels.

The updating of TravelSmart is expected to take place in 2008 and will need to examine the Lac Du Bois grasslands as a development area and to identify the balances of infrastructure costs (bridges, new roads, expanded water and sewer services) against the potential need to create a new residential area. Additionally, identifying development practices, designs and layouts are now critical for the protection of the grasslands and for integrating development into this ecosystem in a sustainable and appropriate manner. Comprehensive site planning for the grasslands area will be required prior to any development approvals. As an example, a preferred development scenario is included in this section and was conducted using an ecological/urban design analysis model (see page 58).



Grasslands serve as natural recreation and hiking areas;  
Photo: Carla Stewart



Grasslands with high development pressures;  
Photo: Carla Stewart



Development encroaching into grassland areas;  
Photo: Carla Stewart



# General Policies

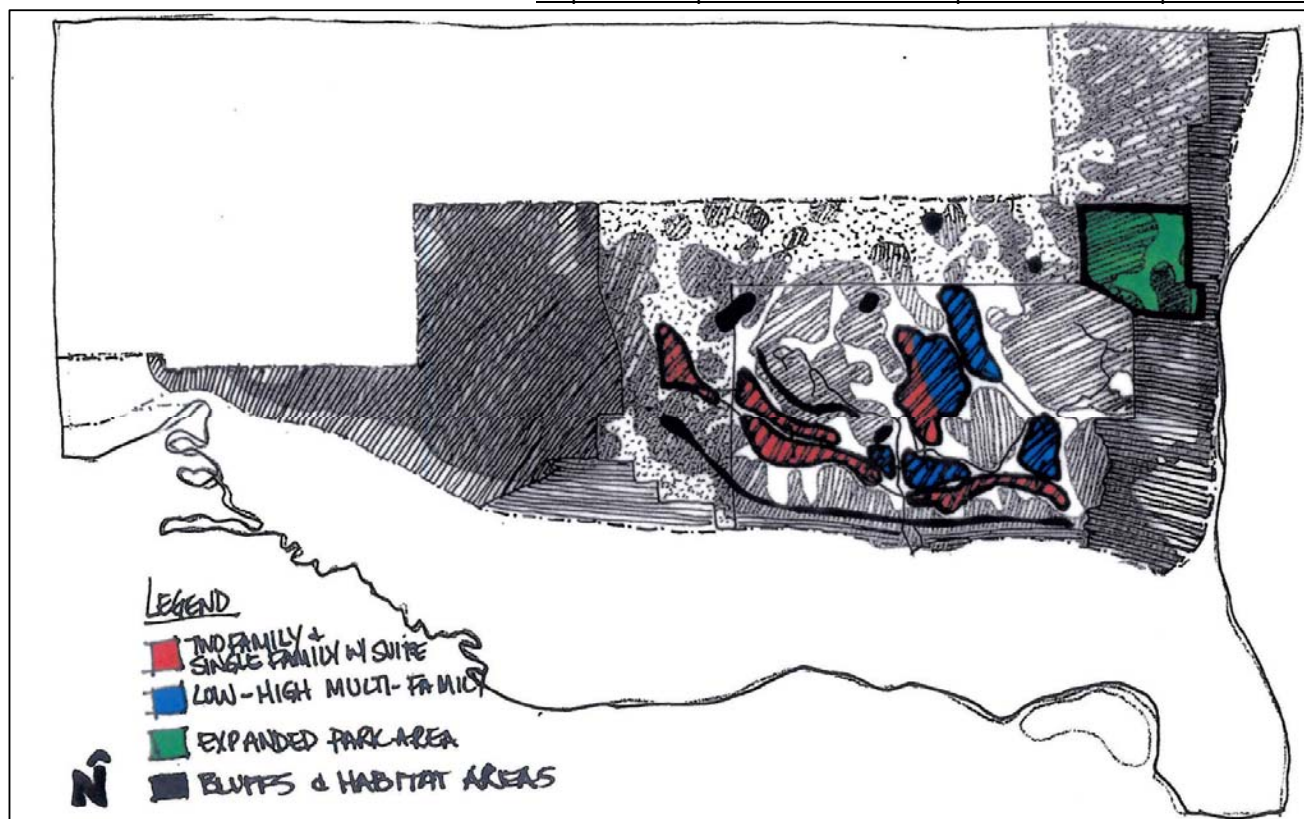
## 3.8 ENVIRONMENTALLY SENSITIVE AREAS (cont.)

### 3.8.2 Grasslands (cont.)

The Lac Du Bois grasslands are a unique area of Kamloops. Development of this area is not a guarantee but if it were to take place, it would require specialized forms of development layouts and building and site design. A comprehensive planning and development review would be required prior to any development taking place which would need to address the following: recommendations made in the Grasslands Conservation Council environmental assessment of the area; any TravelSmart details regarding necessary capital works expansions; and the inclusion, construction and implementation of an integrated water and sewer system that is sustainable and that functions within capacities. If new development were to take place, an example of the type of restrictions intended for this area and how they can influence development layout is provided below:

- That the grasslands ecosystem, even throughout developed areas, remains physically connected;
- That the major mammal, bird and reptile nesting areas and dens are protected, preserved and/or retained;
- That existing paths and significant recreational areas are not destroyed or compromised;
- That wetland areas and the natural flows that feed the wetlands are retained and protected;
- That development take place in a clustered format, encouraging multi-family densities and prohibiting single family densities;
- That underground water streams and sources not be negatively impacted and that wetland feeds are not cut off by new development;
- That existing wetlands are used and integrated into stormwater drainage systems; and
- That native plant materials are used at appropriate elevations to ensure growth success and that widespread use of turf and or lawn areas are prohibited to help reduce the need for large amounts of irrigation water.

Map 17: Example of Lac Du Bois Comprehensive Development Scenario



# General Policies

## 3.8 ENVIRONMENTALLY SENSITIVE AREAS (cont.)

### 3.8.2 Grasslands (cont.)



Grasslands are natural amenities that also serve as viewsapes;  
Photo: Carla Stewart

#### **ACTION ITEMS:**

- a. Examine creating a stewardship program and trail connectivity strategy with BC Parks and other appropriate Ministries, for the Lac Du Bois grasslands, to promote the protection, maintenance and enhancement of the existing grasslands ecosystem.
- b. Prepare a comprehensive development strategy for the Lac Du Bois grasslands north of Ord Road.

#### **SUPPORTING REGULATIONS:**

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

#### **POLICY STATEMENTS:**

1. The City will support infill development in existing neighbourhoods prior to extending new development into undeveloped and unzoned portions of the Lac Du Bois grasslands.
2. The City will support the federal Greencover Canada initiative which is aimed at agricultural producers improving grasslands management practices.
3. The City will support protecting the view amenity of the Lac Du Bois grasslands for areas of the North Shore by supporting sustainable development conducive to grasslands protection.
4. The City may only consider or explore development into the Lac Du Bois grasslands area after evaluating and approving a comprehensive development plan that addresses the following:
  - how development is consistent with and supportive of the growth management policies in TravelSmart;
  - how the existing grasslands ecosystem is going to be protected, maintained and enhanced with development taking place;
  - zoning densities that are a minimum of Single Family with Suite;
  - how residential development will be clustered to ensure the integrity, function and health of the grasslands ecosystem;
  - how new development will incorporate alternative site and construction standards including clustered residential designs, clustered parking areas, alternative stormwater systems and green streets designs; and
  - how and what infrastructure in and adjacent to the area will be addressed and funded, including that for drainage, sewer, roads, water and transportation systems.

# General Policies

## 3.9 URBAN DESIGN AND DEVELOPMENT

City streetscapes, neighbourhoods, parks and shopping areas are places for people to interact, move around and carry on their daily lives in work and play. To make these urban spaces places where people of all types and ages want to be, urban design is essential to creating dynamic, sustainable neighbourhoods. Urban design is more than making an attractive building, it is about developing a complete understanding of the context within which that new development is entering into and establishing a use and building style that adds value to its surroundings rather than causing hardship or negative outcomes.

Urban design strives to create places based on the strength of the existing character or identity of a place, create continuity for ease of movement, establish quality public spaces, create cities that have the ability to adapt easily over time and provide choice and variety. These objectives can be achieved by incorporating the ten principles of urban design into development:

- determine the context of the area;
- save and celebrate the place;
- recognize the natural features as critical form makers;
- fit the design to the scale and location of the area;
- accommodate the pedestrian first and the vehicle last in all movement systems;
- include multiple, flex and mixed uses as keystones to sustainability;
- plan for diversity;
- incorporate the public realm as a central component into the design;
- integrate community-building as part of the design process and not at the end; and
- preserve urban centres.

All areas of the North Shore, as part of the preparation of the North Shore Neighbourhood Plan, underwent an urban design analysis, including the intensive analysis of the Simcoe Avenue Area (see Section 4.4.1). From this understanding of existing conditions, better decisions can be made that not only achieve the urban design objectives listed above, but that particularly achieve the vision, goals and principles supported by the North Shore residents and business owners as the preferred future for their community.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

### POLICY STATEMENTS:

1. The City will support the use of urban design analysis and principles in the review of development proposals within existing neighbourhoods to help reduce the negative impact that any infill development has the potential to create.
2. The City will support incorporating a Development Checklist (see Appendix "B") for all development applications to support creating sustainable, livable, high-quality developments within existing and new neighbourhoods.
3. The City will support the continued redevelopment of existing commercial areas in a manner that will not result in any significant negative impacts to the community, will not result in significant traffic impacts, will not utilize excessive sewage and water servicing capacities, will maintain and enhance existing urban design, and that will improve external and internal access, including roads and pedestrian and cycle paths, where feasible.
4. The City will support a high quality of urban design in commercial and infill areas that helps connect new development to an existing, mature community and that encourages appropriate development balances that serve the needs of that community.
5. The City will support the mitigation of the visual impacts of utility and mechanical installations through appropriate screening and locational criteria.
6. The City will support the use of berming and sound attenuation systems, for new subdivisions, that are consistent with the neighbourhood, between residential areas and highways and/or arterial roadways.

### ACTION ITEMS:

- a. Incorporate the Development Checklist into the processing of development applications.



# General Policies

## 3.10 NON-MARKET HOUSING

Segments of Kamloops' population do not earn enough money to pay for their housing needs. Whether it be seniors living off their pension, disabled persons receiving a living allowance, single parents with an inability to work, or full-time employees working in low-paying jobs, their economic reality is such that they are unable to pay for market-driven housing, pay their bills and buy food on the income they earn. In addition, the financial responsibilities associated with home ownership are often difficult for low-income families to assume, therefore rental housing is the only form of housing within their reach. The inflated real estate market experienced across British Columbia is compounding the problem making it harder for people to house themselves without assistance.

Providing fixed-income housing opportunities in existing neighbourhoods is challenging and often met with resistance. Those living in a conventional single family home often view low-income persons as being less stable and attractors of criminal activity. Ensuring that the housing needs of a neighbourhood are met is the first step to supporting a healthy and vibrant community that is inclusive, diverse, accepting and sustainable. Non-market housing exists throughout the whole of the North Shore and by supporting fixed-income developments, the North Shore can continue to be a model for how neighbourhoods should be and move away from NIMBY (Not In My Back Yard) to YIMBY (Yes! In My Back Yard).

The North Shore is a neighbourhood that supports and offers housing units for all types of residents from a professional, single parent, senior, persons with disability to a large family. The North Shore Background Report also identified that there is a high demand and need for more non-market housing. The high demand for non-market housing on the North Shore is directly associated with why the North Shore is a desirable place to live for all income and family types: affordable, close to shopping and medical and personal services, access to public transit, access to open space and recreation facilities, flat, accessible land and long summers and short winters.



Non-market housing on the North Shore; Photos: Lenore Mitchell

### POLICY STATEMENTS:

1. The City will encourage the continuation of the Housing Reserve Fund and will evaluate requirements for new commercial development to contribute to the Fund to provide opportunities for housing.
2. The City will support housing projects for single parent families, fixed income singles, seniors, persons with special needs and mid-income earners in all residential zones and within the North Shore Towne Centre and Tranquille Commercial District areas.
3. The City will support non-market housing projects that are concentrated along transit routes, and that are in close proximity to shopping and medical services.
4. The City will continue to support waiving Development Cost Charges for social housing units; making City-owned land available for fixed-income housing projects; supporting variances for innovative housing types; providing direct cash contributions where appropriate; or combining any or all of these tools to support the construction of non-market housing units.
5. The City will support the inclusion of non-market housing units into new market housing developments.

(cont...)

# General Policies

## 3.10 NON-MARKET HOUSING (cont.)

### POLICY STATEMENTS: (cont.)

6. The City will consider the potential impact on the loss of rental accommodation when contemplating whether or not to support the conversion of multiple family development or duplexes to strata title ownership.
7. The City will support the use of tri-plexes and four-plexes in existing duplexes where the additional suites are used for non-market rental housing at an affordable rate and where the property owner enters into a Housing Agreement with the City of Kamloops agreeing to provide rental accommodation at below market rates.
8. The City will support providing development incentives to create non-market housing opportunities. These incentives can include, but are not limited to, supporting parking reductions, setback variances, reductions in Development Cost Charges, providing financial assistance by using the Housing Reserve Fund, using profits from the sale of surplus land, purchasing units directly from developers and/or refunding development application fees.
9. The City will support charging Development Cost Charges on a square foot basis, rather than on the number of units, to encourage the construction of smaller, more affordable units.
10. The City will support entering into Housing Agreements with developers and landowners to ensure that rental rates are kept affordable for a minimum of five years for newly constructed non-market housing units in a market driven multiple family development.
11. The City will support built-in bonuses and variances into the North Shore Zoning Code to encourage the construction of non-market housing units thereby reducing time and costs associated with new construction.



Non-market housing; Photo: Lenore Mitchell

### ACTION ITEMS:

- a. Examine the use of the Housing Reserve Fund to directly support and encourage the construction of non-market housing units.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines



# General Policies

## 3.11 SUSTAINABILITY

According to the International Institute for Sustainable Development and the Brundtland Report, sustainable development is "development that meets the needs of the present without compromising the ability of future generations to meet their own needs." At the core of sustainability therefore, is the need to focus on the world as a cohesive unit or system and recognize that everything we do, regardless of how small, has an impact on the global system we all live in and share.

Locally, communities can begin to take steps toward supporting sustainability, particularly in the design and function of urban environments. Comprehensive community development, infill development, using Transit Oriented Development techniques, relying more on public transportation, supporting mixed-use developments and incorporating sustainable building forms and site layouts are all things a community can do and are all supported within the North Shore Neighbourhood Plan.

Like urban design principles, sustainability principles examine creating balances that take into consideration social, environmental, economic and cultural interests and ensure that government and infrastructure are on line and are appropriate to carry out the implementation and practice of sustainable development.

The North Shore is a mature community with very little vacant land. One area of undeveloped residential land exists adjacent to the airport which is currently designated for single family densities. The preparation of the North Shore Neighbourhood Plan has created an opportunity to keep sustainability in mind and ensure that development of this area is carried out appropriately with opportunities for connective road patterns, increased and efficient densities and pockets of multiple family and non-market housing.

For those areas of the North Shore that undergo redevelopment or infill development, ensuring that appropriate, attractive, high quality and sustainable construction takes place is critical to maintaining a healthy community and a healthy planet. This section provides and supports small and incremental approaches that can take place by the development community and municipal government to ensure that Kamloops and the North Shore is being creative and responsible in its efforts to form a sustainable urban environment.

### ACTION ITEMS:

- a. Incorporate the Development Checklist into the processing of development applications.
- b. Create a Sustainability Officer, to act as a coordinator between City departments, to ensure the principles and concepts of sustainable development are instituted in the Best Management Practices of the City of Kamloops.
- c. Create a 'Replace-the-Turf' program which offers financial and informational assistance to home owners wishing to remove turf to replace it with xeriscape landscaping treatments.
- d. Coordinate preparation of the proposed Greenways Plan, Sustainability Plan and the revision of the Parks Master Plan with the policies of the North Shore Neighbourhood Plan.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines



Green roof creating a sustainable environment;  
Source: Unknown internet site

# General Policies

## 3.11 SUSTAINABILITY (cont.)

### POLICY STATEMENTS:

1. The City will support the concept that sustainability is a long-term need requiring decision making that may not result in immediate gain.
2. The City will support minimizing travel and the use of vehicle dependency by supporting development to locate in areas that are highly accessible by public transit and cycle and pedestrian routes.
3. The City will support, where practical, developments that use sustainable building methods and locally sourced materials, including recycled materials, and developments that recycle and reuse waste materials after project completion.
4. The City will support development that minimizes the use of energy and resources both during and after construction, using energy efficient designs and materials and by appropriately siting and orienting buildings and the position of uses within buildings.
5. The City will encourage and support development that uses water conservation techniques such as water saving devices, rainwater harvesting and grey water recycling.
6. The City will support the use of a Development Checklist (see Appendix "B") to allow a proponent to demonstrate to City Council that the applicant has properly considered the impact of their proposal and has taken into account all relevant factors in design and placement of a development.
7. The City will support development that takes into consideration sustainable construction methods and practices including: recycling construction waste, re-using the existing topsoil removed from the site with the required landscaping areas and reusing a minimum of 50 per cent of a demolished building for recycling.
8. The City will support working toward mitigating climate change by examining new development proposals to ensure that they work to: protect and enhance the character and appearance of landscapes and natural heritage; protect and enhance biodiversity; protect and improve the quality of the water environment; minimize light pollution; appreciate and safeguard our resources by maximizing the re-use and recycling of materials; and protect and improve air quality.
9. The City will support the use of sustainable drainage systems, particularly green streets, eco-roofs and/or green stormwater systems which work to create biodiversity and aesthetic benefits in urban environments as well as work to reduce post-development run-off rates, protecting natural water systems and wildlife habitats.
10. The City will support using Transportation Demand Management Guidelines for site layout and parking standards to work toward promoting sustainable forms of transportation.
11. The City will support sustainable urban places through the use of a mix of housing styles and affordability levels by supporting redevelopment that, where appropriate, seeks to provide or address the displacement of residents from existing affordable market housing units.

(cont...)

# General Policies

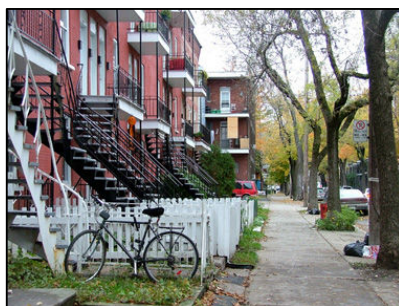
## 3.11 SUSTAINABILITY (cont.)

### POLICY STATEMENTS: (cont.)

12. The City will support the maintenance and enhancement of the environmental health of the North Thompson and Thompson Rivers by preserving the quality of surface and groundwater resources through natural stormwater drainage systems and the use of eco-friendly development standards.
13. The City will support commercial development that is constructed in a sustainable manner, with high-quality form and character for the building and the site, creating spaces that naturally attract people and that create spaces where the building and use become the destination sought by the user rather than the parking lot.
14. The City will support development that integrates and provides space for on-site composting and recycling.
15. The City will support new development that does not rely on extensive earthworks but rather that tailors site design and layout to the natural features of the area. Variances in setbacks for new construction will be supported where the reduced setback will help retain existing trees or other significant landscaped or natural features.
16. The City will support the use of appropriate sustainable building evaluation systems, including LEED Canada NC, the Canadian Green Building Council, Built Green, Go Green Plus, Green Globes and LEED BC as a means to promoting and encouraging green buildings and green developments on the North Shore.
17. The City will support fish and water fowl habitat enhancement in the McArthur Island slough.



Alternative Modes of Transportation;  
Photo: Carla Stewart



Designing urban areas that are pedestrian  
not auto oriented;  
Photo: Unknown Internet Site



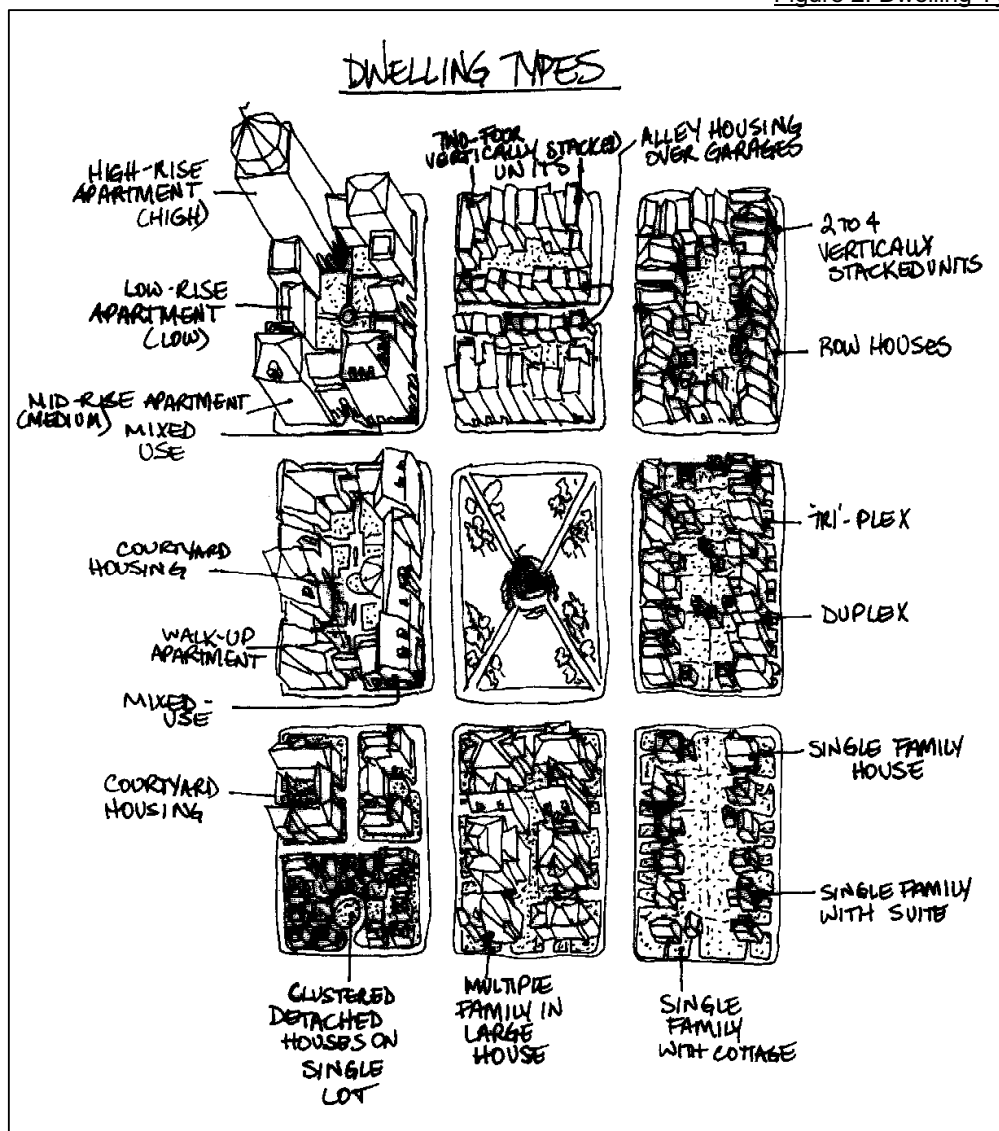
Encouraging and facilitating cycle use;  
Photo: Carla Stewart

# General Policies

## 3.11 SUSTAINABILITY (cont.)

The North Shore has excellent examples of sustainable neighbourhoods. Although improvements can be made in any community, the North Shore encompasses compact urban form, mixed use developments, multiple family, non-market housing, connected pedestrian and cycle trails, extensive public transit use, a lack of large areas of single family densities, production of locally grown fruits and vegetables, neighbourhood commercial sites, and central commercial districts that are well serviced by transportation methods other than the Single Occupancy Vehicle (SOV). As the image below illustrates, there are many different types of dwellings that house the residents of urban areas. From a sustainability point of view, the North Shore is already a step in the right direction by offering all but two of these dwelling types within its neighbourhoods: high-rise apartments and courtyard housing. As Kamloops continues to expand and new residents arrive that are looking for compact, efficient neighbourhoods, the North Shore will see new additions to its landscape that may in fact include the two missing housing types, creating a truly diverse, sustainable and progressive community.

Figure 2: Dwelling Types



Original Image Source: Jones, Tom, Pettus, William, and Pyatok, Michael, *Good Neighbours: Affordable Family Housing*, New York: 1995; Drawing: Carla Stewart



# General Policies

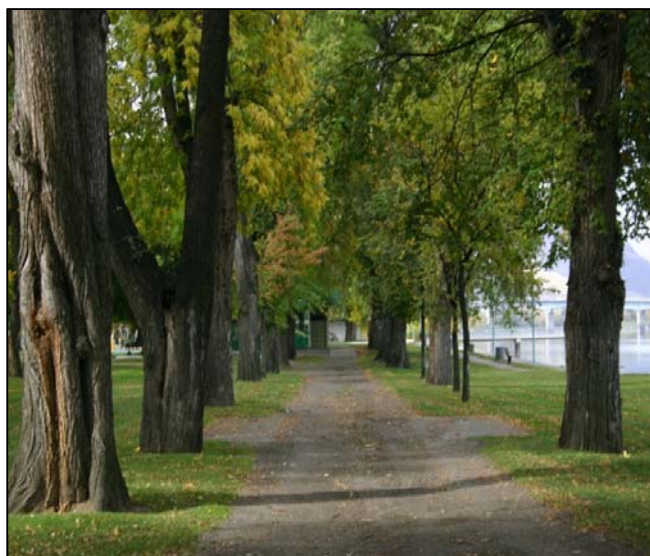
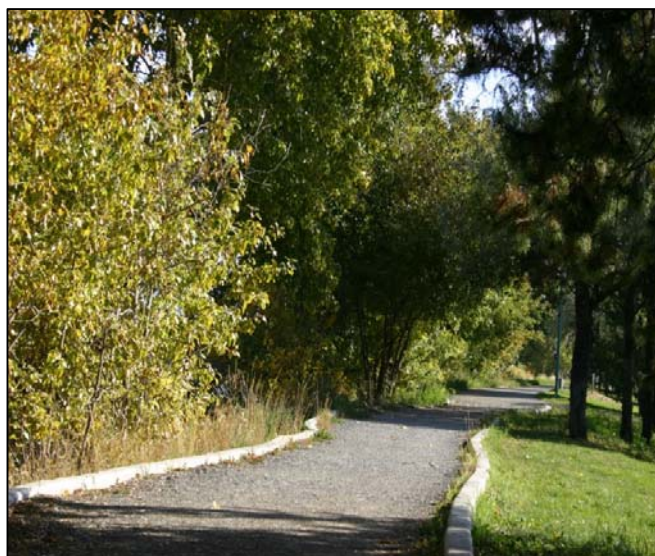
## 3.11 SUSTAINABILITY (cont.)

The preparation of the North Shore Neighbourhood Plan has focused on encouraging sustainable and responsible development by supporting increased densities, by creating a Green Streets Infrastructure Design Pilot Project, and by providing incentives to incorporate green building and site design standards into new developments. In addition to those initiatives, the North Shore Neighbourhood Plan has identified a vision and goals and principles that encourage and support environmentally sensitive development as the North Shore continues to evolve over time. To that end, supporting ecologically responsible development is important not only because it protects and enhances the environment, but because by doing so, it creates community value, quality and appeal, increases access to recreational areas and reduces current and future life cycle infrastructure costs.

The following are examples of ecological guidelines that should and can be incorporated into new development on the North Shore:

- residential development should be designed and constructed so as to minimize impervious surfaces but with material that is appropriate to the climatic conditions of the area, so as to not require extensive amounts of irrigation;
- permeable areas should be designed to accept the drainage and runoff of roofs, patios, and driveways;
- tree planting should occur in conjunction with parking areas to create a canopy over impervious surfaces;
- grass filter strips and elevated yard drains should be used to reduce storm sewer use and to reduce the amount of unfiltered water entering nearby water sources;
- topsoil should be retained on site to be reused for landscaped areas after construction is complete; existing topsoil contains the best growing medium and if used, will result in better growing conditions and success rates for landscaping and also requires less irrigation, compared to the topsoil that is brought in after construction;
- trees should be planted along road rights-of-way to significantly contribute to the urban canopy base.

These guidelines for ecologically responsible development are only a few suggestions that, if incorporated, can have a significant and immediate positive impact on air and water quality within our city.



Tree canopy covering impervious surfaces; Photos: City of Kamloops



# General Policies

## 3.12 TRANSPORTATION AND MOBILITY

Providing a transportation network that accommodates pedestrians, cyclists, vehicles, public transportation, transport trucks and railway systems in a safe and efficient manner is a key element to a healthy and well functioning community. In addition, progressive and sustainable communities recognize that accommodating safe pedestrian and cycle linkages above automobile access is the key to creating and sustaining a healthy and vibrant environment within which people want to live. The North Shore has an excellent base from which to begin to build solid and safe pedestrian and cycle routes and linkages that can be used as an example to emulate future development throughout the rest of the city. Using green street designs is a key component to providing safe transportation routes for all users of the existing road system.



Public transit; rail way line; safe cycle and pedestrian routes; Photos: Carla Stewart

### ACTION ITEMS:

- Identify the top five commuter streets for pedestrians and cyclists to upgrade for improved accessibility by all citizens of Kamloops.
- Create Transit-Oriented Development Guidelines for the Subdivision By-law.
- Identify the feasibility of establishing additional transit coverage throughout the North Shore.
- Prepare a Pedestrian Management Plan to identify areas for improvement to increase safety, visibility and accessibility for pedestrians within the North Shore Towne Centre, Tranquille Commercial District and Tranquille South area.

### POLICY STATEMENTS:

- The City will support the creation of an environment conducive to pedestrian and cycling circulation by supporting the recommendations in both the Bicycle Master Plan and Pedestrian Master Plan.
- The City will consider investing in measures such as landscaping, lighting, street furniture and weather protection in order to enhance the pedestrian environment, particularly along significant pedestrian greenways and linkages within the Tranquille Commercial District and the North Shore Towne Centre areas.
- The City will consider the impact of land use decisions on the movement of pedestrians, cyclists and motorists.
- The City will encourage urban intensification and compact, mixed-use forms of development in order to make walking and cycling more convenient as forms of transportation.
- The City will evaluate the design of new development projects and will consider ways to incorporate amenities and features for pedestrians and cyclists, including adding sidewalks, bike lanes and off-street linkages and trails.
- The City will recognize the importance of public transportation to the community and will support the continued provisions of public transit services.
- The City will support the use of Transit-Oriented Development Guidelines in the review of subdivision plans and development proposals.
- The City will support the periodic review of transit routes and hours of operation to ensure that the best possible level of service is being provided.

(cont...)

# General Policies

## 3.12 TRANSPORTATION AND MOBILITY (cont.)

### POLICY STATEMENTS: (cont.)

9. The City will support creating and maintaining a greenway or pathway system throughout the North Shore to connect all residential neighbourhoods and to create designated cycle commuter routes.
10. The City will recognize that the moving of people, goods and vehicles are part of an integrated transportation system that requires connections between land use patterns and transportation demands; requires efficient and safe vehicle circulations; fosters and supports existing and increased use of public transportation; and accommodates the needs of pedestrians and cyclists as priorities above Single Occupancy Vehicle movements.
11. The City will support the elimination, over time, of points of conflict between the railway and other modes of transportation, particularly at grade-level crossings, throughout the North Shore.
12. The City will enhance arterial corridors to make them more pedestrian and cycle friendly, particularly within residential areas.
13. The City will support Ord Road to be included as a major upgrade item to the Development Cost Charge capital budget to accommodate future development of the Tranquille lands, the commercial and industrial development of Ord Road and the airport lands.
14. The City will support employing traffic calming techniques, where applicable, including adding signage, using alternative paving standards, narrowing roads and adding landscaping areas to further enhance neighbourhood safety.
15. The City will support the strategic location of off-site parking facilities to promote pedestrian use and park and share opportunities.
16. The City will support extending public transit service to McArthur Island, particularly at peak use times, for special events, on weekends, and in conjunction with events at the Boys and Girls Club.
17. The City will support Transportation Demand Management strategies to educate the public about the alternatives to vehicle use.
18. The City will support reductions in parking requirements for strata developments that engage in alternative forms of transportation such as providing bike lockers, transit passes and hybrid cars to use in a pool system for residents.
19. The City will support the creation of well-connected street networks in order to facilitate the orderly circulation of traffic and to reduce congestion by offering motorists multiple choices for their destinations.
20. The City will support creating a safe and efficient pedestrian crossing between the Northhills Mall and the Fortune Shopping Centre, across Fortune Drive, to provide access between the two major economic generators of the North Shore Towne Centre.

# General Policies

## 3.12 TRANSPORTATION AND MOBILITY (cont.)

### 3.12.1 Specialized Development Standards

The City of Kamloops has been working with the Kamloops Airport Authority Society (KAAS) for many years to support increased capacities at the Kamloops Airport. In conjunction with this expansion is an increase in airplane traffic and the potential for greater noise disturbances to adjacent residential neighbourhoods.

In 2007, the KAAS commissioned a study to examine the projected noise levels to those lands and uses surrounding the airport. From that review, it was identified that significant portions of Brocklehurst, the Airport Entry Corridor and the Agricultural Lands would experience increases in airplane noises, with specific areas identified on the Airplane Noise Exposure Contours Map (shown below). In order to manage these noise disturbances into the future, additional noise abatement measures will be required for new construction. Specifications for construction, with a focus on insulation type and placement, will be detailed in a covenant that will be registered against a property at the time of a zoning or subdivision process.

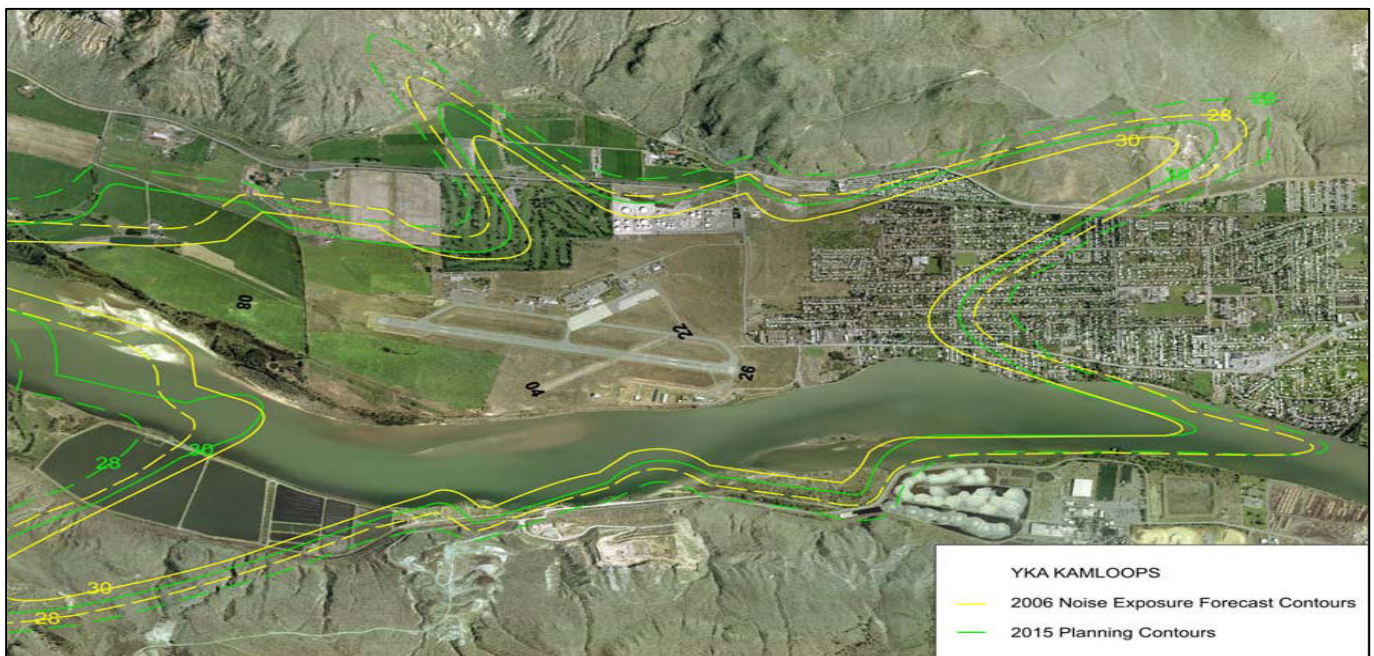
#### POLICY STATEMENTS:

1. The City will support the management of indoor noise in homes built within the Airplane Noise Exposure Contours (see Map 18), requiring noise abatement covenants for sites being zoned, rezoned or subdivided for residential development.

#### ACTION ITEMS:

- a. Prepare a by-law and covenant, to be used for those properties falling within the Airplane Noise Exposure Contours area, requiring additional soundproofing for residential construction.

Map 18: Airplane Noise Exposure Contours



Source: Kamloops Airport Noise Exposure Forecasts and Vicinity Noise Impacts;  
Kamloops Airport Authority Society



# General Policies

## 3.13 RECREATION AND OPEN SPACE

The North Shore has access to an impressive amount of recreational and open space. The North Thompson River, Thompson River, McArthur Island, Rivers Trail, McDonald Park, several tot lots, Henry Grube Centre, school yards, grasslands trails and flat, wide residential street boulevards are some of the areas available to residents and visitors of the North Shore. Because of this extensive list of opportunities however, it is easy to miss that there are also gaps on the North Shore, particularly in providing areas of open space in a highly built out and dense community and more importantly, providing safe and continuous pedestrian routes to connect neighbourhoods and commercial districts.

Supporting the continued use of neighbourhood parks, creating greenways that connect all neighbourhoods, counteracting the effect of the dangerous goods route and subsequent negative effect the frequent passing of large semi-truck trailers has on a pedestrian environment, and supporting creating pockets of public spaces along and within the Tranquille Commercial District and North Shore Towne Centre are all essential to maintaining the health of a community and creating opportunities for growth and sustainability.

### POLICY STATEMENTS:

1. The City will support creating and maintaining a greenway or pathway system throughout the North Shore to connect all residential neighbourhoods and to create designated cycle commuter routes.
2. The City will support the provision of recreational needs for the North Shore and the city by maximizing recreational opportunities on existing open space areas and facilities, while planning for an integrated parkland system to meet future needs.
3. The City will support maintaining and enhancing the value of the North Shore's scenic and natural features which are the keystones of the neighbourhood's quality of life.
4. The City will support developing North Shore parks and recreation facilities with an interconnected system of trails and urban pathways to meet the community's recreational needs and provide access to open space.
5. The City will support integrating the North Shore's heritage and cultural diversity into the development of park sites and recreation facilities, as appropriate.

### ACTION ITEMS:

- a. Investigate establishing an 'Adopt-a-Trail' Program to engage community involvement in the development and maintenance of current and future trails.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

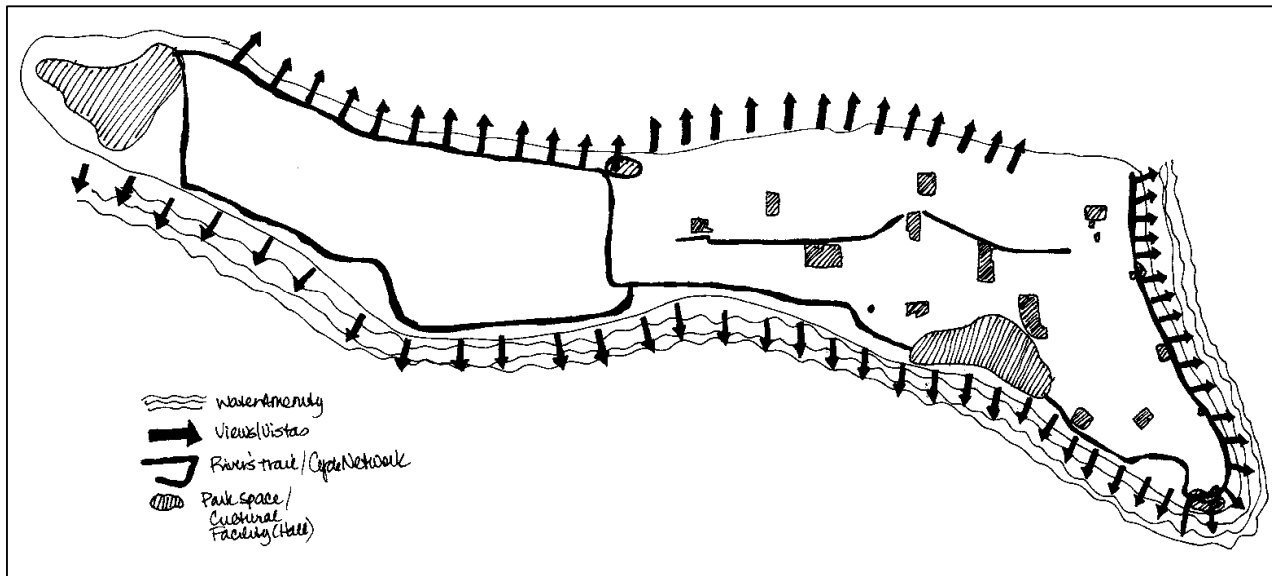


# General Policies

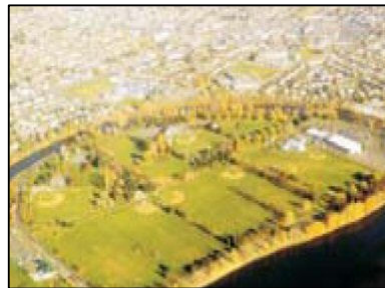
## 3.13 RECREATION AND OPEN SPACE (cont.)

As the Amenities and Connections Map (see below) illustrates, there are significant vistas and water amenities available on the North Shore.

Map18: Amenities and Connections



Rivers Trail along Schubert Drive; Photo: Carla Stewart



McArthur Island Active Recreation Areas; Source: City of Kamloops



McArthur Island Passive Recreation Areas; Photo: Carla Stewart



Rivers Trail along Agricultural Lands; Photo: Carla Stewart



Schubert Drive Moose Tot Lot; Photo: Carla Stewart



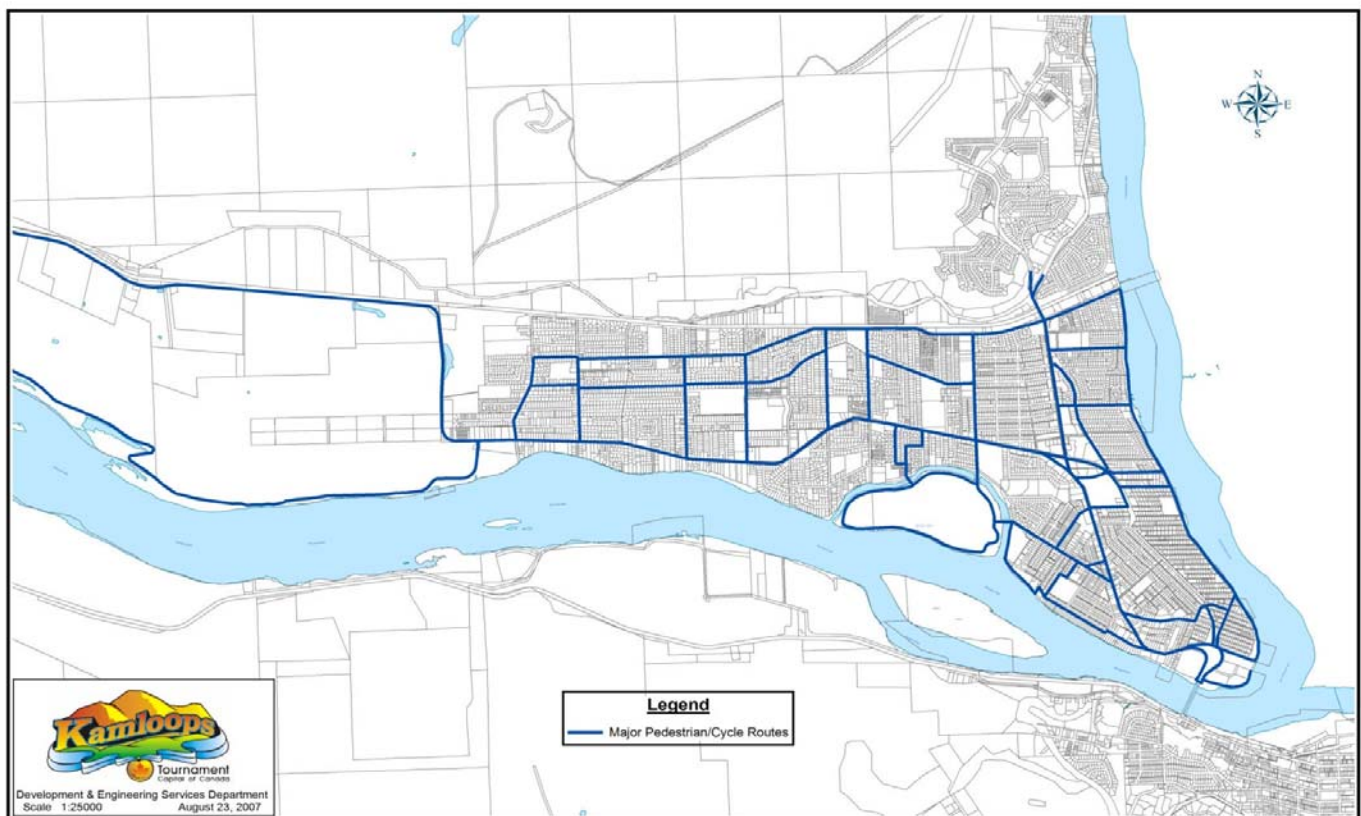
McDonald Park; Photo: Carla Stewart

# General Policies

## 3.13 RECREATION AND OPEN SPACE (cont.)

The Mobility Connections Map (see below) illustrates the kind of connectivity that exists on the North Shore for both cyclists and pedestrians. This map was created from information compiled from both the Pedestrian Master Plan and the Bicycle Master Plan and information from the community on what routes are used for transportation and recreation purposes. The North Shore comprises many streets that connect to one another offering a substantial number of ways in which both pedestrians and cyclists can move throughout the community. Although gaps do exist (see Map 16: Accessibility Gaps on page 49) the wide streets, boulevards and grid pattern street layout create opportunities for recreation and access to open space that other residential communities within Kamloops do not have.

Map19: Mobility Connections



# General Policies

## 3.14 SERVICING AND UTILITIES

Many areas of Kamloops experience servicing and utilities constraints related to water, storm sewer and sanitary sewer capacities. These constraints are due to a myriad of reasons including: upgrades to infrastructure that has not kept pace with the rate of growth in existing residential areas; a lack of the use of substantial water-saving techniques within residential areas; the high cost to maintain the existing and extensive infrastructure system throughout the whole city; and the expansion of single family dwelling development which places higher demands on services compared to higher density development.

The North Shore has experienced some of these servicing issues and is currently dealing with a storm sewer rock pit system that requires upgrades and water capacity limitations due to the existing 20-30 year old pipes reaching capacity. Addressing these servicing issues is complex, costly and requires multiple solutions.

The North Shore Neighbourhood Plan supports several measures as a way to address the servicing constraints that exist. These include:

- supporting increased densities in key areas of the North Shore to reduce the strain on the water system;
- supporting a minimum density of single family with a suite to make more efficient use of existing servicing levels;
- supporting green roofs and integrated storm management systems into commercial development;
- supporting water conservation methods and techniques for both commercial and residential development;
- supporting identifying alternatives to infrastructure by engaging in the Green Streets Infrastructure Design Pilot Project; and
- developing incentives to encourage the use of ecologically friendly building and site design.

The North Shore Neighbourhood Plan is not intended to be used as a means to solve these larger servicing issues but to provide alternatives and the means by which better development can begin to take place, one site at a time.

### **POLICY STATEMENTS:**

1. The City will support the use of sustainable infrastructure systems for both residential and commercial development.
2. The City will support commercial and multiple family development using green infrastructure and stormwater systems.
3. The City will support the use of water conservation measures, including the use of residential rain barrels, water meters, commercial grey water systems, increases in required permeable surface areas and the use of permeable parking surface materials such as grass strips or grasscrete products.
4. The City will support the use of well water, on large lots, for irrigation purposes only.

### **ACTION ITEMS:**

- a. Evaluate the use of water conservation methods for residential and commercial development.

### **SUPPORTING REGULATIONS:**

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

## Land Use Policies



# Land Use Policies

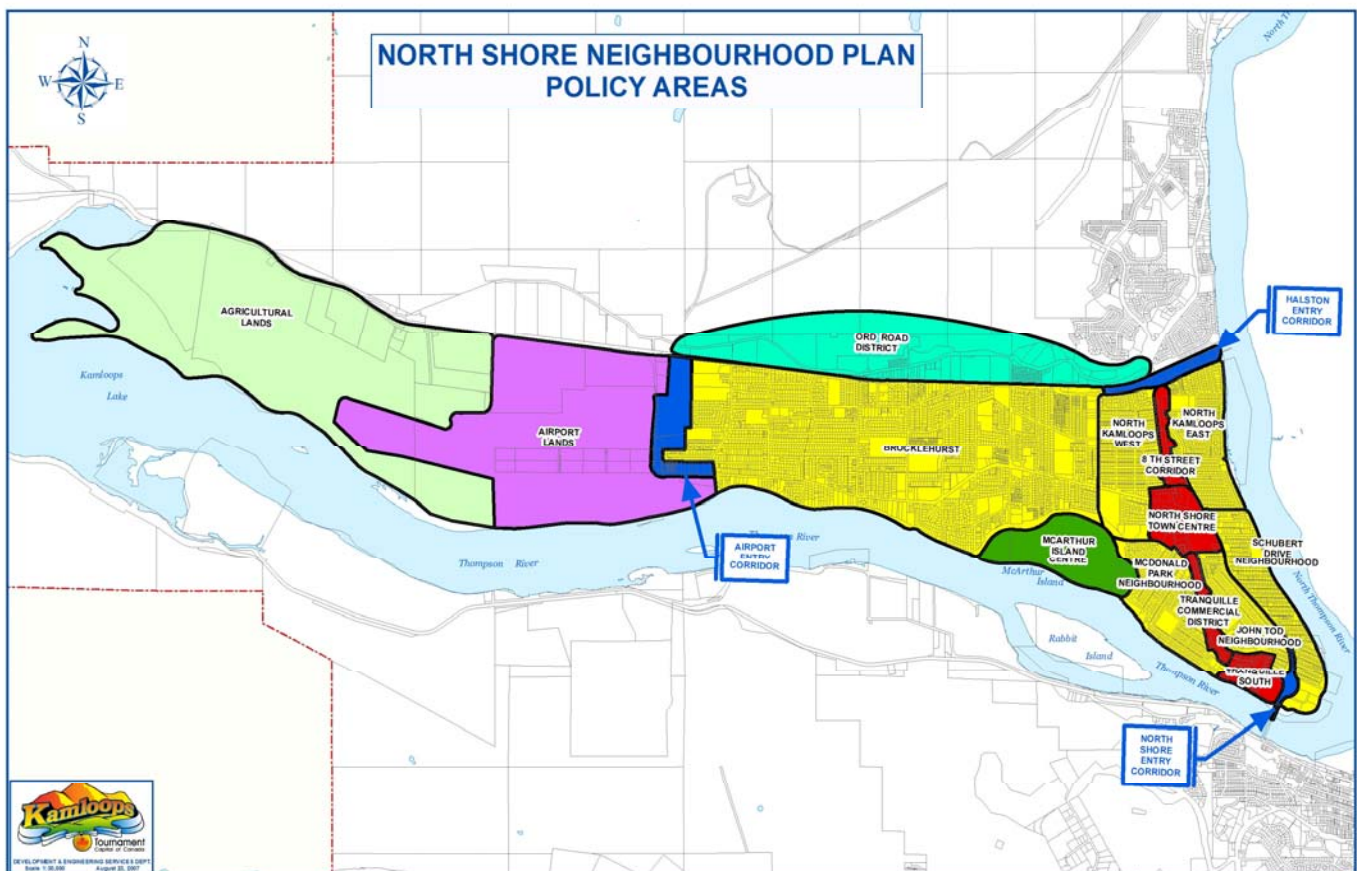
## 4.0 INTRODUCTION

For illustrative purposes intended to aid policy development for this land use Plan, the two neighbourhoods of the North Shore, Brocklehurst and North Kamloops, were divided into separate and distinct districts. Creation of each area was initially conducted by the North Shore Advisory Committee and was determined based on road layout patterns, lot size, the presence of significant natural features, type of housing stock, cultural and ethnic qualities, and/or a dominant type of land use. From the initial categorization, 17 of those areas were used for this Plan, including: the North Shore Entry Corridor, the Schubert Drive Neighbourhood, North Kamloops East and West, 8th Street Corridor area, the Halston Entry Corridor, Ord Road District, Brocklehurst, the North Shore Towne Centre, the Tranquille Commercial District, Tranquille South commercial node, John Tod Neighbourhood, McDonald Park Neighbourhood, Airport Lands, Airport Entry Corridor and the Agricultural Lands at the far west of the project area.

The North Shore Neighbourhood Plan Policy Areas Map identifies each district and is used to provide distinctions between the individual areas. Each district has been analyzed using current urban design principles to determine their main characteristics. Focus was placed on movement systems, connections, barriers, block patterns, topography, environmentally sensitive limitations, views, building form, economic influences and historical uses and features. The urban design analysis is intended to provide general information on existing conditions and does not represent a specific development scenario.

This Section consists of policies created from the work conducted by the North Shore Advisory Committee, from the feedback received from the public and from the urban design analysis completed for the Plan.

Map 20: Project Policy Areas



# Land Use Policies

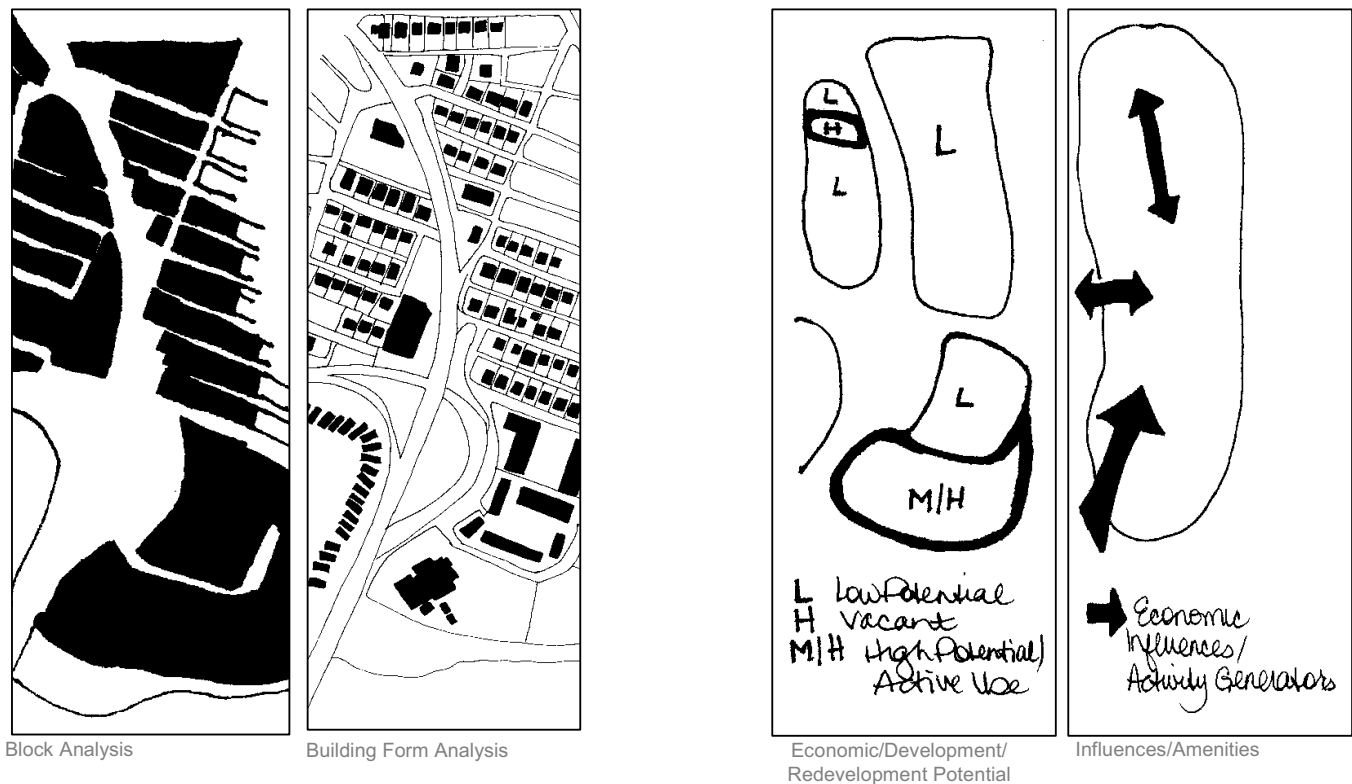
## 4.1 NORTH SHORE ENTRY CORRIDOR

### Urban Design Analysis

The characteristics identified for the North Shore Entry Corridor through urban design analysis include the following:

- block pattern reflecting a grid system with back alleys and a significant jug-handle, off-ramp road network connecting traffic from the Overlanders Bridge to the Tranquille South and Tranquille Commercial District commercial areas;
- built form pattern showing mostly small residential buildings with some vacant parcels and other commercial facilities, including two churches and a funeral home;
- the whole area is located within the 20 and/or 200 year flood plain;
- this entry corridor funnels activity to and from the South Shore, and to both the North Shore Towne Centre and the Tranquille Commercial District; the corridor is also a major thoroughfare for residents from North Kamloops, Brocklehurst, Batchelor Heights and Westsyde, who are accessing the City Centre and other parts of the South Shore; and
- this area has several heritage structures and is also an access point for all city residents who wish to use the Rivers Trail and who wish to access the Overlanders Beach.

Figure 3: North Shore Entry Corridor Urban Design Analysis



# Land Use Policies

## 4.1 NORTH SHORE ENTRY CORRIDOR (cont.)

One of the main gateways for the North Shore is the southern entry point from the South Shore via the Overlanders Bridge. Significant effort has been put into creating expansive landscaped areas at the off-ramp from the bridge which also takes advantage of the long growing season and high levels of sun enjoyed by North Shore residents.

Uses along this entry corridor are well established but some boulevard areas and the North Shore Business Improvement Association entry sign should be updated to create a welcoming feeling and provide a reflection of the diversity and unique elements of the North Shore.



North Shore Entry Corridor; Photos: Carla Stewart

### POLICY STATEMENTS:

1. The City will support development within the North Shore Entry Corridor area that incorporates architectural, landscaping and site design characteristics that provide sensitive infill development into an existing neighbourhood.
2. The City will support incorporating features such as landscaping and signage in the road allowance to clearly identify gateway areas.
3. The City will support upgrading the boulevards of key intersections in the North Shore Entry Corridor area to provide a positive, welcoming environment to the North Shore.

### ACTION ITEMS:

- a. Work with the North Shore Business Improvement Association to install information and identification signage about the North Shore within the North Shore Entry Corridor area.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

# Land Use Policies

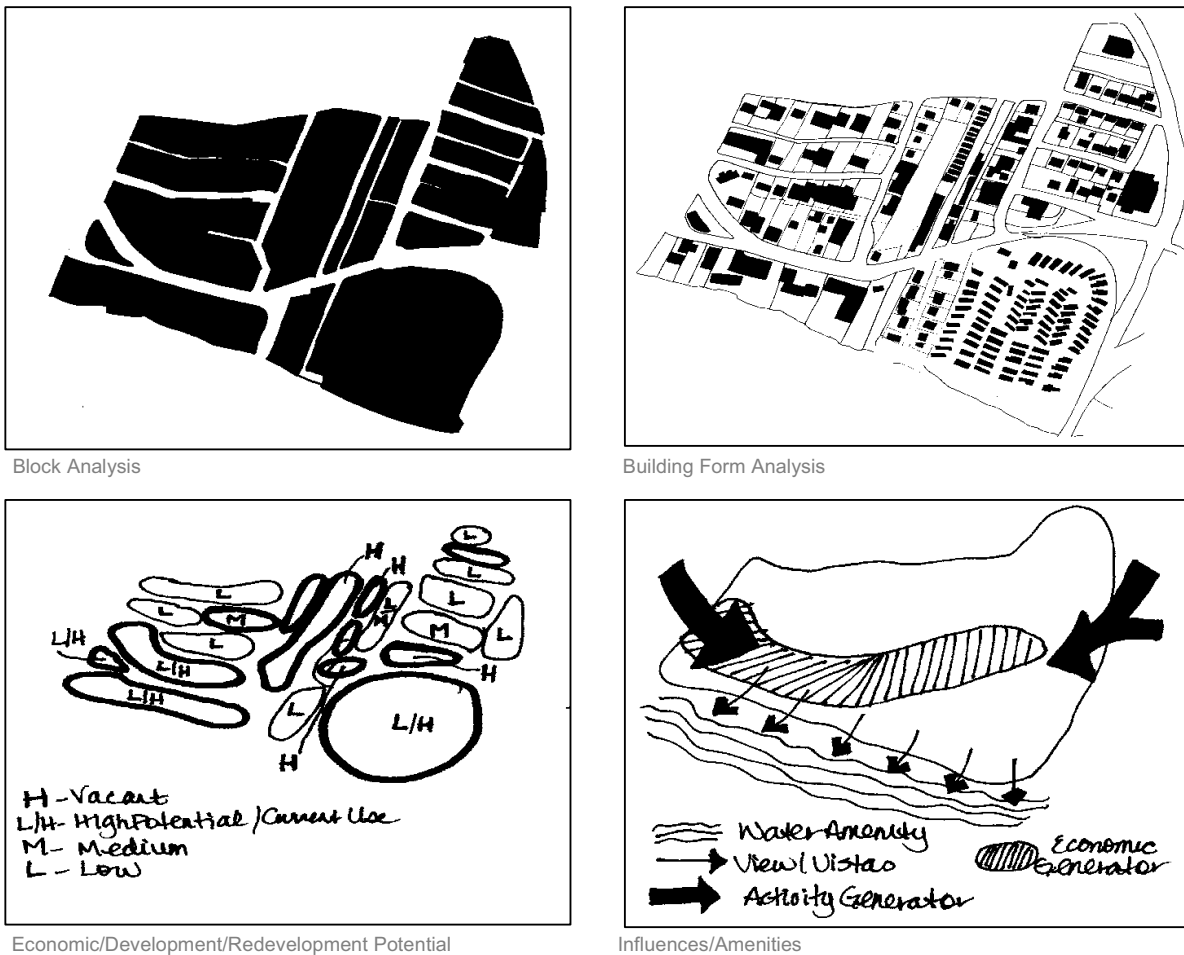
## 4.2 TRANQUILLE SOUTH

### Urban Design Analysis

The area characteristics identified for the Tranquille South area through urban design analysis include the following:

- block pattern showing inconsistent sizes and locations of properties with some back alleys that don't always extend from street to street;
- built form pattern showing several vacant or underdeveloped areas and a mixed-use neighbourhood comprising a mobile home park, service commercial uses, neighbourhood pub, retail sales, restaurants, churches, multiple family development and an ambulance station;
- the whole area is located in the 20 and/or 200 year flood plain;
- several economic generators exist in this area including auto sales and service businesses, convenience stores, gas stations, delis, restaurants, bookstores, hardware stores, and used goods stores;
- there are a few heritage resources in the area, in particular, the Wilson House, which was rehabilitated into an office and which is the new home of the North Shore Business Improvement Association;
- commercial uses are focused toward the automobile which is a historic use; and
- the area represents a gateway to the Tranquille Commercial District and offers several opportunities for redevelopment to showcase the North Shore commercial district.

Figure 4: Tranquille South Urban Design Analysis





# Land Use Policies

## 4.2 TRANQUILLE SOUTH (cont.)

The southern portion of the Tranquille Commercial District is a key component to this significant commercial area of the North Shore. Representing the entry point to Tranquille's commercial district, the Tranquille South node also represents an area originally dominated by auto-oriented uses that have been slow to transition into pedestrian-oriented businesses. Tire sales, gas stations and auto-repair are the first type of businesses that greet visitors to this downtown-like street despite having design guidelines and zoning regulations for over 15 years that support alternative forms of land uses and development types.

As part of the public participation process of the North Shore Neighbourhood Plan, the North Shore Advisory Committee identified the Tranquille South node as an important area requiring landmark building construction and the discontinuance of auto-oriented uses. Similar to other entry corridors located on the North Shore, having building design and uses that act as a reflection of a community is essential for long-term economic prosperity and for the continued health and well-being of that community's residents and businesses.



East Entrance to Tranquille South;  
Photo: Carla Stewart



Tranquille South Streetscape;  
Photo: John Popoff



New Home of North Shore Business Improvement Association;  
Photo: City of Kamloops

### POLICY STATEMENTS:

1. Should redevelopment of the Riverdale Mobile Home Park take place, the City will support that redevelopment to a mixed commercial and residential use and only where the existing density of the Mobile Home Park, at a minimum, is retained within the new development.
2. The City will support the Tranquille South area as an entry to the Tranquille Commercial District and will support development standards that reflect this important role.
3. The City will support incorporating features such as landscaping and signage in the road allowance to clearly identify the gateway.
4. The City will support pedestrian-oriented uses directly abutting Tranquille Road, such as mixed-use commercial and multiple family developments, cultural and institutional facilities, and office and retail services within the Tranquille South Area.
5. The City will support multiple family residential in that portion of the Tranquille South Area north, or northeast of Tranquille Road.
6. The City will support restricting drive-thrus in this area where the property abuts directly onto Tranquille Road.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

# Land Use Policies

## 4.2 TRANQUILLE SOUTH (cont.)

Designing urban environments in order to create places that relate to people and exhibit attractive architectural building details and space layouts is a complex and challenging endeavour. In an effort to provide a visual example of the type of development that is supported in this document, the following images are provided as functioning urban environments and of the type of infill that would be appropriate within this area of the North Shore.

Before



East Entrance to Tranquille South;  
Photo: ND Lea Consultants Ltd.

After



East Entrance to Tranquille South;  
Source: ND Lea Consultants Ltd.



Streetscape Example: Pedestrian Friendly, Visually Appealing; Mixed Uses  
Original Image Source: See Sources and References Section  
Drawing: Carla Stewart

# Land Use Policies

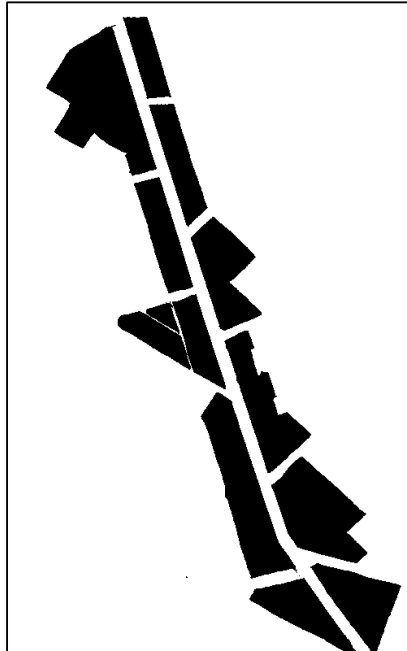
## 4.3 TRANQUILLE COMMERCIAL DISTRICT

### Urban Design Analysis

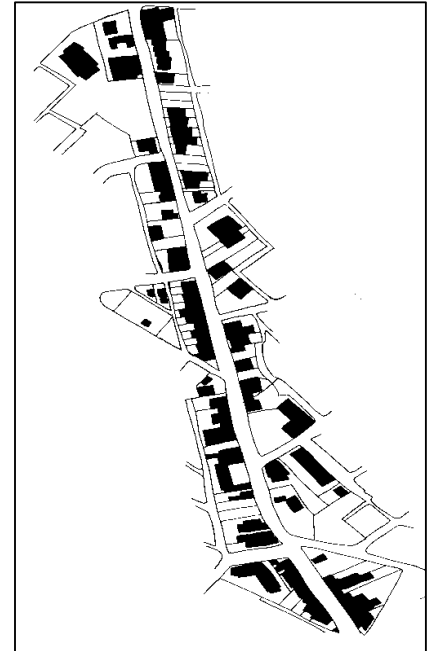
The characteristics identified for the Tranquille Commercial District area through urban design analysis include the following:

- block pattern reflecting varied agglomerations of land serviced by one road with several smaller local roads feeding into the area creating many intersections;
- was designed and functions as a main street which is reflective of its historical function as the downtown to the Village of North Kamloops;
- built form pattern shows some areas of land vacancy or underdeveloped lots and the corridor is serviced by back alleys where some parking has been allocated;
- has aspects of proper urban form with buildings pushed to the sidewalk edge and some suburban development form with auto-oriented land uses and/or parking lots located along the street front;
- there are no parkades located within this commercial area and there are no on-street parking meters;
- most of the corridor, with the exception of the northern tip, is located within the 20 and/or 200 year flood plain;
- corridor represents a major economic generator for the North Shore and particularly for the two residential neighbourhoods that flank it to the east and west: McDonald Park and John Tod;
- public transportation is available along this route and approximately 18 heritage buildings are scattered throughout; and
- the street is very long and has a significant amount of pedestrian traffic however, walking from one end to the other takes longer than 10 minutes and is prohibitive for seniors or persons with disabilities.

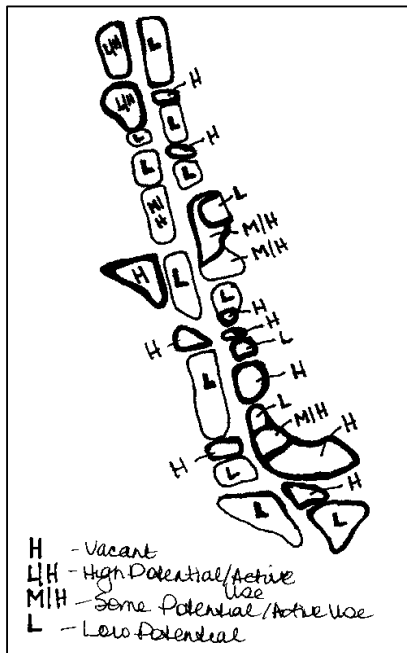
Figure 5: Tranquille Commercial District Urban Design Analysis



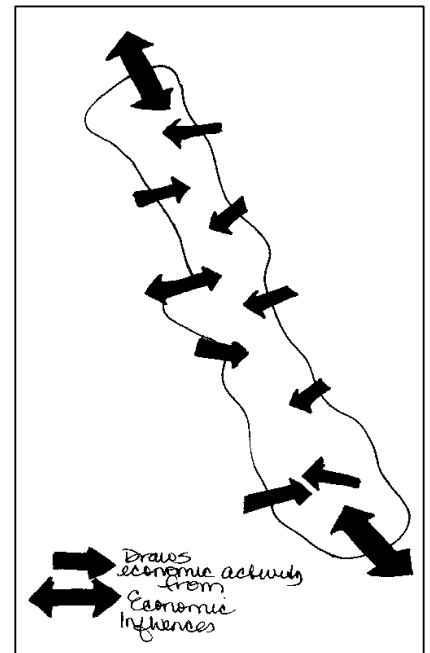
Block Analysis



Building Form Analysis



Economic/Development/  
Redevelopment Potential



Influences/Amenities

# Land Use Policies

## 4.3 TRANQUILLE COMMERCIAL DISTRICT (cont.)

The Tranquille Commercial District is a focal point for the North Shore and is often one of the main places where visitors will judge and remember this community. Accommodating the largest and most diverse concentration of central functions for the North Shore, including retail, office, services, entertainment, government, institutional, residential and community activities is paramount to ensuring the success of the Tranquille Commercial District. This diversity is supported by ensuring there are legitimate day and night uses occurring, and by encouraging residential uses to take place in order to create a neighbourhood that is a home and not just a transportation corridor.

### **ACTION ITEMS:**

- a. Maintain and enhance a vibrant and mixed use Commercial District by establishing a long-term program of community improvement projects and a strategy for implementation and improvement in a planned and coordinated manner.
- b. Examine the feasibility of installing a traffic circle at Yew Street and Tranquille Road.
- c. Develop a comprehensive parking strategy for the Tranquille Commercial District.
- d. Develop a streetscape enhancement program for Tranquille Road and for main intersections within the Tranquille Commercial District.

### **SUPPORTING REGULATIONS:**

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

### **POLICY STATEMENTS:**

1. The City will support and encourage the strengthening of the Tranquille Commercial District as a focal point for retail, office, residential, cultural, community and civic facilities.
2. The City will encourage and enhance the character of the Tranquille Commercial District as the historic commercial core of the North Shore, through heritage conservation, streetscape improvements and by ensuring the compatibility of new development within and adjacent to the Commercial District designated area.
3. The City will foster a Tranquille Commercial District that facilitates and attracts a high level of social activity throughout the day and night and that contributes positively to the North Shore's sense of identity.
4. The City will support the beautification of the Tranquille Commercial District through financial and other means, including the use of targeted public investments, incentive programs and cost sharing opportunities with private interests and businesses.
5. The City will support strengthening the viability of existing businesses in the Tranquille Commercial District by stimulating and encouraging new private investment in high and medium residential development.
6. The City will support the Spirit Square program by examining the use of the property located along Yew Street and Mackenzie Avenue as the location for a new community square.
7. The City will support the Mackenzie Avenue "triangle" as a Special Development Area requiring a comprehensive development review prior to redevelopment approval where intensive residential development, mixed uses and the potential use of a portion of the laneway are incorporated into the design of the area.

(cont...)



# Land Use Policies

## 4.3 TRANQUILLE COMMERCIAL DISTRICT (cont.)

### POLICY STATEMENTS: (cont)

8. The City will discourage auto-oriented commercial uses to locate in the Tranquille Commercial District.
9. The City will encourage new public or private sector office development to locate within the Tranquille Commercial District.
10. The City will encourage multiple family residential development in conjunction with main floor commercial uses fronting Tranquille Road, where those commercial uses encourage pedestrian-oriented and retail activities.
11. The City will continue to recognize the importance of the Tranquille Commercial District area as a link between adjacent neighbourhoods for pedestrian and cycle accesses.
12. The City will stimulate and encourage new private investment in the Tranquille Commercial District by permitting increased densities, setback reductions, tax exemptions for qualified development, Development Cost Charge (DCC) reductions and other potential programs.
13. The City will support improving the image and attractiveness of the Tranquille Commercial District and the North Shore by creating a more appealing, interesting and friendly place to do business, visit and live.
14. The City will examine the feasibility of reducing the clustering of pawn shops along Tranquille Road by permitting pawn shops to locate in commercial buildings that are not stand-alone, single-use or single-business structures or that are buildings located along street fronts.
15. The City will support creating a comprehensive off-street, behind the building parking program with easily identifiable parking locations to support pedestrian-oriented development.
16. The City will support the use of Transit Oriented Development within the Tranquille Commercial District.

(cont...)



Tranquille Commercial District; Photos: John Popoff

# Land Use Policies

## 4.3 TRANQUILLE COMMERCIAL DISTRICT (cont.)

### POLICY STATEMENTS: (cont)

17. The City will support alternative land uses within the Tranquille Commercial District to include small business centres and other neighbourhood-oriented commercial uses.
18. The City will encourage high standards of design in new facilities and developments within this area.
19. The City will support building heights of two (2) storeys along the Tranquille Commercial District corridor with permitted height increases to a maximum of six (6) storeys when green buildings, green infrastructure, mixed-uses and/or non-market housing has been incorporated into the development.
20. The City will support multi-storey development along the Tranquille Commercial District corridor provided that consideration be given to the following: the impact on sunshine reaching public spaces and streets; compatibility with surrounding buildings; impact on wind creation; and the prevention of criminal opportunities.
21. The City will support the use of a Development Checklist (see Appendix "B") for proponents to identify how development fits into the neighbourhood, what creative and innovative designs are being proposed, what crime prevention measures are being used, what positive and negative impacts to the community will result, and how the overall development will be adding benefit to the immediate neighbourhood and the community as a whole.
22. The City will encourage the use of permeable surfaces, green infrastructure, green building design, green roofs and green stormwater systems in all new development and will explore direct incentives for providing environmentally friendly and health sustaining development within the Tranquille Commercial District.
23. The City will encourage and support the use of various regulations in parking, setback requirements, densities and landscaping requirements to encourage green development and encourage creative solutions to infill challenges within the Tranquille Commercial District area.
24. The City will support the maintenance and enhancement of a vibrant, mixed use environment in the Tranquille Commercial District by establishing a long-term program of community improvement projects and a strategy for implementing improvements in a planned and co-ordinated manner.
25. The City will encourage new development to be massed to fit harmoniously into its surroundings, to respect and improve the local scale and character and to minimize the impact on neighbouring buildings and open space.
26. The City will support travel corridors and streetscape improvements that create safe and efficient pedestrian and cycle routes through the Tranquille Commercial District.
27. The City will support restricting drive-thrus from being located within the Tranquille Commercial District to support a pedestrian-oriented environment.
28. The City will support the use of parking structures, integrated into mixed-use developments, to help reduce the off-street parking minimum requirements.

# Land Use Policies

## 4.3 TRANQUILLE COMMERCIAL DISTRICT (cont.)

The Tranquille Commercial District supports a mixture of building forms, styles and heights. Infill development has the potential to support multi-storey buildings or smaller, more discreet developments. The illustrations shown here are examples of the type of development that could take place and the type of streetscape environment that is supported by the policies in this Plan.

Before



South Entrance to Tranquille Commercial District;  
Photo: ND Lea Consultants Ltd.

After



Infill of vacant parcel within the Tranquille Commercial District  
that takes into consideration the form and scale of existing  
buildings adjacent to the infill site;  
Source: ND Lea Consultants Ltd.



Streetscape Example: Pedestrian Friendly, Visually Appealing; Traffic Calming; Mixed Uses  
Original Image Source: *The Next American Metropolis*  
Drawing: Carla Stewart

# Land Use Policies

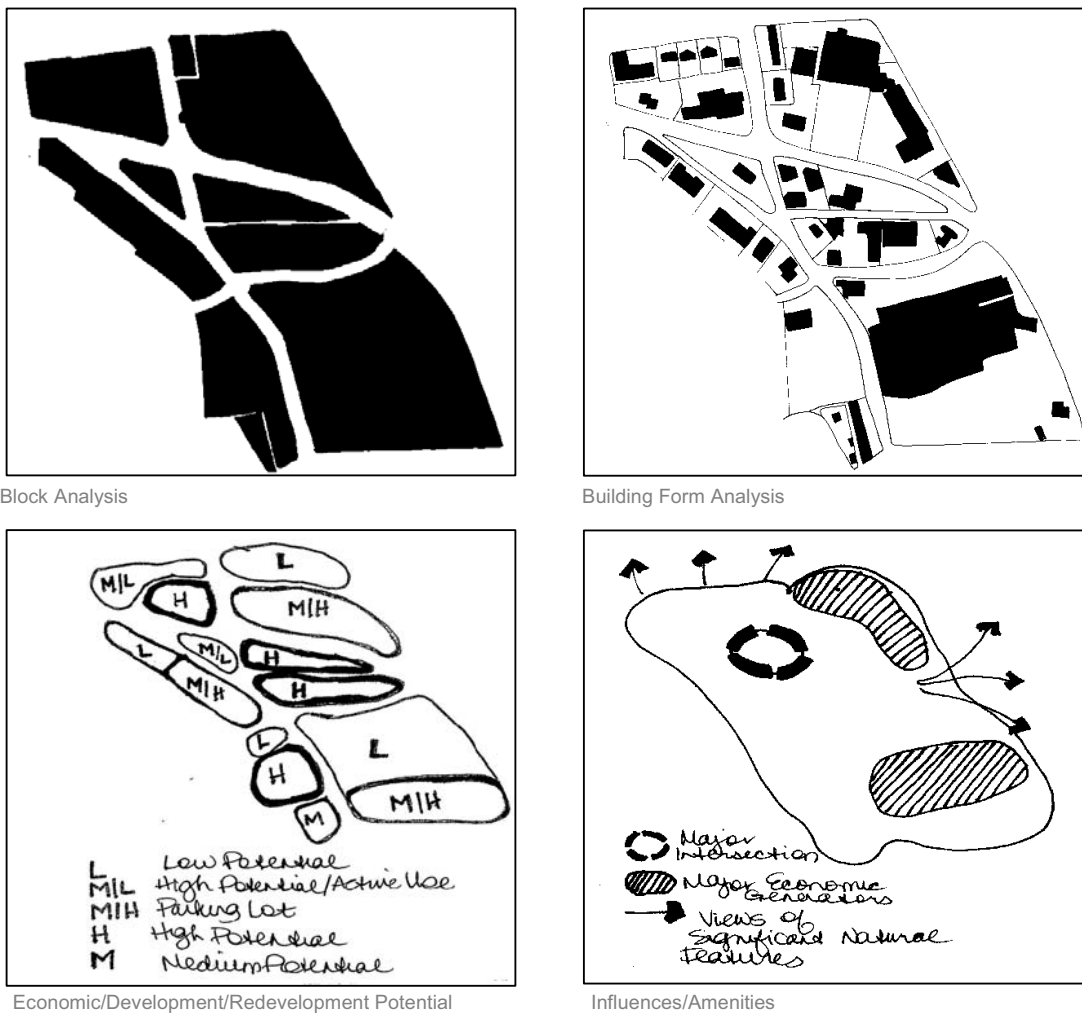
## 4.4 NORTH SHORE TOWNE CENTRE

### Urban Design Analysis

The characteristics identified for the North Shore Towne Centre area through urban design analysis include the following:

- block pattern reflecting large parcels of land centered around the second busiest auto-oriented intersection in the city;
- built form analysis shows underdeveloped land with large expanses of parking lots or vacant areas;
- a Dangerous Goods Route extends through the area and is the meeting point for four North Shore Neighbourhoods: North Kamloops East, North Kamloops West, McDonald Park and John Tod;
- the lands in the area have varying degrees of development potential with two large economic generators that attract residents not only from within the North Shore but also from Batchelor Heights, Westsyde, the Kamloops Indian Band and Rayleigh and Heffley Creek;
- the flood plain does not extend into this area and there are no heritage resources;
- the only cultural facility that exists here is the Thompson-Nicola Regional District's library; and
- a complete urban design analysis was conducted for this area and is shown in its entirety in Section 4.4.1.

Figure 6: North Shore Towne Centre Urban Design Analysis





# Land Use Policies

## 4.4 NORTH SHORE TOWNE CENTRE (cont.)

Through public participation opportunities for the preparation of the North Shore Neighbourhood Plan, the North Shore Towne Centre was identified as the most significant development area on the North Shore. Located adjacent to the two largest economic generators - the Northhills Mall and the Fortune Shopping Centre - and the second busiest auto-oriented intersection in the city, redevelopment of this area is encouraged to include landmark buildings, pedestrian-oriented facades and designs, the inclusion of multiple family units and compatible commercial uses, and alterations to the intersection to make it easier for pedestrians, particularly a growing seniors population, to use this area. Continuing to accommodate large semi-trailer trucks on the designated Dangerous Goods Route is also important and precludes eliminating the right-hand turn lanes.

Transforming this area into a true towne centre is a long-term process requiring considerable public and private effort. Phasing the changes is a key element to success. The Simcoe Avenue area was identified as the most appropriate place to begin that phasing which, ultimately, would help to spur on redevelopment into the other areas of the Towne Centre. A full site analysis of Simcoe Avenue was conducted and is showcased in Section 4.4.1. Additionally, locating a transit exchange near the Northhills Mall will create a demand for pedestrian-oriented businesses and can work to increase connections with the other commercial areas of the Tranquille Commercial District.

### ACTION ITEMS:

- a. Prepare a Pedestrian Management Plan to identify areas for improvement to increase safety, visibility and accessibility for pedestrians within the North Shore Towne Centre, Tranquille Commercial District and Tranquille South area.
- b. Examine the feasibility of reducing traffic volumes through the North Shore Towne Centre to encourage a safer pedestrian environment.
- c. Identify the feasibility of establishing additional transit coverage throughout the North Shore.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines



Simcoe Avenue;  
Photo: Carla Stewart



Towne Centre Existing Uses  
Photo: Carla Stewart



Vacant Property;  
Photo: Carla Stewart

# Land Use Policies

## 4.4 NORTH SHORE TOWNE CENTRE (cont.)



Potential Building Massing and Densities  
Source: North Shore Advisory Committee



Potential Building Massing and Densities  
Source: ND Lea Consultants Ltd.



Under Utilized Commercial Area;  
Photo: Carla Stewart



Potential Building Massing and Densities  
Source: ND Lea Consultants Ltd.



North Shore Towne Centre;  
Photo: Carla Stewart



North Shore Towne Centre;  
Photo: Carla Stewart



Proposed Improvements to  
Pedestrian Environment in North  
Shore Towne Centre;  
Photo: Carla Stewart



Existing North Shore Towne Centre  
Pedestrian Environment;  
Photo: Carla Stewart

## POLICY STATEMENTS:

1. The City will support creating a designated Farmers' Market on the North Shore, particularly along Simcoe Avenue, in conjunction with pedestrian access and possible temporary road closures.
2. The City will support and enhance a vibrant and mixed-use North Shore Towne Centre by establishing a long-term program of community improvement projects and a strategy for implementing improvements in a planned and coordinated manner.
3. The City will support a North Shore Towne Centre that facilitates and attracts a high level of social activity throughout the day and night and that contributes positively to the North Shore's sense of identity.
4. The City will support the beautification of the North Shore Towne Centre including the use of targeted public investment, incentive programs and cost sharing initiatives.
5. The City will encourage the creation of a Towne Centre by ensuring that new development addresses accessibility between parcels, compatibility with adjacent land uses, and pedestrian and cycle links and that a high standard of design is incorporated into the development including the use of CPTED principles and development guidelines to address building form and character.
6. The City supports a North Shore transit exchange within the North Shore Towne Centre.
7. The City will support Transit Oriented Development to locate within the North Shore Towne Centre in conjunction with a North Shore transit exchange.
8. The City will support examining using the Simcoe Avenue area as a location for a public amenity space.

(cont...)

# Land Use Policies

## 4.4 NORTH SHORE TOWNE CENTRE (cont.)

### POLICY STATEMENTS: (cont.)

9. The City will support reducing and minimizing the visual impact of parking and loading areas on primary frontage roads.
10. The City will support buildings and streetscapes to be designed attractively to create a sense of identity through the treatment of architectural forms, landscaping, signage and orientation. The goal of new development should be to enhance the community and image of the North Shore and create a positive gain to the neighbourhood.
11. The City will support and encourage the unique economic and business identity of the North Shore by supporting small businesses as well as tourist-supported commercial and accommodation opportunities within the North Shore Towne Centre.
12. The City will support and encourage enhancing the pedestrian environment within the North Shore Towne Centre by increasing pedestrian safety at the 8th Street, Tranquille Road and Fortune Drive intersection (e.g. by adding 'safe zones' on existing medians) and by supporting development designs that take into consideration the pedestrian environment first, over and above, a vehicle-dominated environment.
13. The City will support building heights of four (4) storeys within the North Shore Towne Centre with permitted height increases to a maximum of ten (10) storeys when green buildings, green infrastructure, mixed-uses and/or non-market housing has been incorporated into the development.
14. The City will support the use of a Development Checklist (see Appendix "B") for proponents to identify how development fits into the neighbourhood, what creative and innovative designs are being proposed, what crime prevention measures are being used, what positive and negative impacts to the community will result, and how the overall development will be adding benefit to the immediate neighbourhood and the community as a whole.
15. The City will support redevelopment that will encourage mixed-use multiple family, commercial-oriented uses, promote development of public gathering places and public art and enhance the existing sense of community identity within the North Shore Towne Centre.
16. The City will support the provision of development incentives to encourage construction of more residential units above commercial, for the construction of green roof, green buildings, and green infrastructure and stormwater designs within the North Shore Towne Centre.
17. The City will support enhancing and preserving the small-town character of the North Shore by concentrating denser development into a Towne Centre allowing for retention of open spaces.
18. The City will support restricting drive-thrus from being located within the North Shore Towne Centre to support a pedestrian-oriented environment.
19. The City will support alternative use classifications within the North Shore Towne Centre to include small business centres and other neighbourhood-oriented commercial uses.



# Land Use Policies

## 4.4 NORTH SHORE TOWNE CENTRE (cont.)

The North Shore Towne Centre supports a mixture of building forms, styles and heights. Infill development has the potential to support multi-storey buildings, a mix of commercial and residential uses, public open spaces and pedestrian-oriented street environments. The illustrations shown here are examples of the type of development that could take place and the type of streetscape environment that is supported by the policies in this Plan.

Before

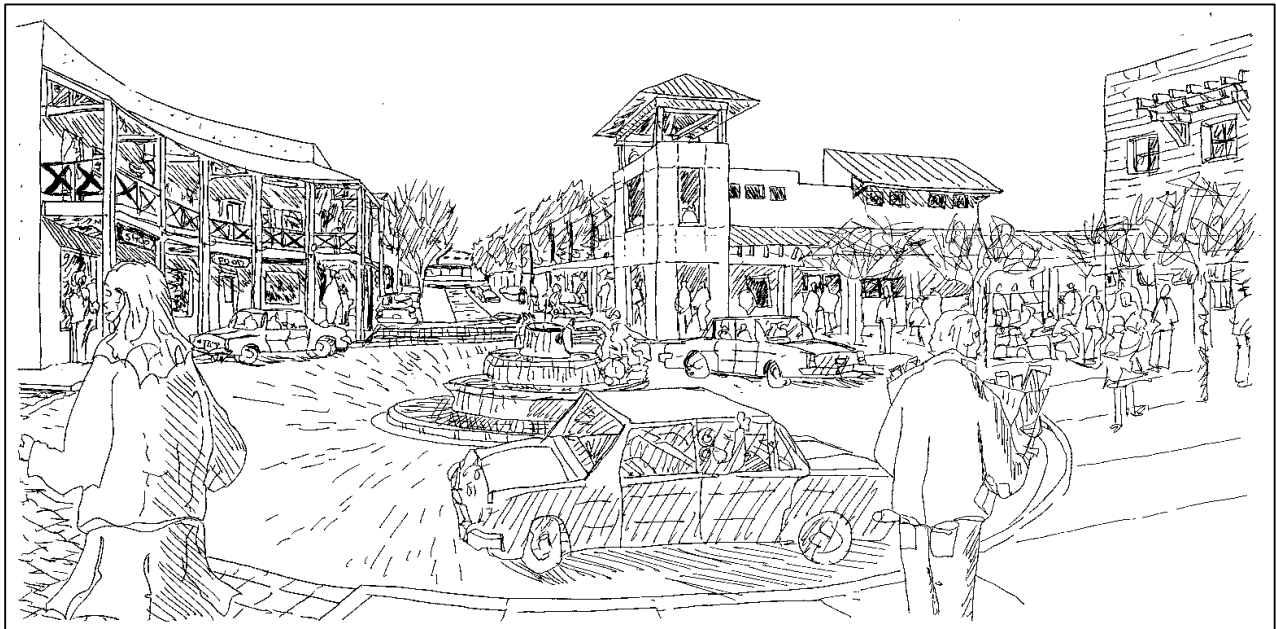


Current uses and activities along Simcoe Avenue;  
Photo: ND Lea Consultants Ltd.

After



Infill of vacant areas; pedestrian-oriented uses added; street improvements made; mix of uses supported;  
Source: ND Lea Consultants Ltd.



Streetscape Example: Pedestrian Friendly, Visually Appealing; Traffic Calming; Mixed Uses  
Original Image Source: *The Next American Metropolis*  
Drawing: Carla Stewart



# Land Use Policies

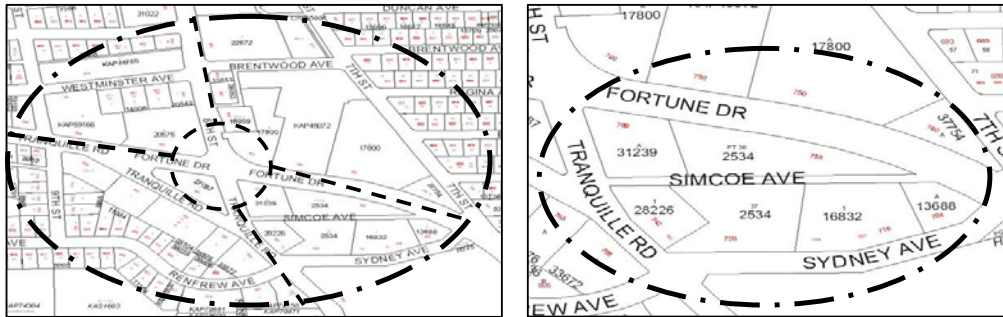
## 4.4 NORTH SHORE TOWNE CENTRE (cont.)

### 4.4.1 Simcoe Avenue Urban Design Analysis

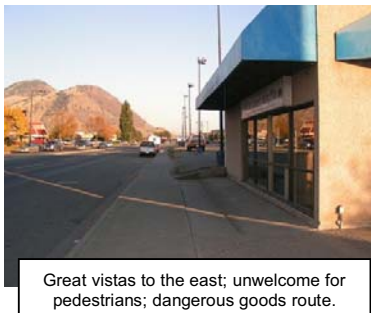
#### Subject Property:

The Simcoe Avenue area is situated within the southeast section of a 33 ha neighbourhood centre known as the North Shore Towne Centre. The area has been identified for increased densities in multiple family development in a mixed-use design that accommodates commercial uses on the main floors.

Simcoe Avenue is centred between the two busiest commercial areas of the North Shore and is adjacent to a Dangerous Goods Route, the second busiest auto-oriented intersection in the city, an increasing seniors population, public transit, arterial roadways and the largest areas of undeveloped commercial land within the community. Concrete block construction, flat land and almost 100 per cent hard surface paving make up the physical characteristics of the site.



**Site Context:** A visual summary of the physical conditions of the site, including building type, building locations, maintenance levels, vehicle dominance and current land uses is provided below.



Great vistas to the east; unwelcome for pedestrians; dangerous goods route.



Narrow right-of-way and current businesses use roadway as a lane.



Very auto-oriented design; no outdoor seating areas for pub on the left.



Boxy building design; highly auto-oriented uses adjacent to bus stops.



Narrow right-of-way; no connection to the street from the buildings.



Currently used by pedestrians, cyclists, transit and single occupancy vehicles.

Photos: Carla Stewart

# Land Use Policies

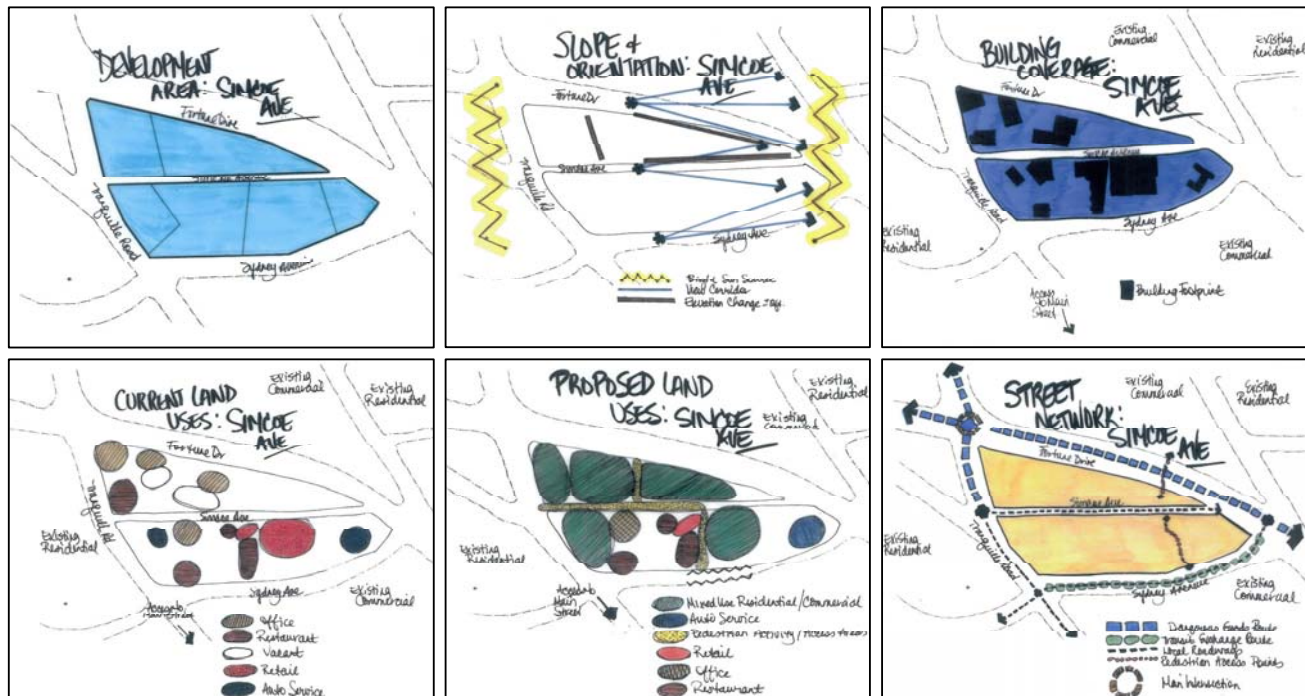
## 4.4 NORTH SHORE TOWNE CENTRE (cont.)

### 4.4.1 Simcoe Avenue Urban Design Analysis (cont.)

#### Project Area Analysis

The following analysis provides a brief understanding of the area:

1. **Building Coverage** - An underutilized site; odd building locations; a lot of vacant land making redevelopment easier and cheaper since there are not a lot of buildings that have to be demolished.
2. **Slope and Orientation** - A view corridor of a mountain range exists to the east down the whole street. Bright sun in the morning from the east and in the afternoon from the west provide for warmth but also a need for the provision of shade. The area is mostly flat with a slight elevation change of approximately two feet for the northern properties.
3. **Street Network** - A Dangerous Goods Route, arterial roadway, collector roadway and high pedestrian traffic areas exist within the area. Pedestrian and vehicle conflicts occur when pedestrians try to cross the busy arterial to get to the other main commercial areas.



Analysis: Carla Stewart

4. **Landmarks and Nodes** - There are two large commercial malls on either side of this area and the second busiest auto-oriented intersection for the whole city is located to the northwest of the site.
5. **Land Uses** - There is a mix of office, retail, restaurant, gas station and vacant buildings in the area. Parking lots are the most dominant feature of the whole subject property area. Anywhere where there is no building there is a parking lot; there is very little landscaping, what does exist is kept to the perimeter in very narrow strips.
6. **Hard/Soft Analysis** - The whole area has only five property owners and a lot of paved surface making it a great site for lot consolidation to build bigger footprints. The double fronting lots on either side of Simcoe Avenue create challenges but offer greater possibilities for introducing mixed uses in the area with pedestrian uses oriented to both street fronts.

# Land Use Policies

## 4.4 NORTH SHORE TOWNE CENTRE (cont.)

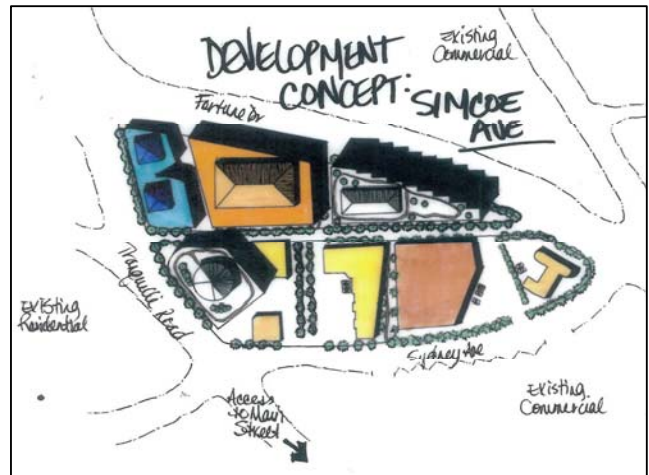
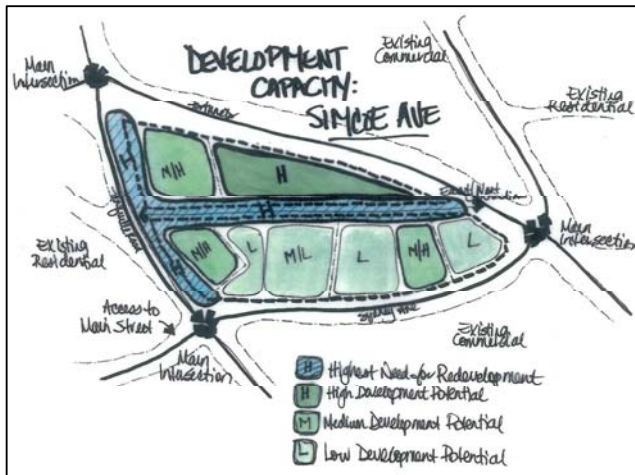
### 4.4.1 Simcoe Avenue Urban Design Analysis (cont.)

#### Conclusions

The North Shore Neighbourhood Plan, through public consultation, has identified several areas of the North Shore that have high redevelopment potential. This potential is not identified as a highest and best use for the area but rather a development that will benefit the residents of the neighbourhood and the broader community.

The citizens of the North Shore have identified an area where a Town Centre would be appropriate and would fit with their vision and goals for the future of their community. The whole town centre area comprises approximately 33 ha and includes well established road networks and existing buildings, necessitating a long-term redevelopment opportunity. As a way to minimize the complexity of the project, Simcoe Avenue was selected as a beginning point; the place to plant seeds for change that can and will spread to the other areas of the Town Centre.

The Simcoe Avenue area was analyzed from an Urban Design point of view. It was revealed that the area has had little maintenance or upgrading and very much reflects, both in design and use, its connection to the first uses of the area: a car dealership and several gas stations. The wide open spaces provide opportunities for new buildings and the narrow width of Simcoe Avenue creates a unique spatial opportunity to focus development and uses to an area that already has a human scale. A priority list for redevelopment will be required in order to move forward with changes.



Analysis: Carla Stewart

# Land Use Policies

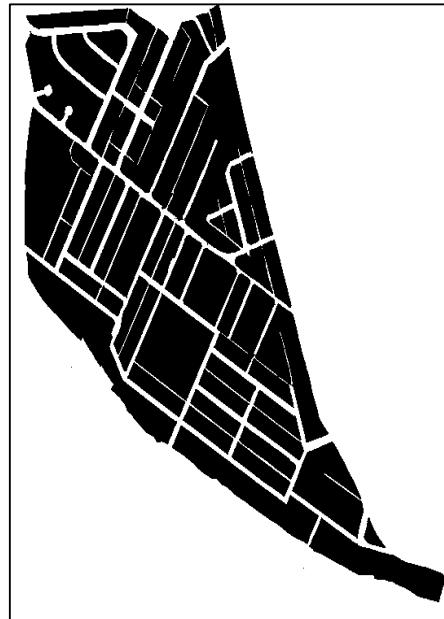
## 4.5 McDONALD PARK NEIGHBOURHOOD

### Urban Design Analysis

The characteristics identified for the McDonald Park Neighbourhood area through urban design analysis include the following:

- block pattern reflecting a grid system with rear alleys and many roads and intersections providing a pedestrian friendly environment;
- built form pattern showing very small single family dwellings on small lots with some multiple family developments dispersed throughout;
- neighbourhood is adjacent to the Thompson River to the south;
- a large neighbourhood park is central and an important amenity space for the residents; it offers a water park, outdoor pool, basketball court, music in the park and art markets;
- major economic generators influencing the neighbourhood include portions of the Tranquille Commercial District and McArthur Island which is one of the biggest tourist attractors for the whole city;
- one main road dissects the neighbourhood and is heavily used to provide access to McArthur Island;
- the neighbourhood is serviced by public transportation, has portions of the Rivers Trail running along the southern edge and has a mix of housing including non-market housing, mid-income housing, a mobile home park, and single and two family dwellings;
- the neighbourhood is also serviced by a church which is a significant heritage resource; and
- the whole area is located within the 20 and/or 200 year flood plain and represents the oldest residential neighbourhood on the North Shore with many single family heritage homes that are approximately 400-500 sq. ft. in size.

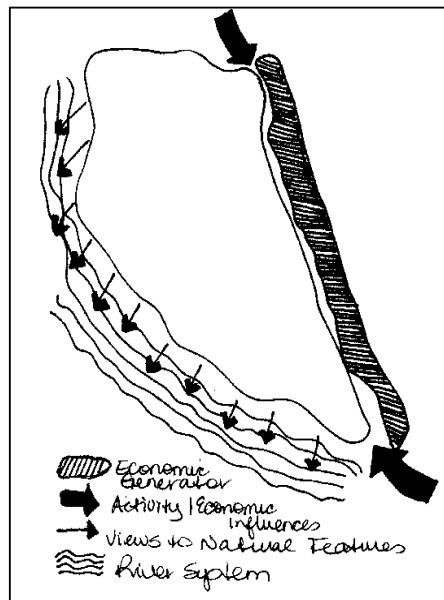
Figure 7: McDonald Park Neighbourhood Urban Design Analysis



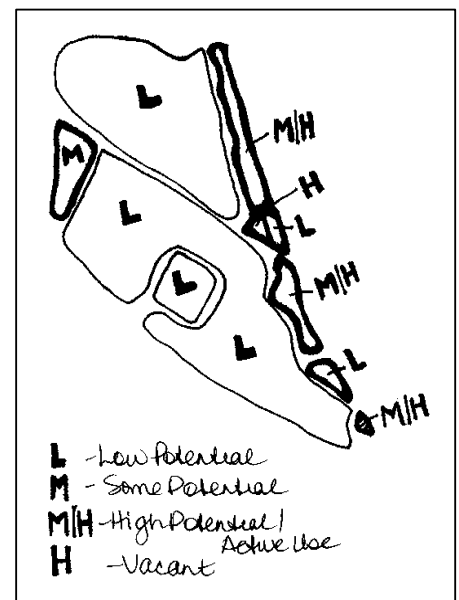
Block Analysis



Building Form Analysis



Economic/Development/  
Redevelopment Potential



Influences/Amenities



# Land Use Policies

## 4.5 McDONALD PARK NEIGHBOURHOOD (cont.)

The McDonald Park Neighbourhood is one of the oldest residential areas of the North Shore and of Kamloops. Situated along the Thompson River between the Tranquille Road commercial core and McArthur Island, the McDonald Park Neighbourhood boasts a large, active and central neighbourhood park, a mix of housing types and densities, community facilities, heritage structures and access to the river and sport complexes.

In 2003, the McDonald Park Neighbourhood engaged in a community process to identify specific strategies that would help maintain and enhance the livability and sustainability of the area. This process focused on safety, beautification, community pride and identity. Since the adoption of the McDonald Park Neighbourhood Plan, the community has wholeheartedly backed its policies and has worked continually with the City to incorporate the elements of the Plan into the neighbourhood.



Typical home type in the McDonald Park Neighbourhood;  
Photo: Carla Stewart



Thompson River Beach;  
Photo: John Popoff



Royal Avenue Single Family Home;  
Photo: Carla Stewart



McDonald Park Neighbourhood  
Heritage Character Home;  
Photo: Carla Stewart

### POLICY STATEMENTS:

1. The City will continue to support the implementation of the McDonald Park Neighbourhood Plan, where appropriate.
2. The City will continue to support Music in the Park at McDonald Park.
3. The City will support the use of small scale commercial activities within McDonald Park, that are oriented toward supporting civic events or that are geared toward supporting local artists, farmers or small scale businesses. These events can include such things as Art Markets, Farmer's Markets and local, small-scale home-based business expos.
4. The City will support retaining and enhancing the desirable qualities and character of the McDonald Park Neighbourhood by supporting and encouraging new development and redevelopment that is at an appropriate scale and character consistent with the existing neighbourhood.
5. The City will support enhanced, safe and effective pedestrian and cycle links through and to and from the McDonald Park Neighbourhood.
6. The City will support alternative forms of housing in flood prone areas by permitting garden suites above garages and the conversion of garages into one residential unit, where those garages and garden suites are adjacent to a rear alley and where the garden suite is not subdivided into a separate legal parcel.
7. Should redevelopment of the Woodland Mobile Home Park take place, the City will support that redevelopment to a multiple family residential development.

(cont...)

# Land Use Policies

## 4.5 McDONALD PARK NEIGHBOURHOOD (cont.)



McDonald Park Entry Feature;  
Photo: Carla Stewart



McDonald Park Art Market;  
Photo: Carla Stewart



Rivers Trail;  
Photo: Carla Stewart

### POLICY STATEMENTS: (cont.)

8. The City will support, where possible, obtaining public access at the river's edge, where it was once previously controlled through private ownership.
9. The City will support examining the creation of a Heritage Conservation Area for the McDonald Park Neighbourhood to protect and enhance the oldest neighbourhood on the North Shore and its unique qualities and characteristics.
10. The City will examine the feasibility of permitting flex-work housing in the McDonald Park Neighbourhood, particularly for artists' studios and small home-based artist boutiques.
11. The City will examine the feasibility of adding beautification treatments to the Rivers Trail along Royal Avenue.

### ACTION ITEMS:

- a. Pursue the completion of an 'On-the-Ground' marking system for the Rivers Trail, to clearly identify pedestrian and cycle routes, to and from and within, the McDonald Park Neighbourhood.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

# Land Use Policies

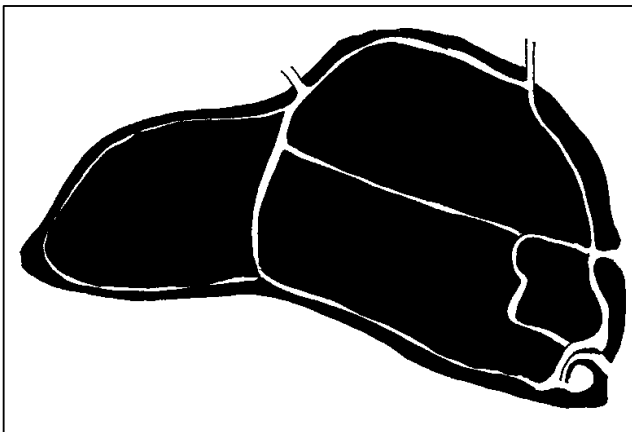
## 4.6 McARTHUR ISLAND PARK CENTRE

### Urban Design Analysis

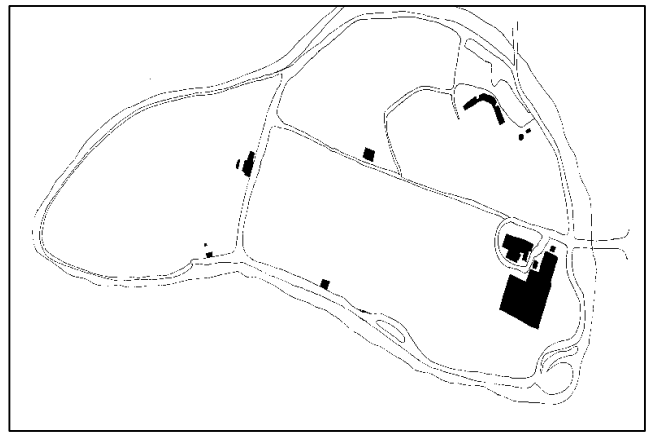
The characteristics identified in the McArthur Island Park Centre area through urban design analysis include the following:

- block pattern reflecting one large piece of property serviced by two access points, one main, internal loop road and a built form pattern showing two recreation buildings in amongst the vast expanses of playing fields, ball fields, soccer pitches etc.;
- lands are bordered by the Thompson River to the south and a slough around the rest of the property creating a land island;
- the 20 and/or 200 year flood plain extends into the whole property;
- one portion of the slough is identified in the Official Community Plan as an Environmentally Sensitive Area due to the fish and wildlife habitat located there;
- currently not directly serviced by public transportation;
- the western portion of the island is leased to a private business which operates a golf course, driving range and restaurant;
- this island represents a significant economic generator for the North Shore and Kamloops due to the City's designation as the Tournament Capital of Canada; people from Kamloops, the province, the country and the rest of the world are drawn here to participate in recreation and sporting events on a year-round basis;
- there are no heritage structures on the property; and
- a boat launch and the Rivers Trail are additional public amenities available on the island.

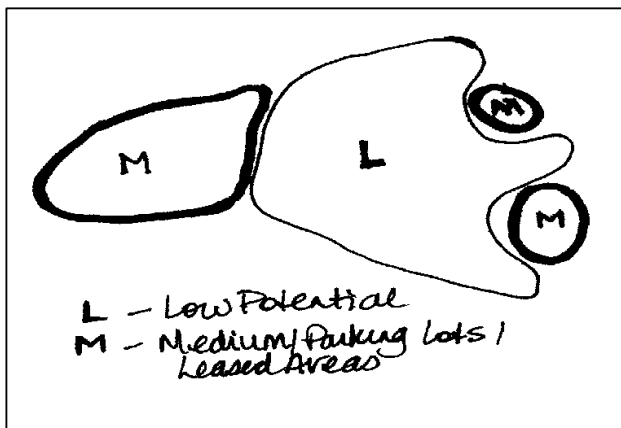
Figure 8: McArthur Island Urban Design Analysis



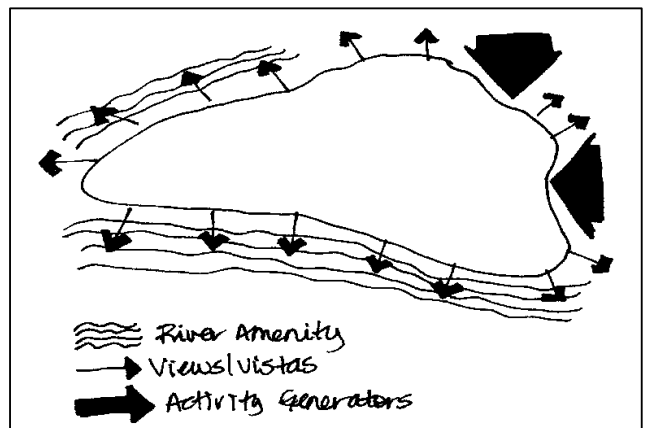
Block Analysis



Building Form Analysis



Economic/Development/Redevelopment Potential



Influences/Amenities

# Land Use Policies

## 4.6 McARTHUR ISLAND PARK CENTRE (cont.)

McArthur Island is the premier sporting and recreation facility within Kamloops and serves as a major economic generator for the North Shore. Accommodating a myriad of sporting facilities, McArthur Island is an essential component to the City's Tournament Capital of Canada designation. Over 20 types of sports can be accommodated on the Island including: baseball, softball, soccer, slo-pitch, lacrosse, hockey, curling, BMX racing, lawn bowling, field hockey, running, boating, skating (skate park), golfing, walking, cricket, cycling, football, tennis, and places to rollerblade. McArthur Island Sports and Events Centre also has a number of indoor facilities, such as the Boys and Girls Club which offers gymnasium and classroom space for indoor recreational needs. McArthur Island also supports numerous trade shows and events throughout the year.

McArthur Island is the home to many types of natural habitat. A slough, which physically creates the island, has natural habitat for turtles, muskrats, beaver, marmots, fish and numerous bird species and also contributes to the success of a nearby butterfly garden. All this wildlife can be viewed on a regular basis by using one of the many internal pathways and walkways.

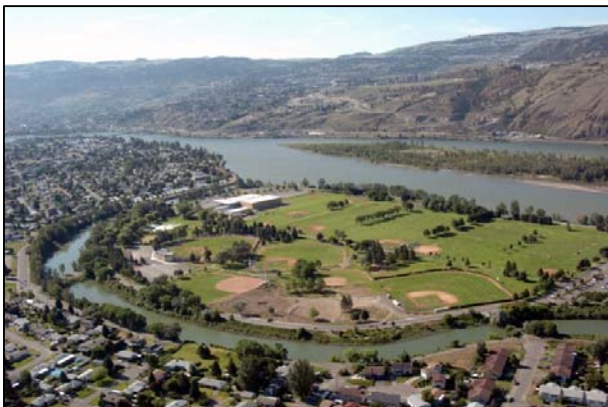
Although McArthur Island has transitioned over the past ten years away from passive park space to more active uses and prescriptive sports facilities, the Island is a premier amenity space for thousands of residents and visitors twelve months of the year. Its adaptability and comprehensive facilities represent a true gem for the city, particularly those residents and businesses of the North Shore.

### POLICY STATEMENTS:

1. The City will examine the feasibility of developing alternative uses at the McArthur Island Park golf course location once/if the golf course ceases to operate from that site.
2. The City will support extending public transit service to McArthur Island, particularly at peak use times, for special events, on weekends, and in conjunction with events at the Boys and Girls Club.
3. The City will continue to support the implementation of the Kamloops Parks and Recreation Master Plan and the Tournament Capital of Canada direction for McArthur Island.
4. The City will support fish and waterfowl habitat enhancement in the McArthur Island Park slough, in conjunction with proposed infrastructure upgrades.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code



McArthur Island Activities; Photos: City of Kamloops



# Land Use Policies

## 4.7 JOHN TOD NEIGHBOURHOOD

### Urban Design Analysis

The characteristics identified for the John Tod Neighbourhood area through urban design analysis include the following:

- block pattern reflecting a grid system, modified in places, with rear alleys and compact form;
- serviced by two main roads to the east and west with several well-connecting internal local roads;
- built form pattern shows small homes on small lots with a central elementary school, several churches, pockets of multiple family developments and a commercial area on the western edge;
- lands are not directly adjacent to any natural amenity but are flat, have access to high morning and afternoon sun for an extended growing season and are not susceptible to any flood plain restrictions;
- major economic generator is from the Tranquille Commercial District area to the west;
- is one of the older neighbourhoods on the North Shore with many heritage structures that collectively, rather than individually, represent a potential Heritage Conservation Area; and
- access to public transportation is via the two main roads to the west or to the east.

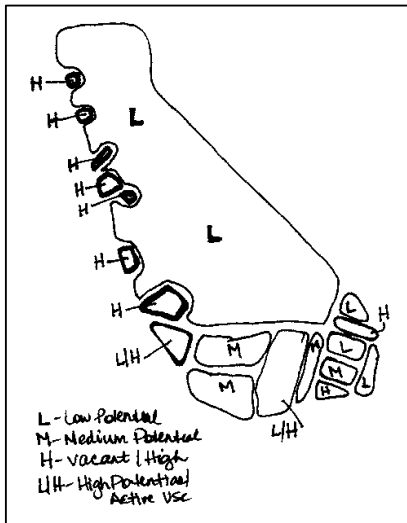
Figure 9: John Tod Neighbourhood Urban Design Analysis



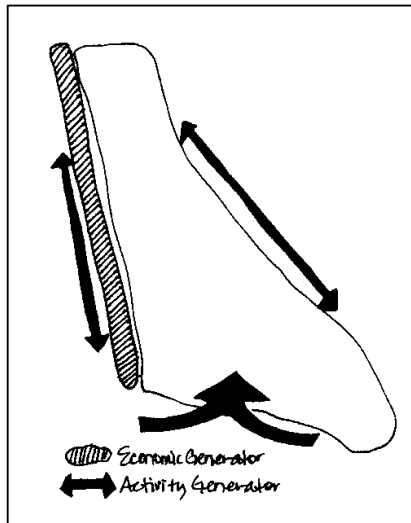
Block Analysis



Building Form Analysis



Economic/Development/  
Redevelopment Potential



Influences/Amenities



Typical homes located within the John  
Tod Neighbourhood;  
Photos: Carla Stewart

# Land Use Policies

## 4.7 JOHN TOD NEIGHBOURHOOD (cont.)

The John Tod Neighbourhood, located between the Schubert Drive Neighbourhood and the McDonald Park neighbourhood, is one of the oldest residential areas on the North Shore. Flanked by the Tranquille Commercial District to the west and one of the busiest arterial roads in the city to the east, the John Tod Neighbourhood has experienced significant social and economic challenges over the past 10-15 years. Although complex socio-economic issues have presented challenges for the John Tod Neighbourhood, the area is supported by an elementary school and two churches. Residents have previously supported a residents' association and a social planning process which culminated in the adoption of the John Tod Neighbourhood Community Development Project Plan. This Plan focused heavily on addressing social issues and was instrumental in starting a volunteer neighbourhood watch program.

Despite the social issues experienced in the area, the John Tod Neighbourhood offers many benefits to residents including affordable housing, flat land, and immediate access to the main commercial area of the North Shore including banks, personal services, retail shopping and unique and very popular ethnic restaurants.

### **ACTION ITEMS:**

- a. Identify appropriate Tranquille Road and Fortune Drive access points from the John Tod Neighbourhood to the McDonald Park Neighbourhood and Schubert Drive Neighbourhood, to ensure those access points provide full mobility function for all citizens, are well marked and lit and function in an efficient manner for pedestrian and cycle movements.

### **SUPPORTING REGULATIONS:**

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

### **POLICY STATEMENTS:**

1. The City will encourage the retention and enhancement of the desirable and existing qualities of the John Tod Neighbourhood including building form and development standards.
2. The City will encourage multiple family development, along the north side of Elm Street to Vernon Avenue, provided that new development achieves an appropriate fit in scale and character with existing residential areas.
3. The City will discourage commercial development along Fortune Drive between Oak Avenue and Leigh Road.
4. The City will encourage and support the provision of pedestrian and cycle connections between residential neighbourhoods particularly across Fortune Drive and Tranquille Road.
5. The City will support the use of green streets designs, where appropriate, within this neighbourhood for new road development and for regularly scheduled maintenance.
6. The City will support alternative forms of housing by permitting garden suites above garages and the conversion of garages into one residential unit, where those garages and garden suites are adjacent to a rear alley and where the garden suite is not subdivided into a separate legal parcel.
7. The City will support examining the creation of a Heritage Conservation Area for the John Tod Neighbourhood to protect and enhance the unique qualities of this area of the North Shore.

# Land Use Policies

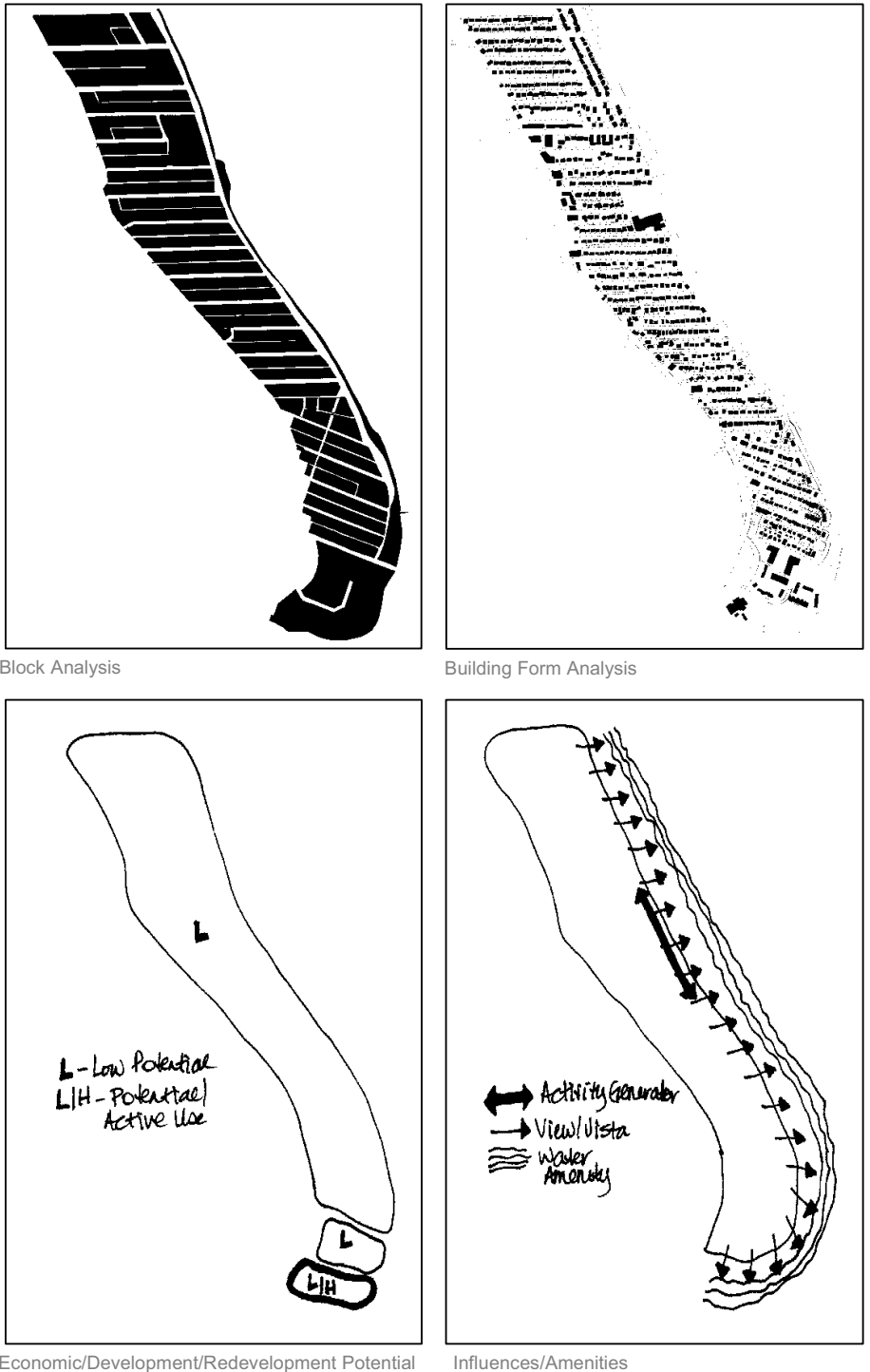
## 4.8 SCHUBERT DRIVE NEIGHBOURHOOD

### Urban Design Analysis

The characteristics identified for the Schubert Drive Neighbourhood area through urban design analysis include the following:

- block pattern analysis reflects linear blocks with a consistent east/west orientation that spans the length between Fortune Drive and Schubert Drive; this layout creates two major transportation corridors to the outside edges; back alleys are abundant throughout the whole neighbourhood;
- the North Thompson River to the east, the view of Mount Peter and Mount Paul, the City Centre and the North Thompson River valley offer access to the beauty surrounding the North Shore;
- built form analysis shows some pockets of multiple family development, particularly along Fortune Drive and at the southern edge of the neighbourhood;
- housing stock in the southern portion is small and among the oldest stock on the North Shore; the typical home is between one to one and a half storeys high;
- two schools exist in this neighbourhood, one having been closed down and used for meeting space by the School District;
- several tot lots exist and are well used by the community;
- the 200 year flood plain is evident for those properties closest to the river and only the southern tip properties are in the 20 and/or 200 year flood plain;
- there are no major economic generators in the area; the only community hall was recently rezoned to permit non-market housing;
- land is flat, easily accessible by pedestrians and cyclists and the neighbourhood has the benefit of easy access to the Rivers Trail which connects to the Overlanders Bridge and to the South Shore;
- access exists to Overlanders Beach, which is heavily used in the summer; extensive mature trees also exist along Schubert Drive; and
- public transportation is available via the arterial road located to the west.

Figure 10: Schubert Drive Neighbourhood Urban Design Analysis



# Land Use Policies

## 4.8 SCHUBERT DRIVE NEIGHBOURHOOD (cont.)

The Schubert Drive Neighbourhood comprises several types of residential buildings and densities and runs most of the length along Schubert Drive, adjacent to the North Thompson River. The southern portion of the neighbourhood represents some of the oldest housing stock on the North Shore and provides affordable housing for young families, singles and seniors. There are pockets of multiple family development throughout, several tot lots, an elementary school and a commercial node in the northwest. One of the city's greatest amenities, the Rivers Trail, runs along the eastern edge, providing the longest stretch of public access to the North Thompson River and to the sunny, sandy Overlanders Beach. Lookouts, habitat trees and spectacular views of the North Thompson, South Thompson, and Mount Peter and Mount Paul are enjoyed by many residents of this neighbourhood.



Schubert Drive; Photos: Carla Stewart

### **ACTION ITEMS:**

- a. Undertake a comprehensive development review of the Henry Grube property to determine appropriate future uses for the site.

### **SUPPORTING REGULATIONS:**

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

### **POLICY STATEMENTS:**

1. The City will encourage the retention and enhancement of the desirable and existing qualities of the Schubert Drive Neighbourhood including building form and development standards by encouraging new development to achieve a good fit in scale and character with the existing housing stock.
2. The City will discourage commercial development along Fortune Drive between Vernon Avenue and the Overlanders Bridge.
3. The City will support the Henry Grube site as a Special Development Area requiring a comprehensive development review prior to any redevelopment approval where the use of the water's edge for public use and enjoyment is retained, where a traffic impact study is undertaken to determine appropriate types of land use and where the use of the Rivers Trail and recreation space is maintained.
4. The City will encourage and support the provision of pedestrian and cycle connections between residential neighbourhoods particularly across Fortune Drive and Tranquille Road.
5. The City will support multiple-family development that is staggered along Fortune Drive, that addresses height and shadow impacts on adjacent properties and that does not provide vehicle access directly from Fortune Drive.
6. The City will support the use of green streets designs in this neighbourhood, where appropriate.
7. The City will support examining the creation of a Heritage Conservation Area for the southern portion of the neighbourhood to protect and enhance the unique qualities of the North Shore.
8. The City will support alternative forms of housing in flood plain areas by permitting garden suites above garages and the conversion of garages into one residential unit, where those garages and garden suites are adjacent to a rear alley and where the garden suite is not subdivided into a separate legal parcel.



# Land Use Policies

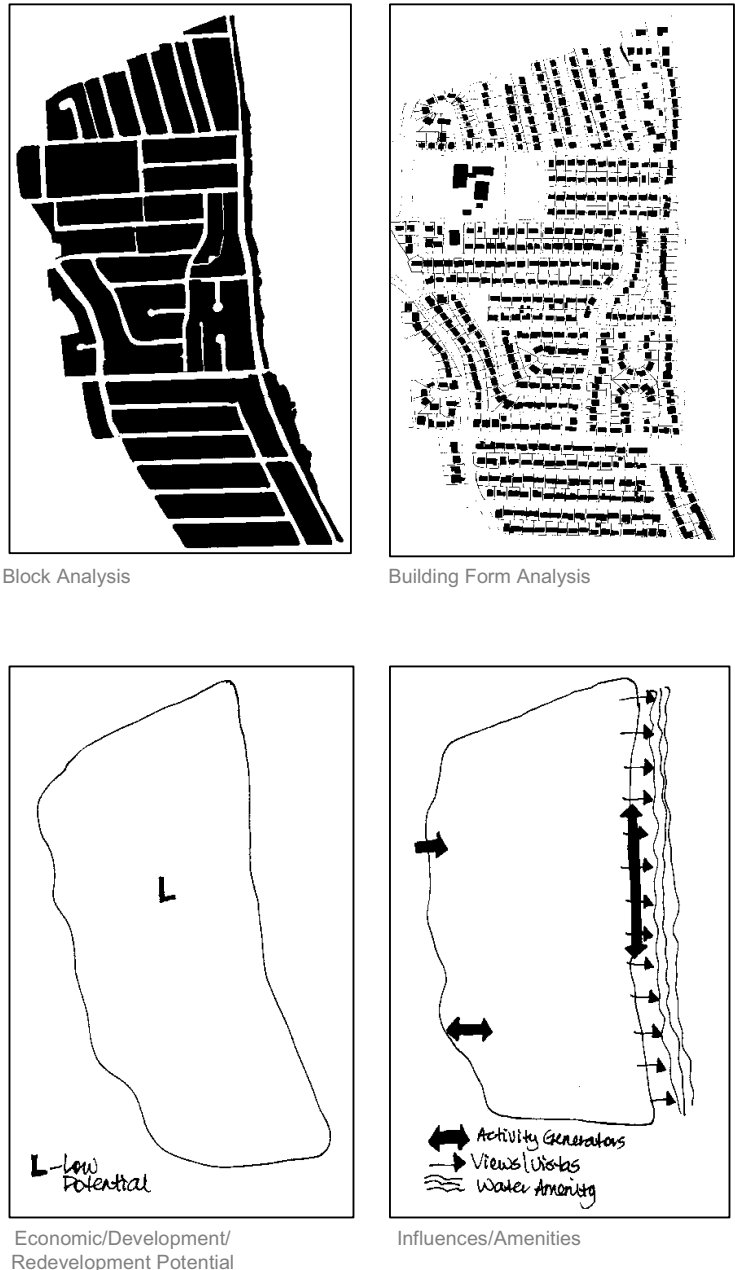
## 4.9 NORTH KAMLOOPS EAST

### Urban Design Analysis

The characteristics for the North Kamloops East area identified through urban design analysis include the following:

- block pattern reflecting an inconsistent east/west and north/south orientation combined with a few cul-de-sacs; area is serviced by many local roads but is flanked by Schubert Drive and the North Thompson River to the east and Fortune Drive/7th Street to the west;
- built form pattern shows average sized lots consisting of a dominance of residential buildings; a few tot lots are scattered throughout the neighbourhood;
- the North Thompson River, the valley to the north and the views of Mount Peter and Mount Paul are spectacular natural features; flat land, a long growing season and high amounts of morning and afternoon sun are key features of the neighbourhood;
- properties fronting Schubert Drive, along the eastern edge of the North Thompson River, are in both the 20 and 200 year flood plains;
- there are no major economic generators for the neighbourhood although influence does exist from the 8th Street corridor to the west;
- there is one elementary school, one active community hall and access to the beach of the North Thompson River; and some streets have a cluster of heritage houses but overall, there are not a lot that exist within the whole neighbourhood.

Figure 11: North Kamloops East Urban Design Analysis



# Land Use Policies

## 4.9 NORTH KAMLOOPS EAST (cont.)

The North Kamloops East Neighbourhood is comprised solely of residential uses with the exception of one community hall and one elementary school. Two tot lots comprise the available green space as does the Rivers Trail extending along the banks of the North Thompson River. This neighbourhood is relatively stable and has larger sized homes compared to other areas along Schubert Drive.



North Kamloops East Neighbourhood;  
Photo: Carla Stewart



Typical homes located within the North Kamloops East Neighbourhood;  
Photos: Carla Stewart

### POLICY STATEMENTS:

1. The City will encourage and support the provision of pedestrian and cycle connections between residential neighbourhoods.
2. The City will encourage the retention and enhancement of the desirable and existing qualities of the North Kamloops East Neighbourhood including building form and development standards.
3. The City will encourage new development to achieve a good fit in scale and character between new and existing residential development.
4. The City will support the use of green streets designs in this neighbourhood, where appropriate, in conjunction with new development and regular maintenance schedules.
5. The City will support the retention of green neighbourhood gathering places and neighbourhood parks and green spaces.
6. The City will support alternative forms of housing in flood prone areas by permitting garden suites above garages and the conversion of garages into one residential unit, where those garages and garden suites are adjacent to a rear alley and where the garden suite is not subdivided into a separate legal parcel.

### ACTION ITEMS:

- a. Work with School District #73 to coordinate with the development and maintenance of neighbourhood park space.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

# Land Use Policies

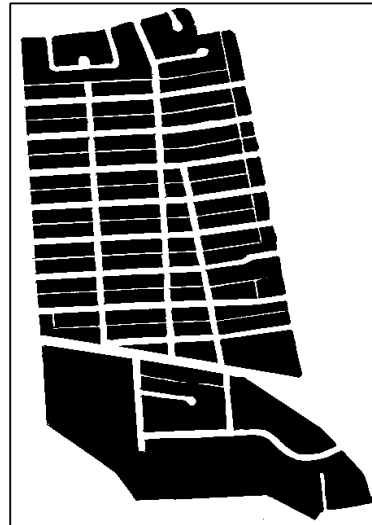
## 4.10 NORTH KAMLOOPS WEST

### Urban Design Analysis

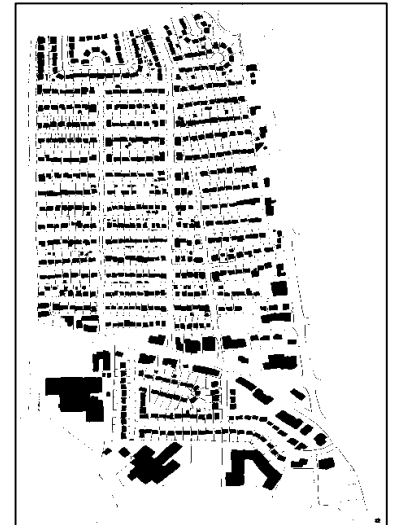
The characteristics identified for the North Kamloops West area through urban design analysis include the following:

- block pattern reflects a traditional grid system with rear alleys and a short block length;
- built form pattern shows mostly a residential neighbourhood with some infill development that took place in the north where cul-de-sacs are located; multiple family and commercial uses exist on the fringes of the neighbourhood to the east and south;
- the southern area of the neighbourhood shows a concentration of community facilities including a high school, elementary school and library;
- many roads service the area and three arterial roads flank the neighbourhood to the north, east and south;
- none of the area is situated within an identified flood plain;
- public transportation is available along the edges and the flat topography accommodates pedestrians and cyclists;
- major economic generators within the area exist along the east and south arterial roads from restaurants, gas stations, and indoor recreation;
- the southern portion of the neighbourhood offers services such as banking, restaurants, retail sales of home-based products, gas stations and auto service repair; this area also has a significant amount of medium density apartments;
- the neighbourhood has pockets of development that very clearly reflect styles of the 1950s, 1960s and the 1970s; some streets have a strong Italian and Japanese style in both the building materials and landscaping treatments; and
- some heritage properties exist but the area is more defined by a larger identity of specific cultural groups.

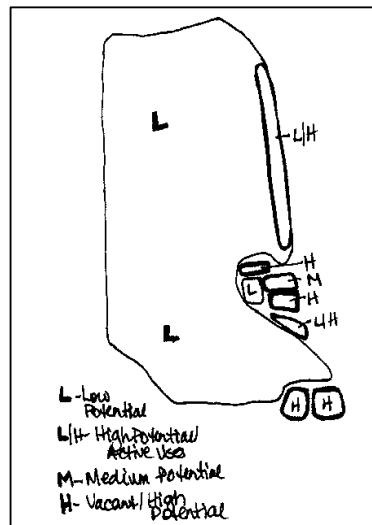
Figure 12: North Kamloops West Urban Design Analysis



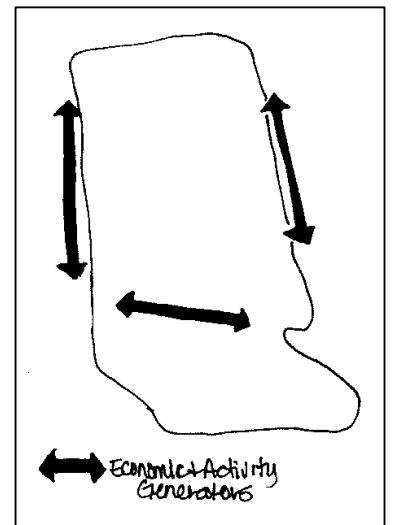
Block Analysis



Building Form Analysis



Economic/Development/Redevelopment Potential



Influences/Amenities

# Land Use Policies

## 4.10 NORTH KAMLOOPS WEST (cont.)

Largely a residential neighbourhood, the North Kamloops West area of the North Shore is sandwiched between two significant economic areas: the small, light industrial park to the west and the 8th Street corridor to the east. The lower portion has some distinct Italian and Japanese cultural indicators in building design, building material and landscaping styles, representing an older housing stock. This area has not seen significant levels of maintenance or redevelopment but continues to offer smaller lots than in Brocklehurst and larger lots compared to the other neighbourhoods of North Kamloops. This area has remained affordable for single parent households, young families and seniors.



Typical home in the North Kamloops West Neighbourhood; Photo: Carla Stewart



Typical duplex in the North Kamloops West Neighbourhood; Photo: Carla Stewart



Typical home in the North Kamloops West Neighbourhood; Photo: Carla Stewart

### POLICY STATEMENTS:

1. The City will encourage and support the provision of pedestrian and cycle connections between residential neighbourhoods.
2. The City will encourage the retention and enhancement of the desirable and existing qualities of the North Kamloops West Neighbourhood including building form and development standards.
3. The City will encourage new development to achieve a good fit in scale and character between new and existing residential development.
4. The City will support the use of green streets designs in this neighbourhood, where appropriate, in conjunction with new development and regular maintenance schedules.
5. The City will support alternative forms of housing by permitting garden suites above garages and the conversion of garages into one residential unit, where those garages and garden suites are adjacent to a rear alley and where the garden suite is not subdivided into a separate legal parcel.
6. The City will support examining the creation of a Heritage Conservation Area for the North Kamloops West neighbourhood to protect and enhance the unique qualities of the North Shore.
7. The City will encourage multiple family development in the area bordered by Tranquille Road, Lethbridge Avenue, Valhalla Drive and 12th Street, provided that new development achieves an appropriate fit in scale with adjacent residential areas.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines



# Land Use Policies

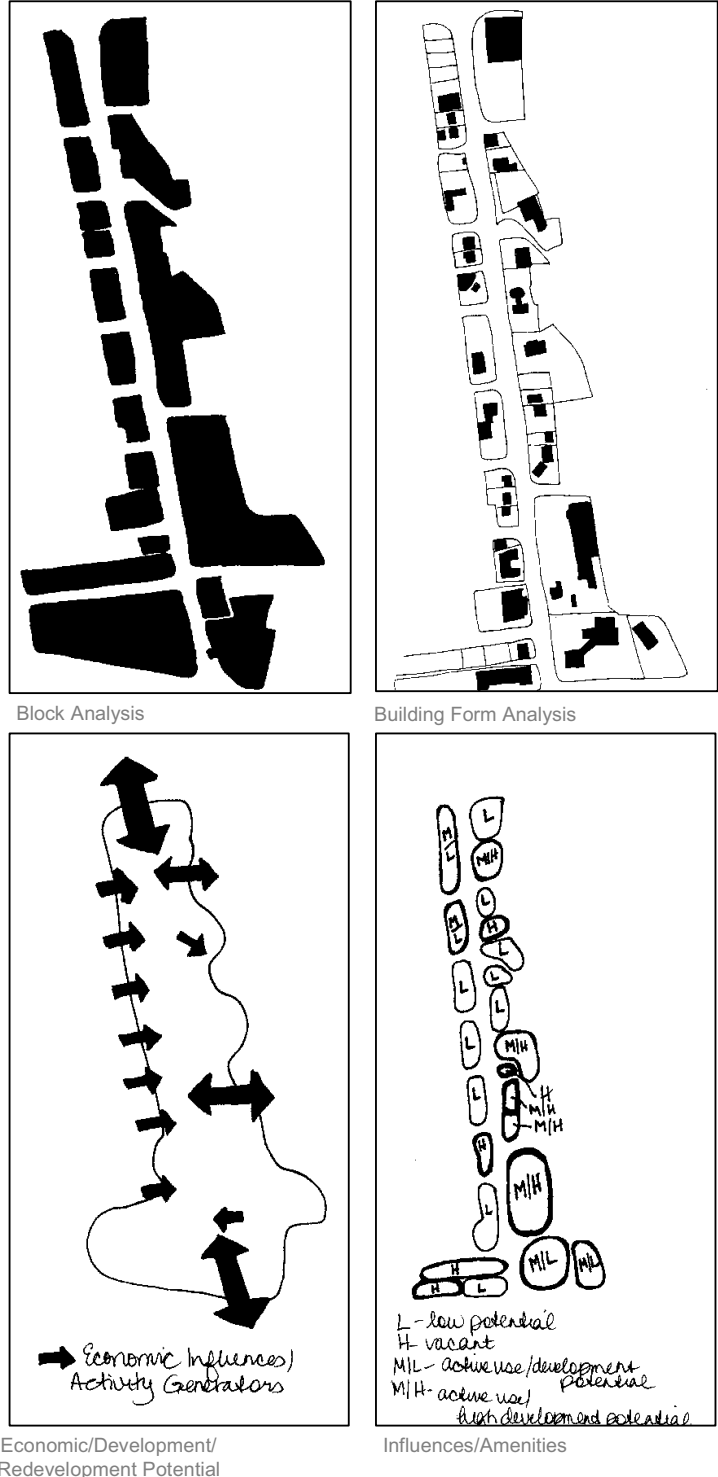
## 4.11 8TH STREET CORRIDOR

### Urban Design Analysis

The characteristics identified for the 8th Street Corridor area through urban design analysis include the following:

- block pattern reflecting a very linear corridor with many access points; actual street layouts are restrictive for left-hand turns but right-hand turns into and away from the corridor are numerous;
- built form pattern showing underdeveloped areas with vacant land or land used for large parking lots; lots are wider than they are deep which requires extra care to site design and building placement; most of the area is serviced by rear alleys;
- corridor is flanked by two residential neighbourhoods; 8th Street acts as a main economic generator in the area;
- none of the properties in the corridor are located in a flood plain;
- there are three churches, an animal shelter, one neighbourhood pub, an indoor recreation facility and several auto-oriented businesses plus a few single family dwellings and multiple family developments located along the strip;
- there are no heritage resources within the area; and
- corridor acts as a significant public transportation route connecting the North Shore to Batchelor Heights, Westsyde, Rayleigh and Heffley Creek.

Figure 13: 8th Street Corridor Urban Design Analysis



# Land Use Policies

## 4.11 8TH STREET CORRIDOR (cont.)

The 8th Street Corridor is a significant access point between the North Shore and Batchelor Heights, Westsyde, the Kamloops Indian Band and nearby Yellowhead Highway. Designed with limited left turn access points, 8th Street has a large, raised and landscaped median which works well to move traffic quickly and efficiently in a north/south direction.

The last land use plan prepared for this street occurred in 1985 with the adoption of the 8th Street Corridor Plan. The policies in that Plan supported a move to service commercial uses, oriented specifically to the automobile. Redevelopment has gradually and inconsistently taken place since the 1980s and has retained that automobile focus.

The North Shore Advisory Committee examined the 8th Street Corridor to determine an appropriate direction for its future. Recognizing that efficient movement of vehicles was a high priority for the function of the street the Committee supported retaining the auto-oriented focus and establishing zoning regulations that would support larger stores similar to the existing Canadian Tire. The Committee also recognized however, that the corridor is a major thoroughfare for pedestrians and therefore also supported implementing higher design standards for both building and site design for new construction. The 8th Street Corridor will continue to orient itself to automobile uses but will incorporate pedestrian elements such as requiring significant landscape buffer areas and having parking lots located away from street frontages.

### **ACTION ITEMS:**

- a. Undertake a transportation and mobility study for the 8th Street Corridor to determine additional east/west connections between the North Kamloops West and East neighbourhoods to create alternatives for transportation modes and to create an efficient and safe community.

### **SUPPORTING REGULATIONS:**

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

### **POLICY STATEMENTS:**

1. The City will encourage automobile-oriented commercial uses to locate along the 8th Street Corridor.
2. The City will encourage ensuring that pedestrian and cycle links and accesses are not compromised along the 8th Street Corridor in conjunction with new development.
3. The City will support the development of the 8th Street Corridor commercial area in an attractive manner and in a way that will enhance the commercial vitality and image of the North Shore.
4. The City will encourage the use of green building standards and environment-friendly storm system designs for new commercial construction along 8th Street.
5. The City will support enhancing inter-neighbourhood mobility and transportation by examining alternative east/west connections and by ensuring that the existing street network in this area is not compromised by additional development in outlying neighbourhoods.



Mock-up of potential Building Massing for 8th Street;  
Source: North Shore Advisory Committee



Potential Building Massing for 8th Street;  
Source: ND Lea Consultants Ltd.

# Land Use Policies

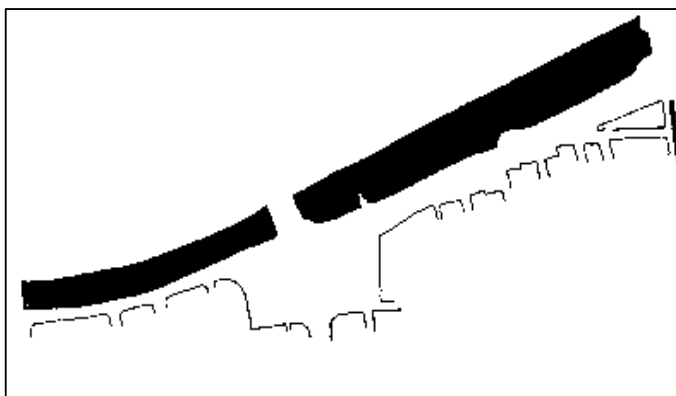
## 4.12 HALSTON ENTRY CORRIDOR

### Urban Design Analysis

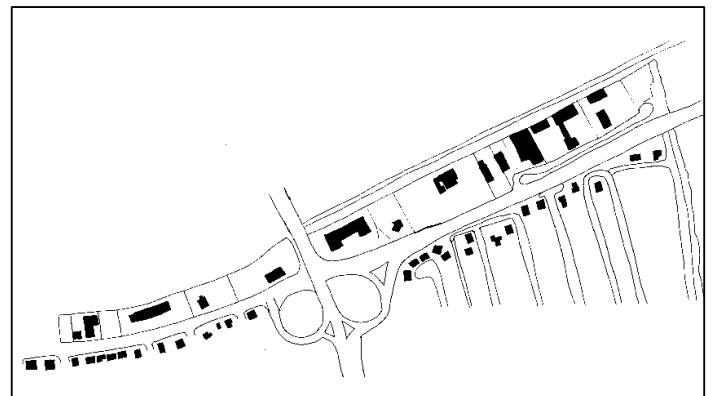
The characteristics identified for the Halston Entry Corridor area through urban design analysis include the following:

- block pattern reflecting large lots serviced by one main road, adjacent to an active rail line and located strategically near the Yellowhead Highway;
- built form pattern showing some underdeveloped areas with small buildings and large storage yards; eastern section is a very active industrial area with one vacant parcel adjacent to the North Thompson River;
- views to the north of the grasslands and to the east of adjacent mountain ranges are prominent;
- portions of the eastern section of Kingston Avenue extend into both the 20 and/or 200 year flood plain;
- represents an economic generator for the area, particularly the eastern section; development potential exists on the western section next to the most recent construction of a non-market apartment building and an indoor entertainment facility; and
- there are no cultural or medical facilities in this area; public transportation is available and the heritage resources are reflected in the natural setting of the grasslands hills and the North Thompson River.

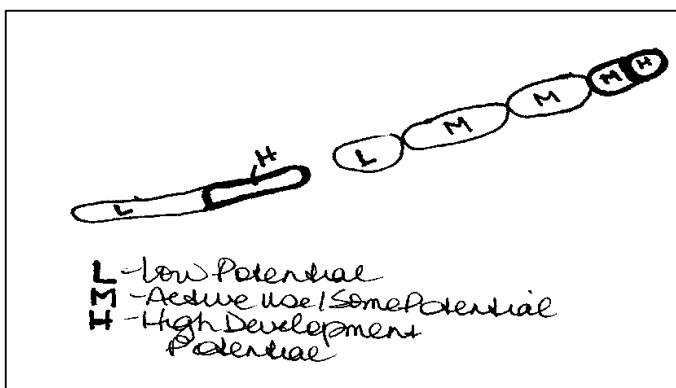
Figure 14: Halston Entry Corridor Urban Design Analysis



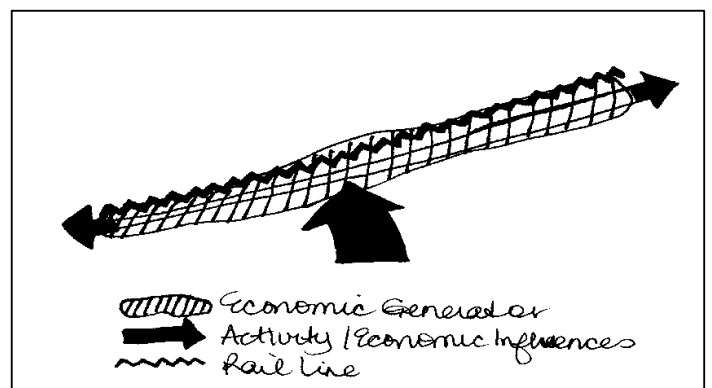
Block Analysis



Building Form Analysis



Economic/Development/Redevelopment Potential



Influences/Amenities

# Land Use Policies

## 4.12 HALSTON ENTRY CORRIDOR (cont.)

The Halston Entry Corridor is the eastern entrance point for the North Shore. Providing access from the Kamloops Indian Band, other areas of Kamloops and traffic using the Yellowhead Highway, the Halston Corridor is an important area providing first impressions of the North Shore. Largely developed with service commercial and industrial uses, the Halston Corridor also has sufficient vacant space and buildings that are ready for redevelopment. Recent construction at the west end of the corridor, with non-market housing and an indoor entertainment centre, have added new life to the area and set a precedent for good building and site design.

The existing industrial uses along Halston Avenue are well established and are set back significantly from the entry corridor by a frontage road and extensive landscaped areas. Updating signage and landscaping in some areas would help project an image of a healthy, vibrant community. Extending Development Permit Guidelines to the Halston Corridor properties would ensure that new construction is of high quality befitting a neighbourhood entry corridor.

### POLICY STATEMENTS:

1. The City will support incorporating features such as landscaping and signage in the road allowance to clearly identify gateway areas.
2. The City will support the development of the Halston Entry Corridor in an attractive manner and in a way that will enhance the commercial vitality and image of the North Shore.
3. The City will support increased retail and higher-end commercial uses to take place along the Halston Entry corridor, west of the 8th Street Interchange.



Halston Entry Areas; Photo: Carla Stewart



Halston Entry Areas; Photo: Carla Stewart

### ACTION ITEMS:

- a. Work with the North Shore Business Improvement Association to install information and identification signage about the North Shore within the Halston Entry Corridor.
- b. Develop a beautification plan for the Halston Entry Corridor, including the Halston Avenue/8th Street intersection.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines



# Land Use Policies

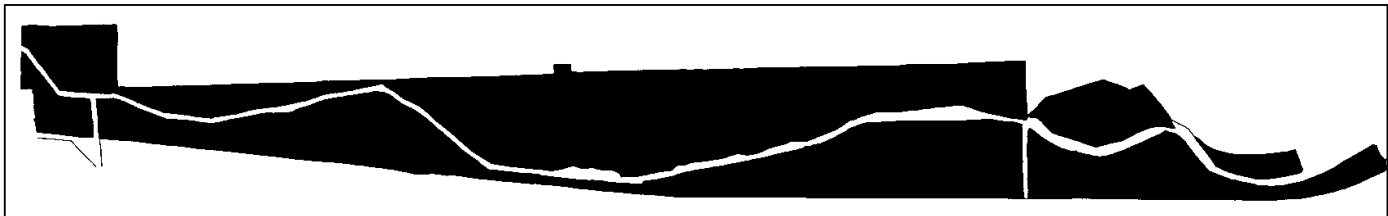
## 4.13 ORD ROAD DISTRICT

### Urban Design Analysis

The area characteristics identified for the Ord Road District area through urban design analysis include the following:

- block pattern analysis shows large lots serviced by one road that is curved in several spots and has no formal pedestrian walkways;
- built pattern shows a clustering of mobile home parks that are developed to a medium density compared to the other residential properties;
- area is characterized by a mix of land uses from residential to heavy industrial and is bound by a rail line to the south and grasslands to the north;
- none of Ord Road falls within a flood plain;
- two very large industrial uses flank the area with other light industrial uses spread unevenly along the road;
- there are no medical facilities in the area; there is one Sikh Temple, a recycling facility, bus maintenance yard, two dog parks and several water retention ponds that are used as soccer pitches;
- only the western half of the road is serviced by public transportation; and
- there are a handful of heritage structures scattered throughout.

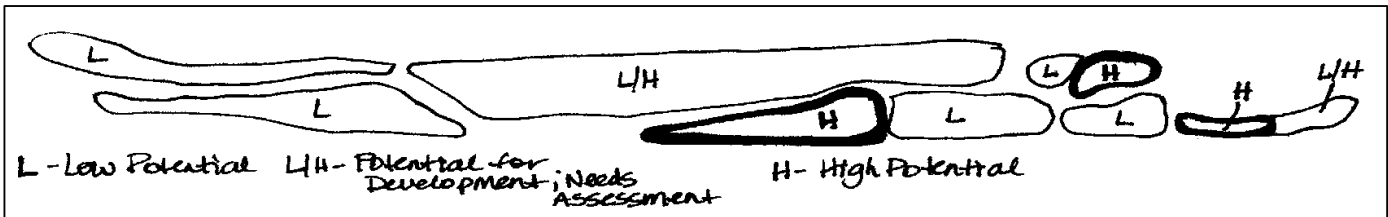
Figure 15: Ord Road District Urban Design Analysis



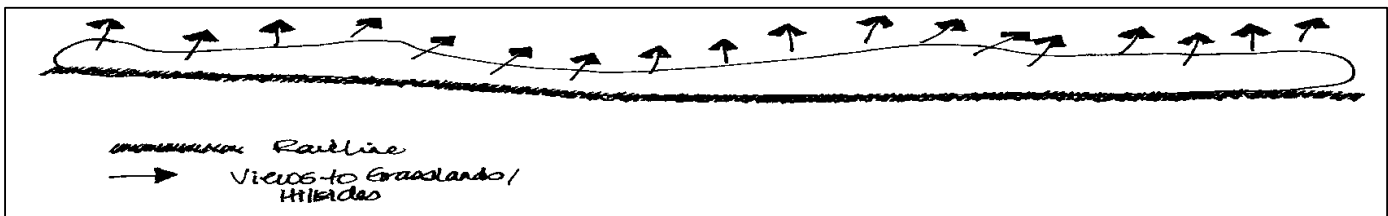
Block Analysis



Building Form Analysis



Economic/Development/Redevelopment Potential



Influences/Amenities

# Land Use Policies

## 4.13 ORD ROAD DISTRICT (cont.)

Comprising a variety of land uses such as single family residential, mobile home parks, scrap metal salvage yards, recycling depots, and small auto-oriented sales, Ord Road has long been an area of transition devoid of one cohesive identity. Ord Road also serves as a designated truck route and was included in a review of the Dangerous Goods Route in 2007 with several recommendations for upgrades proposed before it would be able to physically accommodate dangerous goods traffic. Ord Road also serves as the second east/west connection, besides Tranquille Road, that provides access to the airport and to the agricultural lands beyond.



Industrial Use Along Ord Road;  
Photo: Carla Stewart



Mixed Industrial and Residential Uses  
Along Ord Road;  
Photo: Carla Stewart



Mobile Home Park off Ord Road;  
Photo: Carla Stewart



Service Commercial Uses Along Ord  
Road; Photo: Carla Stewart



Grasslands off of Ord Road;  
Photo: Carla Stewart

### POLICY STATEMENTS:

1. The City will support commercial and industrial uses along Ord Road, the upgrading of Ord Road to accommodate those uses, and when required, will support the use of Development Cost Charges to fund those required road upgrades.
2. The City will support the use of green streets designs along Ord Road, where appropriate and for the purposes of stormwater management only, for new development and for regularly scheduled maintenance.
3. The City will support the development of Ord Road in an attractive manner and in a way that will enhance the vitality and image of the North Shore.
4. The City will support working with the Province, to extend the Lac Du Bois Grasslands Park boundary, in conjunction with a comprehensive development review of the grasslands adjacent to Ord Road.

### ACTION ITEMS:

- a. Identify required road upgrades needed to accommodate further commercial and industrial traffic along Ord Road.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

# Land Use Policies

## 4.13 ORD ROAD DISTRICT (cont.)

The current Official Community Plan designation for the Ord Road area supports urban residential development. Given the potential changes in traffic and the land uses proposed for the airport entry area, Ord Road has become a prime area to support service commercial and light industrial uses rather than expansion of the existing residential. Figure 16: Existing and Proposed Ord Road Land Uses (see below) illustrates the changes in the land use designation for Ord Road.

Figure 16: Existing and Proposed Ord Road Land Uses



# Land Use Policies

## 4.14 BROCKLEHURST

### Urban Design Analysis

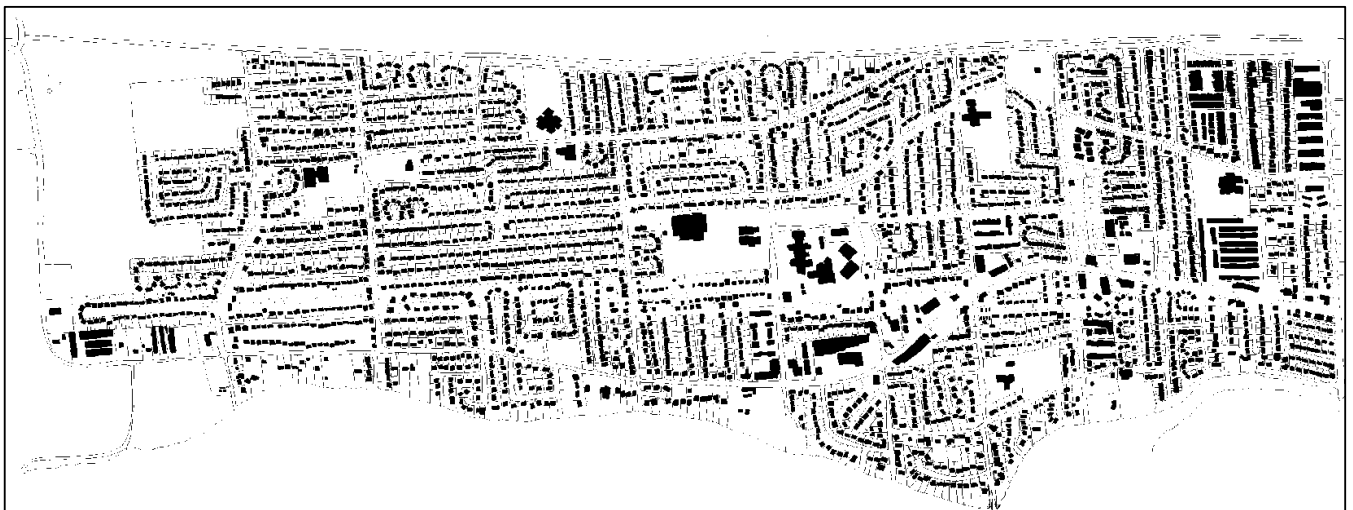
The characteristics identified for the Brocklehurst area through urban design analysis include the following:

- block pattern reflects a quasi-grid system with long, straight roads and interspersed cul-de-sacs with a high level of connectivity throughout the neighbourhood;
- the built form illustrates pockets of commercial, institutional and recreational uses located throughout a large-lot, mixed-density residential neighbourhood;
- neighbourhood characteristics reflect the agricultural history of the community where there are many backyard orchard remnants, large lots, both in width and depth, and mid-size housing with one and a half storeys and 1,500 sq. ft. as the typical housing form;
- lands are bordered by a rail line to the north and the Thompson River to the south; some large estate lots exist along the Thompson River;
- there are some small, underdeveloped infill sites throughout but the neighbourhood is mostly built out;
- the residential areas mostly consist of a Single Family Dwelling style of house even though the area has always allowed for two family densities; and
- the area has flat land throughout with a long growing season and long periods of morning and afternoon sunshine.

Figure 17: Brocklehurst Urban Design Analysis



Block Analysis



Building Form Analysis



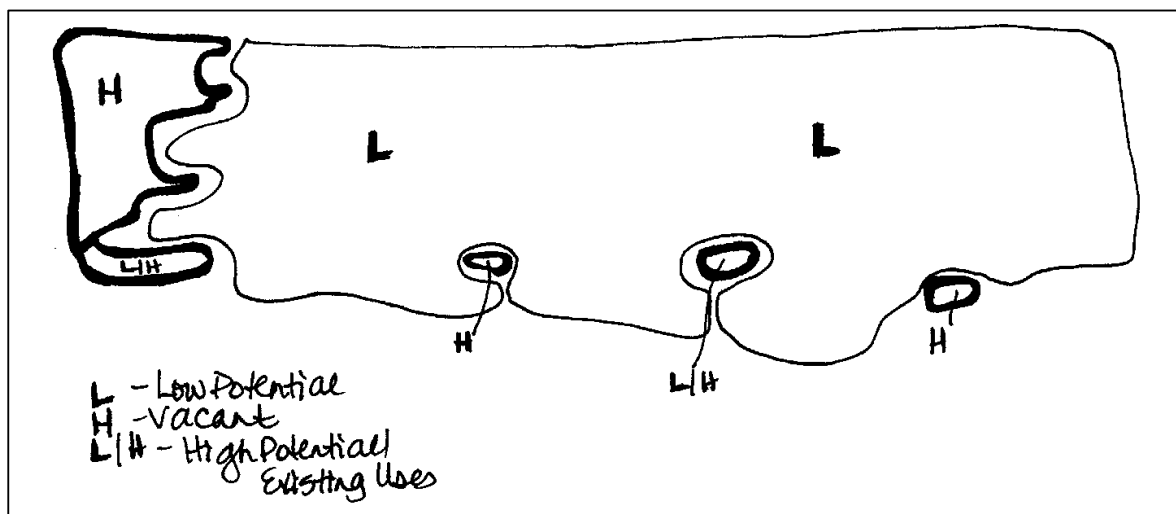
# Land Use Policies

## 4.14 BROCKLEHURST (cont.)

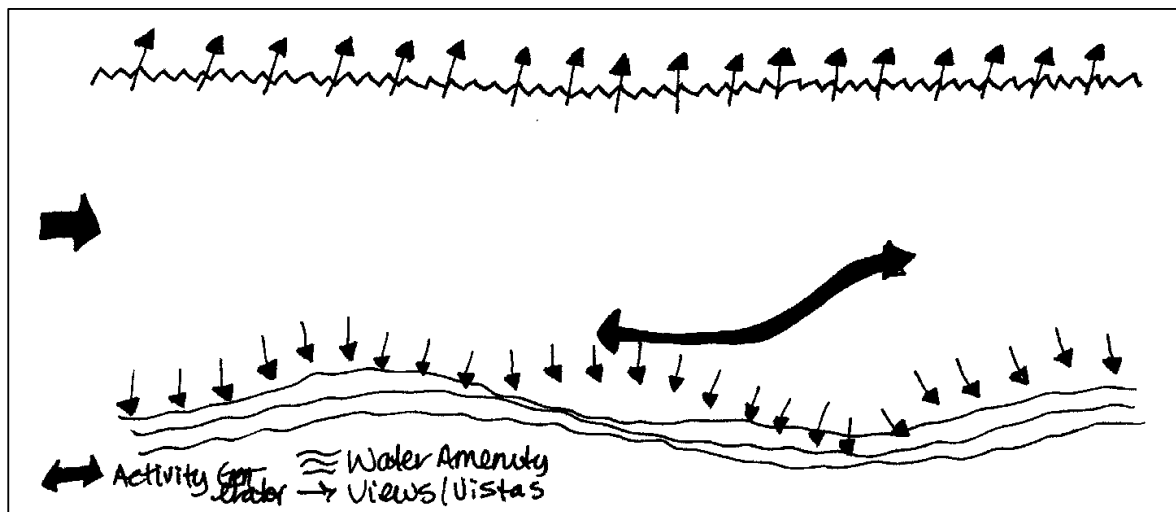
### Urban Design Analysis (cont.)

- vistas of the grasslands to the north are a prominent natural feature; pockets of the 20 and 200 year flood plain extend throughout the area;
- the neighbourhood has a Dangerous Goods Route designation which sees a significant amount of semi-truck trailer activity;
- there are several major economic generators in the area including a shopping centre, light industrial park, pockets of neighbourhood commercial uses, gas stations and neighbourhood pubs;
- there are several schools, recreation centres, ball fields, an outside pool, cycle and pedestrian trails, churches and community halls;
- public transportation routes are extensive throughout the whole neighbourhood and account for one of the highest ridership levels in the city;
- heritage resources are scattered throughout; and
- the area represents one of the most diverse neighbourhoods in Kamloops in terms of housing types provided: single family, duplex, suites, multiple family in varying densities, seniors housing, residential care facilities, non-market housing, condos and apartments.

Figure17: Brocklehurst Urban Design Analysis (cont.)



Economic/Development/Redevelopment Potential



Influences/Amenities

# Land Use Policies

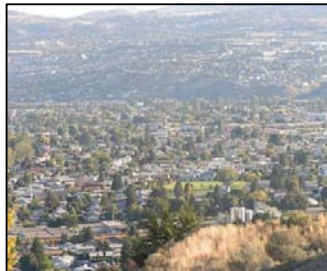
## 4.14 BROCKLEHURST (cont.)

The Township of Brocklehurst amalgamated with the City of Kamloops in 1973 bringing with it a well-established residential neighbourhood with large lots, modest houses and one main commercial centre. The make-up of Brocklehurst has not changed over the past 15 years except for the addition of some recreation sites and some multiple family development.

Through the public consultation phase of this project, the community expressed its support for retaining the character of Brocklehurst while also accommodating infill and higher densities in site specific and site appropriate locations. It is important for the community that the large lots of Brocklehurst be retained which supports its country atmosphere and its connection to its agricultural past. Permitting unique and innovative housing types (for example, garden suites) will help increase densities and support re-using existing infrastructure while also maintaining the large lots. Permitting community gardens and horticulture in Brocklehurst provides income opportunities but is also a way for the neighbourhood to provide food production to the community in an accessible and affordable manner.



New Construction;  
Photo: John Popoff



Brocklehurst Neighbourhood;  
Photo: Carla Stewart



Typical Brocklehurst Home;  
Photo: Carla Stewart



Brocklehurst Streetscape;  
Photo: Carla Stewart

### POLICY STATEMENTS:

1. The City will support the infill of vacant, serviced lots for multiple family where there is direct access to major arterial roads, transit stops and commercial uses within walking distance.
2. The City will support the provision of a broad range of housing types and forms suitable to both owners and renters and/or various household sizes and income levels.
3. The City will support the retention of existing larger lots within Brocklehurst but that this is also supported by the use of secondary suites, horticulture and community gardens.
4. The City will support the use of garden suites in Brocklehurst, on a site specific basis, where those garden suites are proposed for lots larger than the required minimum lot area and where the garden suite is not subdivided into a separate legal parcel.
5. The City will support the development of the Brock Shopping Centre, and adjacent commercial strip along Desmond Street, in an attractive manner and in a way that will enhance the vitality of Brocklehurst and the North Shore.
6. The City will support retaining the use of the water's edge for public enjoyment rather than private development and will, where possible, obtain public access at the river's edge, where it is currently controlled through private ownership.
7. The City will encourage and support the provision of pedestrian and cycle connections between residential neighbourhoods particularly across Tranquille Road.

(cont...)

# Land Use Policies

## 4.14 BROCKLEHURST (cont.)

### POLICY STATEMENTS: (cont.)

8. The City will support neighbourhood design that promotes walking activity to lessen dependence on the automobile, promote human health and strengthen community bonds. This can be achieved by supporting development that creates interconnectivity between existing neighbourhoods, providing a variety of convenient walking routes and by locating public spaces, commercial facilities, schools, places of worship and transit stops so that they are easily accessible by foot from all dwellings.
9. The City will encourage the preservation of existing sources of non-market housing within Brocklehurst including the use of legal secondary suites.
10. The City will support the use of green streets designs for roads upgraded as part of a regular maintenance schedule and for new road development, whereby green streets designs provide for pedestrian pathways, natural drainage systems, additional street trees and vegetation, and traffic calming measures.
11. The City will support creating a dedicated east/west cycle route from Brocklehurst to Schubert Drive and/or from Brocklehurst to McArthur Island, to connect to the Overlanders Bridge, potentially using an on-the-ground marking or identification system.
12. The City will support the Brock Shopping Centre, including the commercial strip abutting directly onto Desmond Street, as a neighbourhood commercial focal point where mixed-use development, in the form of a village district concept, can occur.
13. The City will support building heights of two (2) storeys within the Brocklehurst Development Permit Area with permitted height increases to four (4) storeys when green buildings, green infrastructure, mixed-uses and/or non-market housing has been incorporated into the development.

#### **ACTION ITEMS:**

- a. Dedicate an east/west commuter cycle route from Brocklehurst to Schubert Drive and/or to McArthur Island to create a formalized cycle route to the Overlanders Bridge.
- b. Identify and work toward eliminating any physical or functional barriers that exist that would prevent efficient and convenient cycle travel within and to and from the North Shore.

#### **SUPPORTING REGULATIONS:**

- a. North Shore Zoning Code
- b. North Shore Development Permit Guidelines

# Land Use Policies

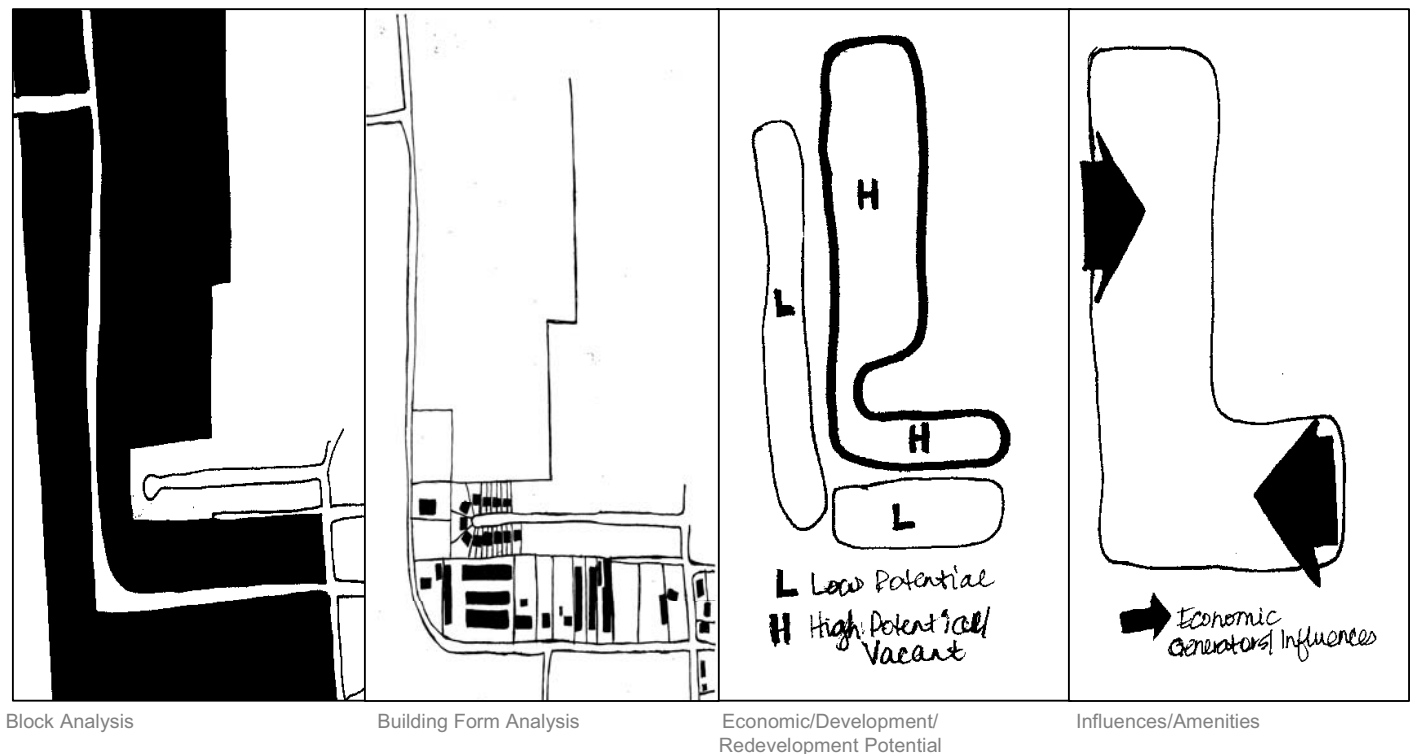
## 4.15 AIRPORT ENTRY CORRIDOR

### Urban Design Analysis

The area characteristics identified for the Airport Entry Corridor area through urban design analysis include the following:

- block pattern reflecting large areas of undeveloped land with some smaller lots located along the arterial road;
- built form pattern shows large tracks of land with some commercial, light industrial and multiple family located in the area where services exist; development along Tranquille Road has been slow due to the lack of some utilities in the area;
- this is the last large greenfield site of the North Shore;
- the whole site falls within the 200 year flood plain, above the 20 year flood plain mark;
- light industrial uses are the major economic generators in the area but are situated on unsightly premises and are appropriate for redevelopment;
- large vacant brownfield parcels also exist near the eastern edge of the site area;
- the whole area is serviced by public transportation; and
- there are no identifiable heritage resources but there is access to the Thompson River and to an underutilized ball field to the south.

Figure 18: Airport Entry Corridor Urban Design Analysis





# Land Use Policies

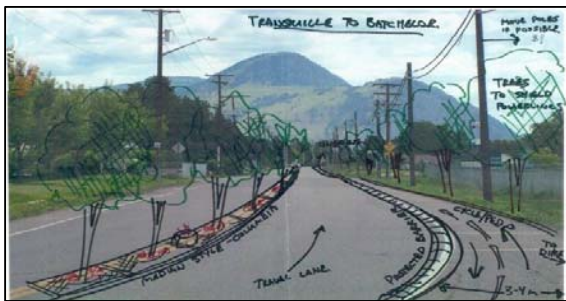
## 4.15 AIRPORT ENTRY CORRIDOR (cont.)

The Airport Entry Corridor represents a significant gateway to the North Shore from the airport and the agricultural lands and proposed destination resort to the west. Ensuring the land uses and public spaces in this area reflect the pride and diversity of the North Shore are important for visitors and for residents.

The Kamloops Airport Land Use and Development Plan identifies commercial and light industrial uses as appropriate for this area. Due to the significance of the entry corridor, the lack of servicing, the restrictions imposed by Nav Canada on uses and the need to ensure adequate housing is made available for Kamloops, the area along the west and north of Tranquille Road is now recommended to also include additional multiple family residential.



Tranquille Road Gateway; Photo: Carla Stewart



Tranquille Gateway Improvements;  
Source: North Shore Advisory Committee



Tranquille Gateway Improvements; Source: ND Lea

### POLICY STATEMENTS:

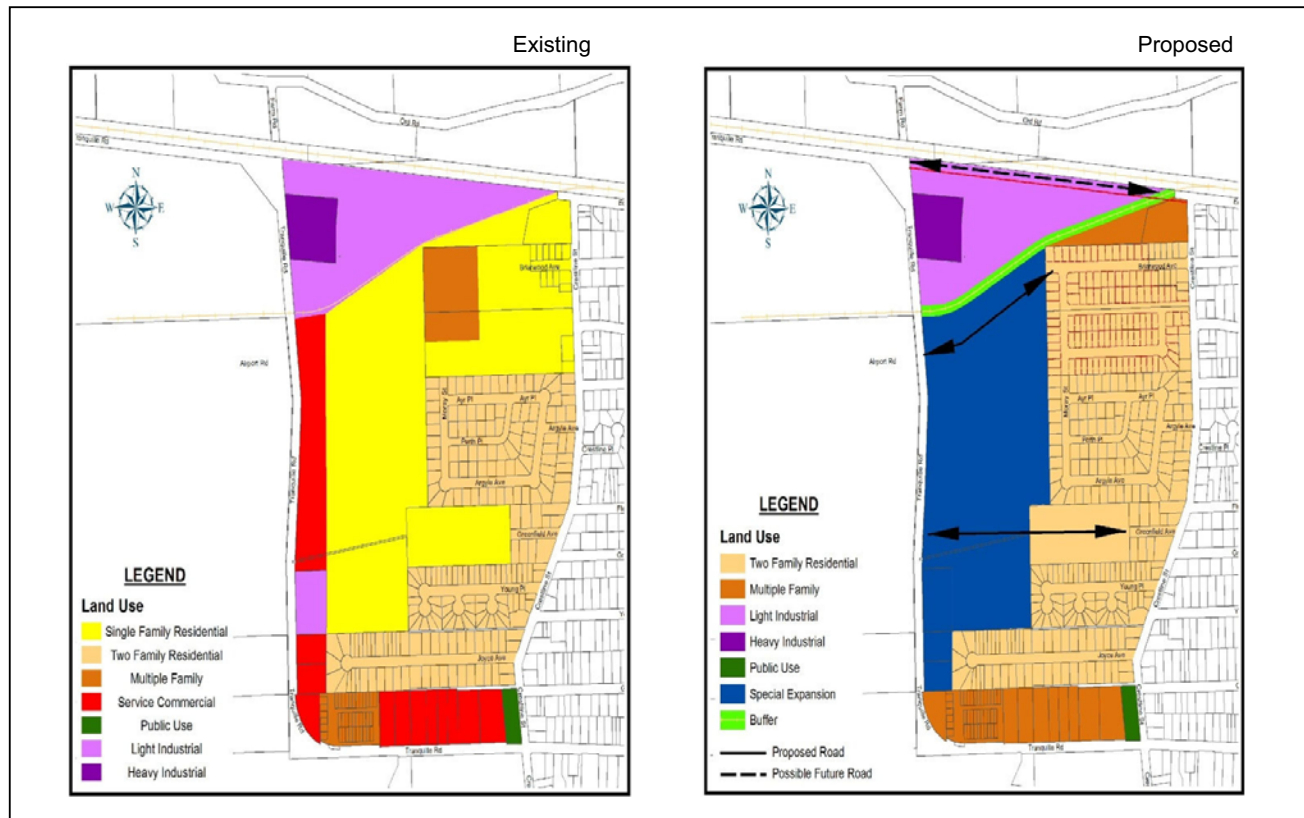
1. The City will encourage upgrading the Airport Entry Corridor to the North Shore by supporting development that incorporates architectural, landscaping and other features that emphasize the unique nature of the community.
2. The City will support changing the land uses on the southern portion of Tranquille Road, north of Aviation Way, to include multiple family residential.
3. The City will support incorporating features such as landscaping and signage in the road allowance to clearly identify the Airport Entry Corridor
4. The City will support the role of the Kamloops Airport as a gateway and entry point to Kamloops by supporting development of the Airport Entry Corridor in a way that will enhance the vitality of the North Shore.
5. The City will support multi-family development within the Airport Entry Corridor area that does not conflict with the Obstacle Limitation Surfaces established by Transport Canada.
6. The City will support the Airport Entry Corridor as a Special Development Area requiring a comprehensive development review prior to development approval, where the review shall address the following: including a mix of residential, commercial and light industrial uses (preference for airport uses); orienting residential uses adjacent to existing residential areas; including a minimum of a 10 m buffer strip or a roadway, between any residential and industrial uses; conducting a comprehensive servicing plan for both storm sewer and water services; including a road network pattern providing a minimum of two road connections through the area to Tranquille Road (see Figure 19); and including a pedestrian buffer strip along the railway spur line.

# Land Use Policies

## 4.15 AIRPORT ENTRY CORRIDOR (cont.)

The Existing and Proposed Airport Entry Land Uses Map (shown below) illustrates the area of the Airport Entry Corridor that is designated as a Special Development Area, requiring a comprehensive development review prior to any development taking place. This area is also subject to additional British Columbia Building Code restrictions due to noise impacts from the adjacent airport (see p. 70, Specialized Development Standards, for more detailed information).

Figure 19: Existing and Proposed Airport Entry Land Uses



Airport Entry Corridor; Photo: Carla Stewart



Vacant Airport Entry Lands; Photo: Carla Stewart



Vacant Airport Entry Lands; Photo: Carla Stewart

# Land Use Policies

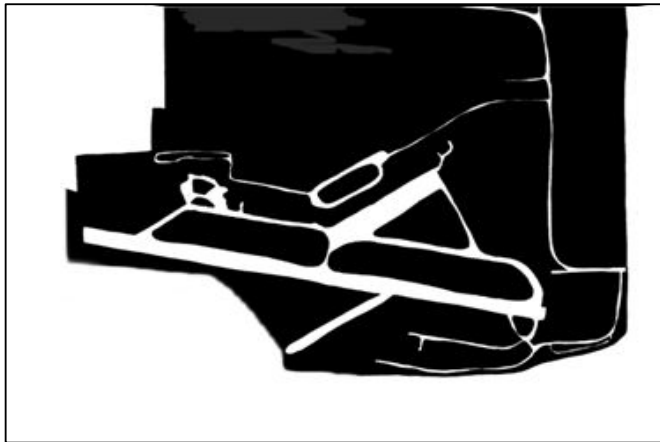
## 4.16 AIRPORT LANDS

### Urban Design Analysis

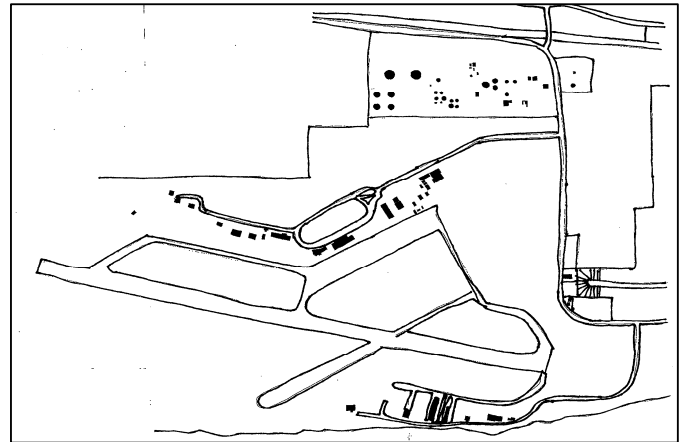
The area characteristics identified for the Airport Lands area through urban design analysis include the following:

- block pattern that shows one main property with an infrastructure base to service air transportation;
- built form pattern shows few structures throughout the site, particularly in the areas designated for industrial and airport-related uses;
- dominance of the Petro-Canada tank farm north of the airport is shown;
- there is one road servicing the property with many internal roads;
- lands are bordered by the Thompson River on the south and agricultural lands to the west;
- the area is also accessed by a rail line, which is used extensively by the tank farm;
- the airport represents a major gateway to the North Shore and Kamloops, particularly for those heading to Sun Peaks, British Columbia's second largest ski resort;
- 200 year flood plain does not extend into the site with overland protection provided by a dyke system; the 20 year flood plain extends into the whole airport lands area;
- the major economic generator is the airport with some airport-related businesses operating on-site; the Petro-Canada tank farm will soon be the only one operating within Kamloops representing a significant economic generator for the area;
- extensive land exists for industrial development on the airport lands; and
- the site is also used by Provincial forestry services as the base for Interior operations of forest fire fighting.

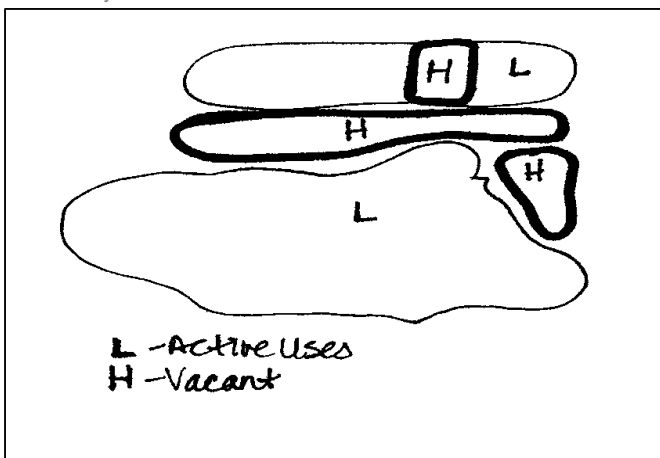
Figure 20: Airport Lands Urban Design Analysis



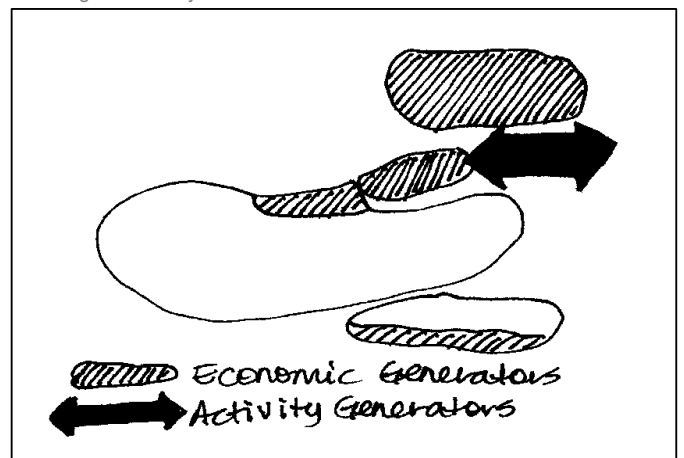
Block Analysis



Building Form Analysis



Economic/Development/Redevelopment Potential



Influences/Amenities

# Land Use Policies

## 4.16 AIRPORT LANDS (cont.)

In September 2000, Urban Systems Ltd. completed the Area Land Use and Development Plan for the Kamloops Airport. Identified in this Land Use Plan were two key recommendations:

- Development along Airport Road and Tranquille Road should occur in such a way so as to reflect the gateway role the airport plays for Kamloops and surrounding regions and should create a good first impression and provide a positive image for the city; and
- Land uses must promote a fully functional airport, with new development complying with regulations established by the Airport Operator and by NAV Canada.

These concepts were also identified in the planning process of the North Shore Neighbourhood Plan as key elements in the development of the Airport Lands.

### POLICY STATEMENTS:

1. The City will support commercial and industrial uses along Ord Road, and in conjunction with the commercial and industrial lands located at the Kamloops Airport, will support the upgrading of Ord Road to accommodate these uses and when required, will support the use of Development Cost Charges to fund those required road upgrades.
2. The City will support the use of the Rivers Trail and dyke system on the Airport Lands for continued use as a recreational area.
3. The City will support that portion of the Airport Lands, located south of the identified runway extension (see Project Policy Areas Map, page 76), to be used for foraging, as part of its agricultural capabilities.



Airport Control Tower;  
Photo: Carla Stewart



Airport Entry Sign;  
Photo: Carla Stewart



Airport Industrial Business;  
Photo: Carla Stewart



Industrial Tank Farm;  
Photo: Carla Stewart

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code
- b. North Shore Development Permit Guideline



# Land Use Policies

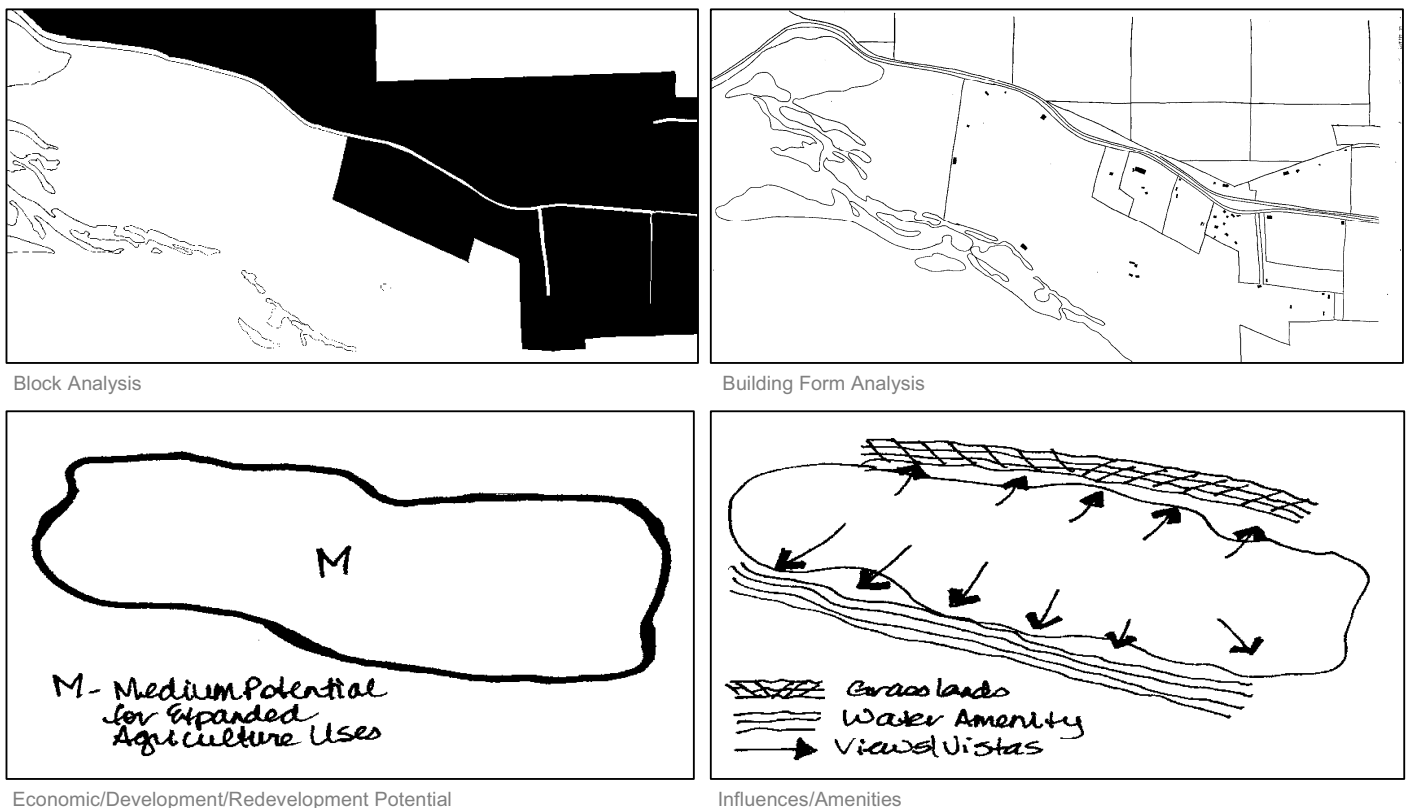
## 4.17 AGRICULTURAL LANDS

### Urban Design Analysis

The area characteristics identified for the Agricultural Lands area through urban design analysis include the following:

- block pattern reflecting large tracks of land serviced by one road and a built form pattern showing a small number of buildings which is consistent with the agricultural land use;
- one road servicing the area is used by large commercial/logging trucks, vehicles and cyclists;
- portions are within the Agricultural Land Reserve, a provincial designation where farming is encouraged for the benefit of agriculture;
- lands are bordered by the Thompson River on the south and a mountain range to the north; both natural features are used as public recreation areas with many informal access points; a bird sanctuary exists at the far west of the area;
- just west of the project area's boundary, and located at the western border of Kamloops, is private property under proposal for a 1,000 unit resort with commercial and recreational uses; access to the rest of Kamloops is only available along the road servicing the agricultural lands; development of this property will occur separately from the North Shore Plan's policies;
- a 200 year flood plain expands into the southern portion of the district;
- the major economic generator for the area is characterized by active farming operations; one golf course operates on the east end;
- there are no commercial uses within the area nor are there cultural facilities, medical facilities or public transportation opportunities; and
- heritage resources are reflected in the natural features, the river, the mountain ranges and the agricultural uses rather than a focus on built form.

Figure 21: Agricultural Lands Urban Design Analysis



# Land Use Policies

## 4.17 AGRICULTURAL LANDS (cont.)

Agricultural production is deeply embedded into the roots of the North Shore. From the early years of the BC Fruitlands Co. and the regional orchard production, to the summer sales of bulk fruit from backyard crops, the North Shore continues to align with its agricultural roots. Although most of the original agricultural lands have been developed into residential areas, a sizable portion of productive agricultural land remains in operation west of the airport. The North Shore community supports expanding the agricultural uses in this area including encouraging the expansion of the agriculture research and development uses already taking place.



Federal Agricultural Research Station; Photo: Carla Stewart



Active Agricultural Land; Photo: Carla Stewart



Vistas Adjacent to Agricultural Lands; Photo: Carla Stewart



Cinnamon Ridge Composting Site; Photo: Carla Stewart

### POLICY STATEMENTS:

1. The City will support the continuation and expansion of agricultural uses and agricultural research within the Agricultural Land Reserve.
2. The City recognizes that agriculture makes a significant and direct financial contribution to the economies of the North Shore, Kamloops and the region and will enhance that economic contribution by supporting agri-tourism opportunities and businesses.
3. The City will support agricultural production that is not detrimental to surrounding ecosystems, water supply and potential environmental impacts.
4. The City will work to enhance access to the natural recreation areas within this district providing for the continued welfare of the citizens of Kamloops, by working with agriculture operators and the Agriculture Land Commission to ensure that any negative impacts arising from the public access is mitigated (e.g. ensuring proper fencing, signage and litter bins are provided).

### ACTION ITEMS:

- a. Conduct a transportation study to determine if there are any safety gaps for cyclists, pedestrians and farm machinery along the main access road through the Agricultural Lands district.

### SUPPORTING REGULATIONS:

- a. North Shore Zoning Code

# Land Use Policies

# Implementation



# Implementation

## 5.0 INTRODUCTION

The success of the North Shore Neighbourhood Plan depends on the implementation of its policies with support from City Council, City administration, all City departments, the development industry, the non-profit sector, other levels of government, representative agencies, and the citizens and business community of the North Shore. Each member of these groups is a key stakeholder in the North Shore and play vital roles in the future of this dynamic and diverse community.

The North Shore Neighbourhood Plan represents a shift in a new direction of development standards and community expectations for the future of Kamloops. Directly supported by the citizens of the North Shore, the implementation methods and policies supported in this Plan are intended to not only showcase the unique qualities of the North Shore but were established to create more sustainable, environmentally and community-friendly development that speaks to the values and desires of the people who live, work and play there.

Successful implementation of the North Shore Neighbourhood Plan also relies on supporting new regulations and guidelines for development specific to the North Shore. These guidelines and zoning specifications work collectively to establish development criteria using innovative and progressive regulatory techniques. These techniques are also supported and encouraged in many Canadian, North American and European cities as a means to promoting smart, sustainable and community-friendly development.

The new guidelines and zoning regulations for the North Shore support urban design techniques, CPTED principles, sustainable development practices, land use classifications and site layouts that work together to not only create a city but that aid development in making a connection to the community within which it takes place.

Identifying a Capital Projects List for the North Shore helps the City prepare reasonable budgets and can set timelines for the completion of upgrades to public spaces. Providing incentives and new regulations to the development community allows the private sector to demonstrate its ability to provide high-quality, meaningful and sustainable development and establishes opportunities for partnerships and collaboration, not only between government and the development industry, but also with the community that lives and interacts with the final product on a daily basis.

North Shore Entry Corridor;  
Photo: Carla Stewart



# Implementation

## 5.1 DESIGN GUIDELINES

The use of design guidelines in urban area redevelopment is a key element in supporting a community's needs. The design guidelines established for the North Shore reflect the diverse and unique qualities of the community and support high-quality, functional, safe and sustainable building and site design.

Design guidelines are important tools that not only provide guidance to City staff but more specifically, provide the development community a glimpse into the values that form and support a desired future. That desired future is expressed in the Vision, Goals and Principles of this document and are reflected in the North Shore Neighbourhood Plan Design Guidelines attached to this Plan as Appendix "A".

Area specific guidelines established for unique areas of the North Shore were created to coincide with these Design Guidelines and include: Tranquille South, Tranquille Commercial District, Brock Shopping Centre, Ord Road, North Shore Towne Centre, 8th Street Corridor, Halston Entry Corridor and the Airport Entry Corridor. General Development Permit guidelines for all other commercial, institutional and industrial areas have also been developed for the North Shore.

Each Development Permit Area supports design guidelines that reflect the unique character of each area, both in terms of form and in function, and are intended to be used to enhance new development and provide developers an opportunity to demonstrate progressive, innovative, safe and sustainable building and site designs.



Design Guidelines help to create places for people and appropriate building design;  
Photos: Carla Stewart

### POLICY STATEMENTS:

1. The City will support the use of the North Shore Neighbourhood Plan Design Guidelines and the creation of new Development Permit Areas identified in Appendix "A".
2. The City, through the use of the North Shore Design Guidelines, will encourage design diversity in new development, redevelopment and sign design to support the characteristic qualities that reflect the North Shore's historic and cultural qualities.
3. The City will encourage and support development that occurs with a logical extension of expansion of services to ensure contiguous growth and to maximize utilization of costly services.
4. The City will support and encourage infill development to increase efficiency of existing services.
5. The City will encourage and support private efforts to rehabilitate and/or redevelop property which will have a positive impact on the stability and growth of the community.
6. The City will support the stabilization and improvement of property and business tax bases in residential, commercial and employment area by upgrading municipal services and thereby stimulating private investment.



A lack of Design Guidelines create places for automobiles which do not welcome pedestrian activity;  
Photo: Carla Stewart



# Implementation

## 5.2 NORTH SHORE ZONING CODE

The North Shore is a highly urban, complex community comprising several neighbourhoods each with a unique form and identity. The North Shore is also the most diverse community within Kamloops and has gone over 40 years without its own neighbourhood plan establishing policy direction and supportive regulatory frameworks.

In order to support the new policies in the North Shore Neighbourhood Plan, a new zoning by-law is needed. The new North Shore Zoning Code provides an opportunity for the North Shore to start new and to have the benefit of using zoning regulations that are directly connected to new policy instead of trying to implement a myriad of changes to the existing regulations to accommodate the North Shore Neighbourhood Plan's policies. Using a new zoning code therefore, allows for immediate implementation of the adopted policies and places every property, within the project area, on a level and even playing field. The new Zoning Code has been designed to allow for flexibility in land uses and development to promote economic diversity and stability. The new zoning regulations also allow for development that is appropriate to the community, is environmentally sustainable, is progressive and innovative and that directly reflects the will of the community in how they wish to see their neighbourhoods and commercial areas evolve over time.

The North Shore Zoning Code is attached to the North Shore Neighbourhood Plan as Attachment "B" and is also cross referenced throughout the document, making a direct connection between policy and regulation.

### POLICY STATEMENTS:

1. The City will support the use of the North Shore Zoning Code, a separate zoning by-law for the North Shore, to provide specific uses and regulations consistent with the policies in this Plan.
2. The City will support new zoning regulations that encourage development patterns and designs consistent with existing neighbourhood character and that promotes, enhances and celebrates the small-town scale of existing development.



New Residential Construction; Photo: John Popoff



Infill Development; Photo: John Popoff

# Implementation

## 5.3 IMPLEMENTATION BARRIERS

Many excellent land use plans have been adopted by City Councils across Canada only to become ineffective and unused shortly thereafter. Identifying some of the barriers to successful implementation of a plan early on can help reduce or address the issues and gaps that ultimately can derail the best planning policy. This section looks to identify some of the barriers to implementation and offers solutions as a way to potentially deal with them.

Table 5: Barriers to Implementation

Implementation Barrier	Solutions to Successful Implementation
A lack of staff resources to support policy requirements.	Ensure sufficient staff resources and/or education to review and approve CPTED designs for all commercial applications. Support use of Development Checklist to provide staff with more information from the proponent on how the development fits with the community's vision and goals.
A lack of appropriate regulations, design guidelines and zoning to support and work in conjunction with the policies.	Support the new North Shore Zoning Code, a zoning by-law specifically tailored to the policies prepared for the North Shore Neighbourhood Plan.
Significant pressure on meeting development time lines when municipal approvals are required prior to proceeding with construction.	Support approving Delegated Authority for staff to approve and issue minor Development Permits to reduce processing times of these applications.
A lack of coordination and communication between all City departments in implementing changes, upgrades and maintenance.	Support the designation of a Sustainability Officer to coordinate the sustainable development policies of the North Shore Neighbourhood Plan between the Development and Engineering Services Department and other City departments.  Support the creation of a Friends of the North Shore to provide a venue for the City to connect with the community when working through public space upgrades, development proposals and street improvements.
A lack of understanding of the importance of supporting policy direction particularly with controversial development proposals.	Support the policies in the North Shore Neighbourhood Plan as the preferred direction for the community and only support OCP amendments when it can be demonstrated that the proposal creates direct benefits to the community.  Provide educational workshops to all new City Councils about CPTED, good urban design and sustainable landscapes in order to support healthy community goals.
Limitations on the capacity of the existing water, sewer and road infrastructure systems to be able to accommodate future growth.	Support compact communities with pedestrian-oriented developments with mixed commercial and residential uses. Support alternative road standards such as green streets designs. Support use of water meters, and other water conservation, to reduce current water consumption levels.
A lack of pre-planning and analysis of the effects to a neighbourhood prior to approving development proposals.	Support long range planning analysis and projects to ensure that appropriate time has been given to examining complex urban issues.  Support the concept that sustainability requires decision making that may not result in immediate gain.



# Implementation

## 5.3 IMPLEMENTATION BARRIERS (cont.)

### POLICY STATEMENTS:

1. The City will support the use of 'Delegated Authority' by providing City staff the ability to approve Development Permit proposals, up to \$250,000 in construction value, to significantly reduce the processing times for Development Permits and to create the ability for City staff to negotiate directly with development proponents.
2. The City will support conducting an annual review of the North Shore Neighbourhood Plan, at the end of each year, to identify any necessary amendments that are needed to ensure the fluid and consistent use and implementation of the policies embedded in the Plan.
3. The City will support the use of a Development Checklist (see Appendix "B") to show that the applicant has properly considered the impact a development proposal may have on an existing community and when proposed in accordance with the vision, goals and principles of the North Shore Plan, will help speed up the planning application process by providing City staff and City Council with a clear statement of the design, landscaping and siting implications of the proposed development.
4. The City will support the designation of a Sustainability Officer to coordinate the sustainable development policies of the North Shore Neighbourhood Plan between the Development and Engineering Services Department and other City departments.
5. The City will support the creation of a Friends of the North Shore group to provide a venue for the City to connect with the North Shore community when working through public space upgrades, development proposals and streets improvements.

**CITY OF KAMLOOPS  
NORTH SHORE NEIGHBOURHOOD PLAN  
QUESTIONNAIRE**

The purpose of this questionnaire is to gather information about the issues that are most important to the residents, business owners, and property owners of the North Shore. Your answers matter. The results of this survey will help the City Council make decisions about the future of the North Shore. Please take the time to complete this questionnaire and return it to the City of Kamloops by the deadline of September 30, 2015. Thank you.

**SECTION I: QUALITY OF LIFE**

The purpose of this section is to gather information about issues that affect your quality of life such as access to recreation, shopping and transportation.

1. Which of the following recreation areas do you use?

	Frequently	Occasionally	Never
1. Markham Island	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. South Fork Kamloops River (e.g. Tut Lake)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Kamloops River Trail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Lee O'Brien's Grassland Provincial Park	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. Old Road Day Park	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6. Chandler Beach	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7. Old Road Recreation Site	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
8. Other	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

2. List the three greatest assets (best things) about the North Shore.

1. \_\_\_\_\_

2. \_\_\_\_\_

3. \_\_\_\_\_

3. List the three greatest challenges (worst things) about the North Shore.

1. \_\_\_\_\_

2. \_\_\_\_\_

3. \_\_\_\_\_

4. Would the addition of the following amenities on the North Shore improve your quality of life?

	Improve Quality of Life	Worsen Quality of Life	Neither Improve or Worsen Quality of Life
1. More public transportation options	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. More small, open in my neighbourhood (e.g. playground, park, etc.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. More opportunities for active recreation	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. More shopping opportunities	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

5. Should the North Shore Neighbourhood Plan encourage the conservation of heritage homes and resources on the North Shore?

	Yes	No	Undecided
1. Yes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Comments:

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

North Shore Neighbourhood Plan Public Input Survey;  
Source: Carla Stewart

### ACTION ITEMS:

- a. Prepare a new Development Cost Charge By-law incorporating reductions for development that uses sustainable building and site construction and design methods.

# Implementation

## 5.4 CAPITAL PROJECT PRIORITIES

The North Shore comprises approximately 2,200 hectares of land representing the largest urban neighbourhood within the city of Kamloops. Providing services, upgrades to infrastructure, installing new public amenities and maintaining levels of existing services is highly complex and expensive. To help set budget priorities and time line estimates, a Capital Project List (see Appendix "C") has been prepared for the North Shore. Costs shown on this list can vary over time and, in many cases, will need refinement at the project stage.

The preparation of the Capital Project List is not intended to provide absolutes but rather to guide Council and budget decisions to proceed with public space and infrastructure improvements in a time appropriate and effective manner. This list is intended to be adjusted as various issues in the community arise that are either unforeseen at this time or that are paid for through other funding sources not known or available at the time of the adoption of this Plan.

### Examples of Projected Street Improvements



Proposed McArthur Island Roundabout; Source: ND Lea Consultants Ltd.



Proposed Parking McDonald Park Parking Lot; Source: ND Lea Consultants Ltd.



Proposed Royal Avenue Crosswalk; Source: ND Lea Consultants Ltd.



Proposed Royal Avenue Crosswalk; Source: ND Lea Consultants Ltd.

# Implementation

## 5.5 DEVELOPMENT INCENTIVES

Development on the North Shore has historically occurred at a slower pace than the South Shore, with smaller commercial buildings that have a neighbourhood orientation rather than a city-wide customer base. Tranquille Road was the downtown of the stand alone, independent community of the Village of North Kamloops prior to amalgamation with Kamloops in 1967. Typically lagging behind in public property upgrades and private investment, the North Shore has, at times and similar to other areas of Kamloops, struggled to retain its commercial businesses. Commercial areas such as the Fortune Shopping Centre, Northhills Mall, 8th Street Corridor and the 12th Street area still managed to maintain an acceptable level of business activity during slower economic times.

Kamloops as a whole has continued to grow at a steady rate. Attracting residents and business owners from the Lower Mainland and Alberta, Kamloops is now entering an evolutionary phase where development, largely infill, is needing to be innovative, of a high quality, appropriate to its surroundings and creative in its design and connection to the community. Adding sustainable development practices into the mix creates additional costs and delays to construction. New regulations that are ahead of the development industry, such as green roofs and eco-stormwater systems, are often misunderstood and/or are simply not recognized for their value added attributes and are typically avoided or ignored in new construction where the tried and true path of least resistance is usually supported.

In order to make the policies and the vision and goals of the North Shore Neighbourhood Plan a reality, municipal leadership is required. Although this can take many forms, financial incentives to developers are typically the types of leadership roles the development industry responds more favourably to.

Recently, the City underwent a process to create a Revitalization Tax Exemption program for the City Centre. This program supports new development and building improvements based on a set of criteria. Although eligible for a similar program on the North Shore, additional incentives are being proposed as a means to jump start some much needed redevelopment and especially to support the sustainable development policies that are at the forefront of the North Shore Neighbourhood Plan.

Green buildings, green infrastructure, eco-roofs and sustainable stormwater systems, although cheaper in the long run, can typically add between 2-15 per cent costs to a development's bottom line. Based on numbers alone, these sustainable forms of development will not be used or implemented without additional help.

As a means to provide that help, a Development Incentive Matrix (see page 136) has been created to provide varying degrees of incentives to those developers who wish to showcase their creative abilities and innovative site and building designs. This Matrix is intended to provide as much upfront information as possible to a developer to reduce uncertainties.

Incentives are provided with a range and are aimed at not only helping the developer reduce hard costs but also the soft costs associated with development. Increasing a rate of return for development that is appropriate and beneficial to the community and the environment over conventional development is the ultimate goal here.

### POLICY STATEMENTS:

1. The City will support the use of the Development Incentive Matrix to encourage sustainable, attractive and appropriate development within North Shore neighbourhoods.
2. The City will support examining the use of a DCC system whereby Development Cost Charges are levied based on the density, green design and location of development and where rates are based on square footage rather than number of units.
3. The City will support the use of performance-based development by using performance-based Development Permit Guidelines and flex based zoning regulations.
4. The City will support developing reductions in Development Cost Charges for sustainable and green development.

Cont...

# Implementation

## 5.5 DEVELOPMENT INCENTIVES (cont.)

The North Shore Neighbourhood Plan identifies several types of development that could be eligible for a development incentive. Development types that support incentives include:

- Constructing market and non-market housing within the same development, with a minimum of 15 per cent of the units allocated to non-market occupants;
- Using a Housing Agreement for a residential development to ensure that there was affordable rent control offered for an identified period of time;
- Constructing a green roof to a minimum of 50 per cent of the total roof area;
- Constructing an on-site/eco-friendly stormwater system;
- Constructing both a green roof and eco-friendly stormwater system;
- Constructing a green building;
- Constructing mixed-use residential and commercial buildings within the North Shore Towne Centre and Tranquille Commercial District.

The types of incentives offered can include a combination of:  
**Tax Reductions** - providing a reduction in property taxes up to 10 (ten) years;

**Development Cost Charge Reductions** - providing a reduction in DCCs owed to the City at the time of development (see additional clarification);

**Parking Requirement Relaxations** - providing for a reduction in the number of parking stalls required;

**Density Bonusing** - providing for an increase in density for development sites;

**Public Realm Partnering** - providing funding and support from the City for improvements to the public right-of-way; and

**Planning Process Priorities** - providing for increases in priority in the development approval process for those proposals that exhibit enhanced building and site design.

Incorporating environmentally sensitive and community appropriate designs into a development can increase construction costs and prevent sustainable landscapes from being incorporated into urban environments. These incentives are intended to provide the property developer with the financial capacity to incorporate innovative and environmentally responsible building forms and site layouts into a development by reducing upfront and long-term costs. The use of these Development Incentives are connected directly to the use of the Development Checklist (see Appendix "B"). The Development Incentives Matrix (see page 136), illustrates the basic standard of design and development required as well as three (3) additional levels of excellence that can be reached and used in negotiations between the City of Kamloops and the development applicant.

### Development Cost Charge Reduction

The North Shore Neighbourhood Plan supports reducing Development Cost Charge rates for green or eco-friendly developments that contribute to sustainable communities and that reduce burdens on the City's infrastructure compared to conventional forms of development. The North Shore Plan also supports reducing DCC rates for social housing as a means to encourage the construction and availability of affordable housing options for the community. Reducing DCCs for social housing is supported by current by-laws, however, in order to encourage a greater variety of development options, the definition for "Not For Profit Rental Housing" will need to be amended. Reduced DCCs for green development on the other hand, is not currently supported by any by-laws and will require a comprehensive review of applicability before it can be adopted and used by the development community. This review is scheduled to take place independently of the North Shore Plan and will be ready for implementation in 2009. Upon completion of the Green DCC review, the North Shore Development Incentive Matrix (see p. 136) will be adjusted showing the types of development that will qualify for Green DCC reductions. In the meantime, the Incentive Matrix illustrates DCC incentive reductions for social housing only.

## POLICY STATEMENTS: (cont.)

5. The City will support expanding the Development Cost Charge By-law "Not For Profit Rental Housing" definition by including provisions for the use of rent control through a Housing Agreement.



# Implementation

## 5.5 DEVELOPMENT INCENTIVES (cont.)

The Development Checklist is separated into six (6) categories: Regulatory Conditions; Public Health and Safety; Urban Design; Social Sustainability; Site Access, Connectivity and Parking; and Environmental Sustainability. Each section contains design and/or construction standards that will be considered in the evaluation process; with baseline scores established for each category. Incentives are available for those developments that meet or exceed a minimum of four of the six categories identified within the Development Checklist.

Table 6: Development Incentives Matrix

Development Checklist Section	Base Level	Level 1	Level 2	Level 3
Section Minimums refer to the minimum point level for each Section listed within the Development Checklist (see Appendix "B")	3 or fewer Section Minimums met	4 out of 6 Section Minimums met	5 out of 6 Section Minimums met	6 out of 6 Section Minimums met
Development Incentive Tool	Reduction Amount	Reduction Amount	Reduction Amount	Reduction Amount
Tax Exemption <sup>1</sup> :	n/a	20%	40%	60%
Within North Shore Towne Centre	n/a	additional 20%	additional 20%	additional 20%
Within Tranquille Commercial District	n/a	additional 15%	additional 15%	additional 15%
Within Tranquille South	n/a	additional 10%	additional 10%	additional 10%
Within Brock Shopping Centre	n/a	additional 10%	additional 10%	additional 10%
Within Halston Entry Corridor or 8th Street	n/a	additional 5%	additional 5%	additional 5%
All Other Areas	n/a	n/a	n/a	n/a
Mixed-use Green Development	n/a	additional 10%	additional 10%	additional 10%
Green Development	n/a	additional 5%	additional 5%	additional 5%
Combined Market and Non-market Housing	n/a	additional 10%	additional 10%	additional 10%
DCC Reduction <sup>2</sup> :				
Social Housing Units	100%	100%	100%	100%
Small Housing Units	TBD	TBD	TBD	TBD
Green Development	n/a	TBD	TBD	TBD
Parking Requirement Relaxation	n/a	5%	10%	25%
Density Bonus <sup>3</sup>	n/a	25%	50%	Maximum Permitted
Public Realm Partnering <sup>4</sup>	n/a	25%	35%	50%
Planning Process Priorities <sup>5</sup>	Normal Process	Normal Process	Expedited Process	Expedited Process

<sup>1</sup>Exemption Periods are for a maximum of 10 years; exemptions calculated on the increase in total assessed value, post-construction; minimum 50 per cent of roof coverage applies to green roof installation; minimum of 50 per cent of units designated as affordable for combined market and non-market housing developments is required;

<sup>2</sup>DCC Reductions pertain to the social housing units only; Green Development DCC reductions are "To Be Determined" as part of the Green DCC review taking place 2008-2009; Small Housing Units are self-contained and are no larger than 30 m<sup>2</sup>.

<sup>3</sup>Density Bonus is an increase limited to the maximum densities identified within Section Four of this Plan and within the North Shore Development Permit Area Guidelines;

<sup>4</sup>Public Realm Partnering (to a maximum of \$100,000) requires the approval of City Council;

<sup>5</sup>Planning Process Priorities will be expedited for all Level 1 developments that incorporate a minimum of 50 per cent of the total units as non-market housing.

# Implementation

## 5.6 SIGN REGULATIONS

Significant efforts are put into establishing preferred design guidelines for building facades and for creating livable and functional public spaces along street corridors for pedestrians to interact in and enjoy. Little effort is put into providing direction for the type of sign style, size and location that is appropriate for a community, often with negative and detrimental results. Sign guidelines included in the Development Permit Guidelines were established to work toward creating pedestrian and human-scale urban spaces that are not dominated by auto-oriented signs.

### Pedestrian Friendly and Appropriate Signage



### POLICY STATEMENTS:

1. The City will support new sign regulations for the North Shore as a way to reduce the corporate commercialization of buildings within a unique character area of Kamloops and as a way to promote and protect a pedestrian-oriented community from the encroachment of auto-oriented structures and designs.
2. The City will support incorporating sign guidelines into the Design Guidelines of the North Shore to ensure that signs are constructed in a coordinated manner with the corresponding business and building and to ensure that signs are of a scale consistent with an environment supportive of pedestrians rather than automobiles.

### Vehicle-Oriented Signage



Photos: Carla Stewart

### ACTION ITEMS:

- a. Prepare new sign regulations for the North Shore consistent with the North Shore Development Permit Guidelines.

# Implementation

## 5.7 NON-CONFORMING USES

The preparation of the North Shore Neighbourhood Plan and supporting North Shore Zoning Code represent the first set of policy and regulations that are unique and tailored to the North Shore community. Challenges will exist during implementation of this new Plan, particularly as the need for sustainable, high quality and appropriate development outweighs the need for development at all costs. The policies in this document will require a creative and innovative way to do business which can and often does encounter resistance. There will also be areas of the North Shore that currently have legitimate uses taking place that will change to a legal, non-conforming status after the adoption of this Plan and the new Zoning Code. For the greater good of the whole of the North Shore, as well as the rest of the city, redeveloping these areas of transition are key opportunities to support a new vision and to provide leadership toward a healthier, preferred future.

### POLICY STATEMENTS:

1. The City will recognize that any land designation and/or use existing at the date of the adoption of this planning document and supporting regulating zoning code, shall be considered pre-existing non-conforming and shall be subject to the legislation established in the Local Government Act.

### Areas of Transition



Airport Entry Corridor; Photo: Carla Stewart



Ord Road; Photo: Carla Stewart



Tranquille Commercial District; Photo: John Popoff



North Shore Towne Centre; Photo: Carla Stewart

# Implementation

## 5.8 OFFICIAL COMMUNITY PLAN AMENDMENTS

The preparation of the North Shore Neighbourhood Plan has been complex and spanned nearly three years of work with stakeholders, the community and an advisory committee. As the first ever comprehensive land use plan for the North Shore, there has been a significant amount of effort put into ensuring that all issues have not only been identified but have been addressed in an appropriate manner consistent with the community's vision and goals.

The public has been involved in the preparation of the Plan and have endorsed a vision and a set of goals and principles for their preferred future. Similar to the Corporate Strategic Plan, these goals and principles are to be used to measure the appropriateness of new development within the North Shore, becoming the criteria to meet. The policies and regulations established under this Plan provide the preferred road map of directions on how to proceed with the evolution of a community. Therefore, when development proposals are being considered that are in contravention of this Plan, an amendment to the Official Community Plan is required.

Engaging in a community monitoring program is also essential to determining the successes of a plan. Performance indicators can be established to gauge whether a community has benefited from the policies in a land use plan or not. These indicators can include, but are not limited to, the amount of green space per capita, availability of public transportation, a reduction in crime, an increase in community activities, changes in social characteristics, and improvements in the environment. A performance indicator for the North Shore Neighbourhood Plan will also help Council determine direction in future decisions, budget allocations, citizen health, business vitality and visitor satisfaction; all important elements in creating a healthy and sustainable community, city and region.

### POLICY STATEMENTS:

1. The City will support limiting Official Community Plan amendments for the North Shore Neighbourhood Plan for development proposals that are not respectful of the vision, goals and principles of the Plan. Official Community Plan amendment proposals will be reviewed on the following criteria:
  - the suitability of the site or area for the proposed use, especially in relation to other sites and areas of the North Shore and/or the city;
  - compatibility of the proposed use with adjacent land use designations;
  - the need for the proposed use in light of projected population and employment targets;
  - the relationship of the application to the planned function of the site or neighbourhood;
  - the extent to which the existing areas of the North Shore, that are designated for the proposed use, are developed or available for development;
  - the impact the proposed use will have on sewer and water systems, transportation systems, community facilities and the natural environment;
  - the efforts put toward creative, innovative and sustainable designs; and
  - the efforts put toward appropriate design and compatibility with the adjacent neighbourhood and the fiscal implications of the proposed amendment on the North Shore and the city as a whole.
2. The City will support developing and using a Performance Indicator review for the North Shore Neighbourhood Plan to identify if adjustments are needed.



# Implementation

## Sources and References

# Sources and References

The following sources of information and reference materials were used in the preparation of policy and regulation for the North Shore Neighbourhood Plan:

## **CPTED (Crime Prevention Through Environmental Design)**

City of Scarborough, Community Safety Audit Program, *Audit Checklist* ([www.toronto.ca/safetyaudits/pdf/scarb\\_safety\\_checklist.pdf](http://www.toronto.ca/safetyaudits/pdf/scarb_safety_checklist.pdf)).

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Shapland, Joanna, Wites, Paul, Wilcox, Paula, *Targeted Crime Prevention for Local Areas*.

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## **GREEN ROOFS**

City of Vancouver ([www.vancouver.ca](http://www.vancouver.ca)).

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City of Surrey, draft *East Clayton Neighbourhood Concept Plan*, 2000.

City of Surrey, *Official Community Plan* ([www.surrey.ca](http://www.surrey.ca)).

The Planning Center, *Center Views* ([www.theplanningcenter.com](http://www.theplanningcenter.com)).

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# Sources and References

# Appendices

# Appendix "A" - North Shore Development Guidelines

# "A" - North Shore Development Guidelines

## AUTHORITY

In accordance with the *Local Government Act*, Section 920, as amended, Development Permit Areas may be designated within a city. The North Shore Neighbourhood Plan identifies one Development Permit Area that corresponds to the land use policies and the future vision and goals of the community. Unless otherwise specified, a Development Permit must be approved by City Council prior to any development, subdivision, construction or alteration within a Development Permit Area.

## JUSTIFICATION

Development Permits are required for the following reasons:

- Protection of the natural environment, its ecosystems and biological diversity;
- Protection of development from hazardous conditions;
- Protection of farming;
- Revitalization of areas in which commercial use is permitted;
- Establishment of objectives for the form and character of commercial, industrial or multi-family residential development;  
or
- Establishment of objectives for the form and character of intensive residential development.

As shown in Section 1.2 of the Official Community Plan, the following Development Permit Guidelines are applicable to any commercial, industrial and/or institutional development located within the boundaries of the North Shore Neighbourhood Plan. These Development Permit Guidelines are intended to supplement the city-wide Multiple Family Development Permit Area Guidelines and the other area-specific Development Permit Areas of the North Shore as described and identified in this document.

## EXEMPTIONS

Development Permit exemptions apply to internal renovations and/or external renovations that do not affect the form and character of the building.

## IMPLEMENTATION

In accordance with By-law No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications with a construction value of \$250,000 or less; multiple family development of eight or fewer units; where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

## VARIANCES

In accordance with the policies, vision, goals and principles of the North Shore Neighbourhood Plan, variances for parking, setbacks, building height and size and landscaping requirements will be considered as part of a Development Permit, where those variances provide for high quality building and site design, eco-friendly building and site design and development that makes a positive contribution to the existing neighbourhood.

## ILLUSTRATIONS

Images depicting various development types are included within these Guidelines to provide illustrative examples for reference.

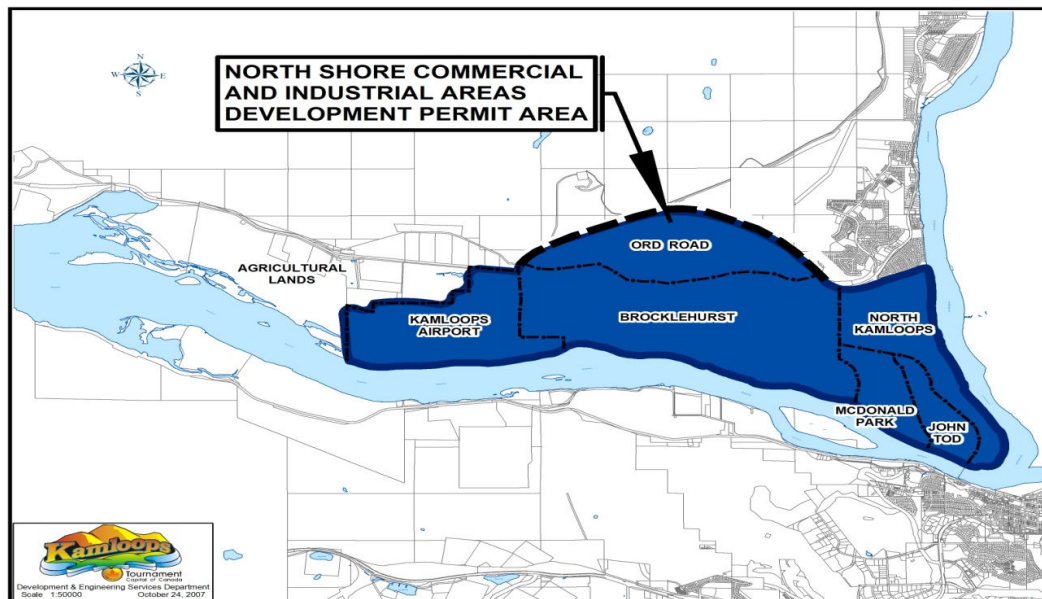


# "A" - North Shore Development Guidelines

## NORTH SHORE

### DEVELOPMENT GUIDELINES

As shown in Section 1.2 of the Official Community Plan, the following Development Permit Guidelines are applicable to the area shown on the Map below and are intended to supplement the city-wide Multiple Family Development Permit Area Guidelines.



### APPLICATION

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial, industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation; pedestrian circulation and access; landscaping, building elevations, site layout; and street enhancements.

Building design and site layout shall be designed in such a way so as to be consistent with the vision, goals and principles identified in the North Shore Neighbourhood Plan by providing pedestrian-focused environments (except as noted), innovative designs, and sustainable building practices.

### DEVELOPMENT PERMIT AREA

The North Shore has several areas of commercial and industrial development scattered throughout both North Kamloops and Brocklehurst. Some of these areas have unique development constraints and characteristics that necessitate additional development permit guidelines over and above the general ones listed here for the whole of the North Shore. These areas include: North Shore Towne Centre, Tranquille Commercial District, Tranquille South, Brock Shopping Centre, Airport Entry Corridor, 8th Street Corridor, Halston Entry Corridor and Ord Road Corridor. The additional specifications for development within these areas is listed within this document.

# "A" - North Shore Development Guidelines

## NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - BUILDING MASSING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> <li>1. Building massing shall avoid long, continuous and blank vertical surfaces adjacent to streets and sidewalks and in particular, adjacent to pedestrian access areas.</li> <li>2. Building massing shall create pedestrian-oriented developments along street fronts and along major pedestrian thoroughfare routes.</li> <li>3. Building massing shall create visual interest and appeal with the use of varied roof lines, roof pitches and roof heights.</li> </ol>	<ol style="list-style-type: none"> <li>1. Building massing shall avoid long, continuous and blank vertical surfaces throughout the entire development.</li> </ol>

## NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - BUILDING DESIGN

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> <li>1. Building design shall consider the relationship to and the transition between adjacent buildings and properties.</li> <li>2. Building design shall completely screen all service areas and utility equipment from adjacent streets and properties and from pedestrian thoroughfares.</li> <li>3. Building design incorporating residential uses shall use balconies. Balconies shall be large enough to allow active use and seating.</li> <li>4. Buildings designed with flat roofs shall be compensated with the combination of the use of varied materials, building projections, articulations and architectural details and modulations so as to detract from the unvaried roof line.</li> <li>5. Building design shall use lighting fixtures that create visual interest and that are located on the building and the site at a human scale rather than an automobile orientation.</li> <li>6. Building design shall incorporate architectural or landscaping elements to screen roof-top mechanical equipment.</li> <li>7. Building design shall orient entrances to the primary street which fronts the building.</li> <li>8. Building design for high density developments shall avoid large wall building massing, shall use various heights and vertical setbacks to create visual appeal and to create a design that works toward appropriate neighbourhood integration.</li> </ol>	<ol style="list-style-type: none"> <li>1. Building design should emphasize texture, pattern, form, shadow, structure and variety.</li> <li>2. Building design should include a significant portion of display windows at the ground floor or pedestrian level of the building.</li> <li>3. Building design should incorporate canopies, awnings and other protective structures complimentary to the design, materials and color to add visual appeal and interest.</li> <li>4. Building design should not include colors that are abrasive or jarring and should work to create pleasing tones and textures appropriate for a pedestrian-focused environment and appropriate to the surrounding natural amenities and natural features.</li> <li>5. Building design using flat roofs should incorporate amenity areas and/or green roof systems.</li> <li>6. Building design for mixed use developments should provide ground floor commercial uses that are accessible at grade.</li> <li>7. Building design should seek to incorporate an urban identity with focal point open spaces throughout building placement and massing.</li> <li>8. Building and site design should support an interesting and desirable image for the community.</li> <li>9. Building design should not incorporate the same colour scheme for accents as is used on the main portion of the building.</li> </ol>

# "A" - North Shore Development Guidelines

## NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - PARKING

Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Parking locations within front and side yards and along street frontages can occur (except as noted) where a minimum of a 4 m landscape buffer strip is provided between the parking area and a property line. Parking areas shall not be located directly adjacent to major pedestrian thoroughfares.</li> <li>2. Bicycle parking shall be provided on site in a convenient and easily accessible manner to a minimum of 0.1 bicycle stalls per vehicle stall.</li> <li>3. Parking lots shall be designed to channel pedestrian movements to reduce conflicts with vehicles and to connect to public sidewalks.</li> <li>4. Parking lot illumination shall be designed for when people are not in their vehicles and shall avoid light pollution or the spilling of light onto adjacent properties.</li> <li>5. Parking areas within parking structures shall not be visible from the building's primary access points and shall be located either below ground or no lower than the second floor of a building and signed appropriately to allow for ease of way finding.</li> </ol>	<ol style="list-style-type: none"> <li>1. Bicycle parking should be provided in a transparent but enclosed area to allow for security and safety of the bicycle parking area.</li> <li>2. Parking areas within parking structures are encouraged to be designed so as to only occupy 50 per cent or less of a single storey, within a multi-storey, mixed-use building.</li> </ol>

## NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - BUILDING MATERIALS

Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Building materials shall not be limited to the use of one type only but rather incorporate a minimum of three different types of building materials to add variety and interest to the design.</li> <li>2. Building materials shall not include vinyl siding, glassy vinyl fabric, mirrored glass, plastics and/or prefabricated metal sheets. Stucco coated exterior installations shall be limited in use. Unpainted concrete shall not be permitted.</li> <li>3. Building materials, at street level, shall not include dark, highly-reflective glass, glass block or any other treatments that preclude viewing into buildings.</li> </ol>	<ol style="list-style-type: none"> <li>1. Building materials should be products such as timber, stone, brick, concrete, metal and glass. Glass block should be used only as accent material.</li> <li>2. Building materials that include the use of stucco, concrete unit masonry, tile, wood trim and siding should be used as accents and secondary materials. Materials should create pattern and texture and convey quality and high standards of design.</li> <li>3. Care should be taken to ensure that building colour does not clash with adjacent building facades.</li> </ol>

## NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - GREEN TECHNOLOGY

Required Elements	Recommended Elements
	<ol style="list-style-type: none"> <li>1. Green technology should be incorporated into the building site, design and function, including the use of on-site stormwater distribution and retention and the incorporation of stormwater features into landscaping and/or public spaces. The use of green roofs and the use of environmentally sustainable building standards such as Green Globes, Built Green or LEED.</li> </ol>

# "A" - North Shore Development Guidelines

## NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - SIGNAGE

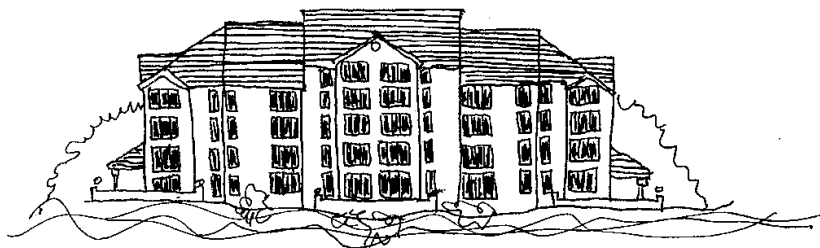
Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Signage shall be incorporated into the building in a manner that creates a coordinated and visually relative and appealing manner to that of the facade design.</li> <li>2. Signage shall be principally pedestrian-oriented, made of durable, weather-resistant material, opaque, and coloured in such a way so as to coordinate with the facade of the building.</li> <li>3. Signage shall not be the dominant feature of the facade or overall development.</li> <li>4. Billboards and portable read-o-graph signs shall be prohibited.</li> </ol>	<ol style="list-style-type: none"> <li>1. Podium or free-standing signs can occur where sign height and sign area are limited to a pedestrian scale and where natural materials and directional lighting is used, in conjunction with landscaping.</li> <li>2. Back-lighting of signage can occur where individual, three-dimensional letters are used.</li> <li>3. Window signs should not cover more than 20 per cent of the total area of the ground floor frontage facing any one street.</li> </ol>

## NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - SITE LAYOUT

Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Site layout and building design shall incorporate the principles of CPTED: Crime Prevention Through Environmental Design.</li> <li>2. Site layout and design shall take into consideration adjacent uses, adjacent existing building setbacks and existing pedestrian travel thoroughfares.</li> <li>3. Site layout shall be designed so that access to primary entrances, from public sidewalks, are along fully accessible and direct pedestrian access routes.</li> </ol>	<ol style="list-style-type: none"> <li>1. Site layout and design should incorporate additional features, such as public art, to create interest and focal points.</li> </ol>

Varied sizes along main commercial cores and multi-family buildings with varied roof lines create attractive urban spaces;

Original Image Source: see Sources and References Section;  
Drawing: Carla Stewart



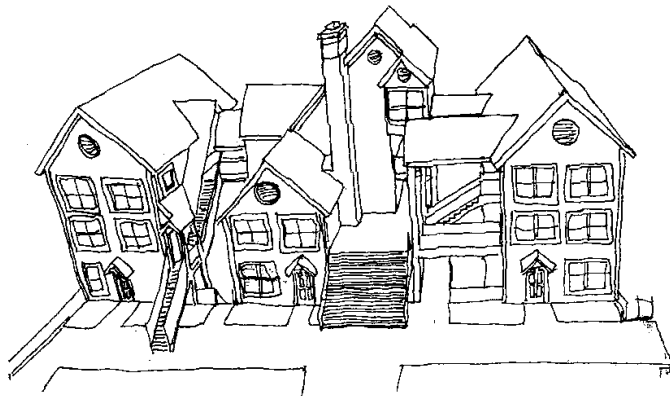
# "A" - North Shore Development Guidelines

## NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - LANDSCAPING

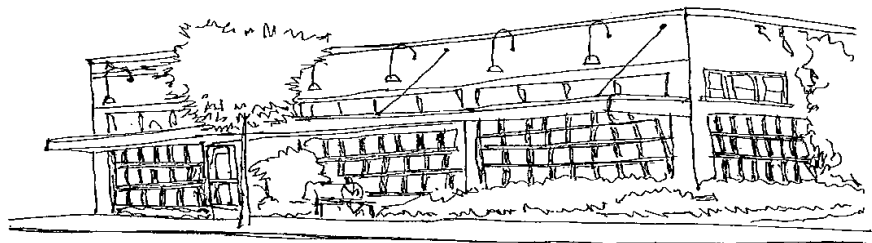
Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Landscaping shall be provided to screen all parking that may be visible from any street.</li> <li>2. Landscaping shall be provided to screen all service areas, utility equipment and/or loading areas.</li> </ol>	<ol style="list-style-type: none"> <li>1. Landscaping should be used to provide amenity spaces on the roof of a building or as part of a green roof/green infrastructure stormwater system.</li> <li>2. Landscaping of an appropriate and sufficient manner, can be used to screen commercial garbage enclosures as a replacement or alternative to other hard material designs.</li> <li>3. Landscaping of multiple family developments should use fruit-bearing species as part of the required amenity feature.</li> <li>4. Landscaping should be incorporated into balconies and roof decks.</li> </ol>

## NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - BUILDING HEIGHT

Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Maximum building height is two (2) storeys.</li> </ol>	<ol style="list-style-type: none"> <li>1. Building height maximum can be increased to four (4) storeys with the addition of a combination of green building designs and eco-friendly stormwater systems and roofs, mixed use development, market housing and non-market housing (if permitted in the zone) and/or public art.</li> </ol>



Residential and commercial developments with varied roof lines and architecturally appealing elements make for unique, vibrant and attractive urban spaces;  
 Original Image Source: see Sources and References Section;  
 Drawing: Carla Stewart



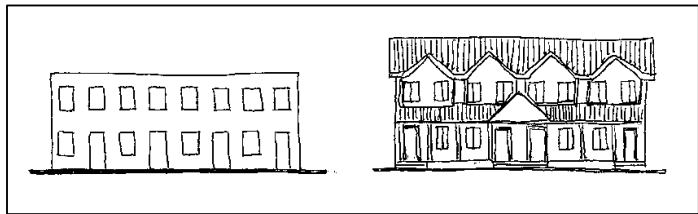


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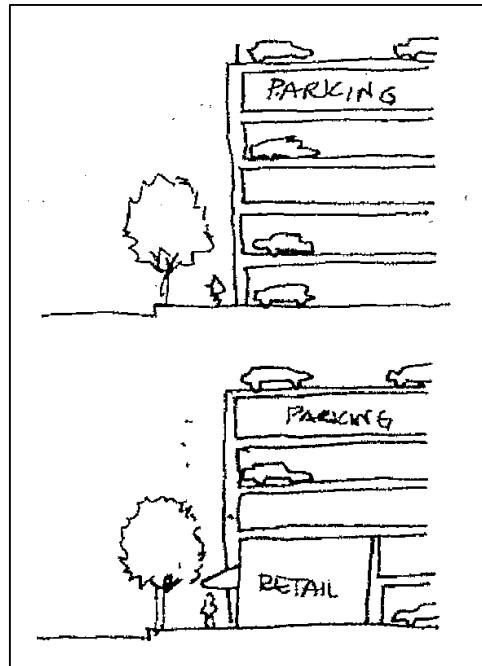
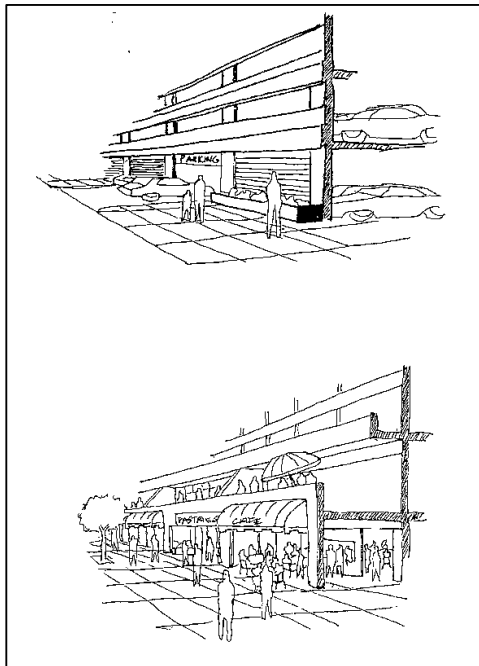
## DEVELOPMENT PERMIT GUIDELINE ILLUSTRATIONS

In order to supplement the Guidelines presented in this document, several graphic sketches are provided below to use as reference material when designing sites and buildings on the North Shore. These graphics are for illustrative purposes only.

*(Original Image Source: for the images used in this section sources can be obtained in the Sources and References Section of this Plan).*



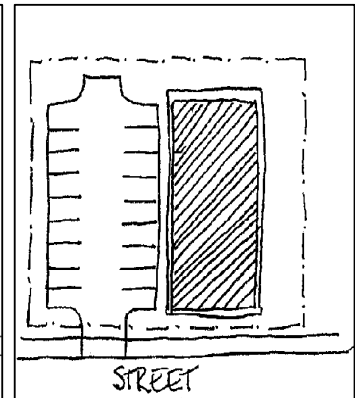
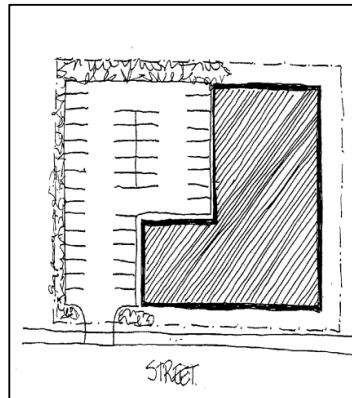
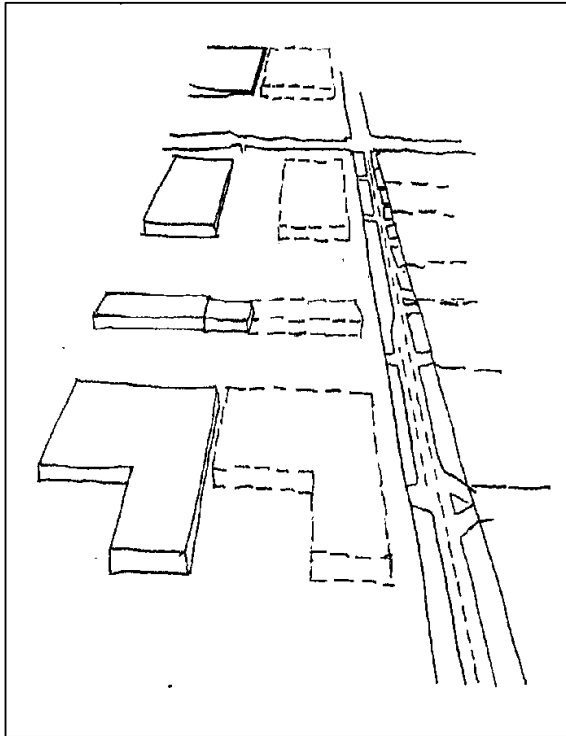
Buildings with varied roof lines and design elements providing attractive urban environments; Drawings: Carla Stewart



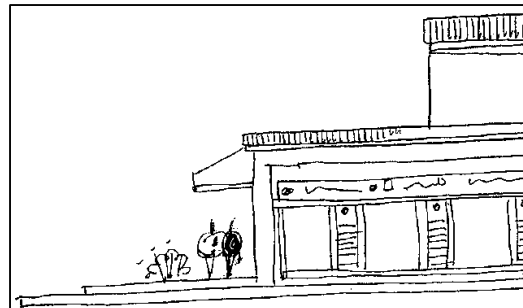
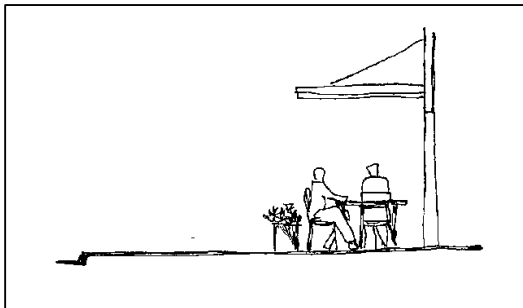
Parking structures need to be incorporated into the building design and function, providing for an active front that attracts pedestrian uses and activities; Drawings: Carla Stewart

# "A" - North Shore Development Guidelines

## DEVELOPMENT PERMIT GUIDELINE ILLUSTRATIONS (cont.)



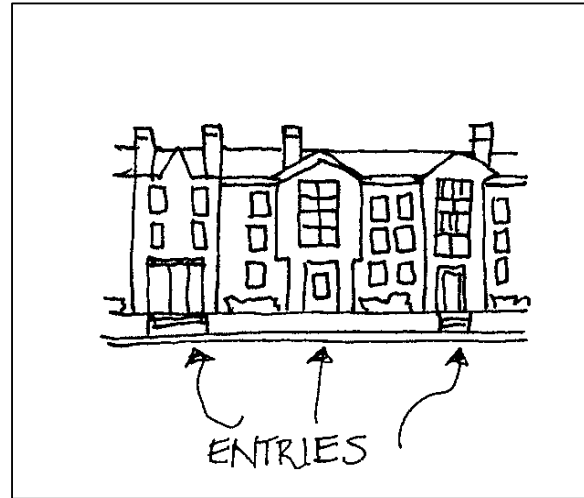
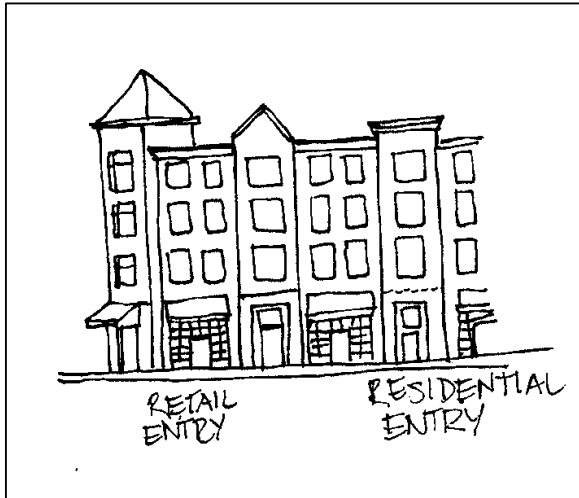
Buildings should be oriented toward the street to encourage pedestrian uses and activities; parking should never be located between a building and a sidewalk; Drawings: Carla Stewart



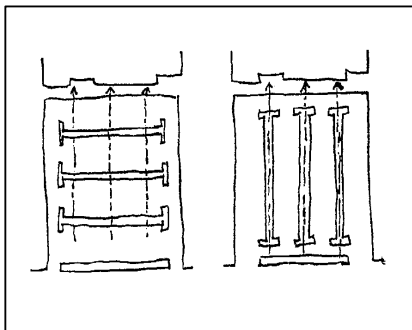
Buildings should encourage pedestrian uses by providing seating areas associated with restaurants and coffee shops, particularly along the Tranquille Commercial Corridor and within the North Shore Town Centre; Drawing: Carla Stewart

# "A" - North Shore Development Guidelines

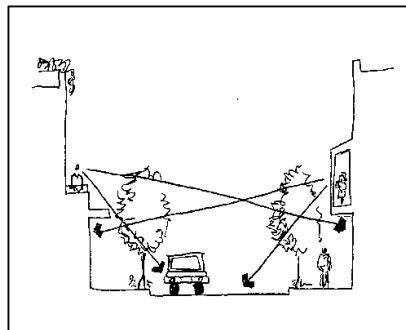
## DEVELOPMENT PERMIT GUIDELINE ILLUSTRATIONS (cont.)



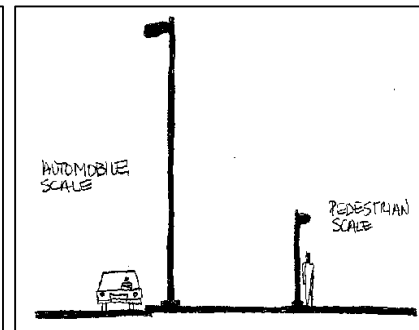
Mixed-use buildings should have multiple entrances and should have separate entrances for the residential and commercial units; Drawings: Carla Stewart



Parking lot layouts should be designed with the drive aisles running perpendicular to the buildings to create easier movements for pedestrians on site; Drawing: Carla Stewart



Buildings should be designed with natural surveillance opportunities including and especially useable balconies; Drawing: Carla Stewart



Light fixtures should be designed and installed to accommodate a pedestrian scale rather than a scale for automobiles; Drawing: Carla Stewart

# "A" - North Shore Development Guidelines

## NORTH SHORE TOWNE CENTRE

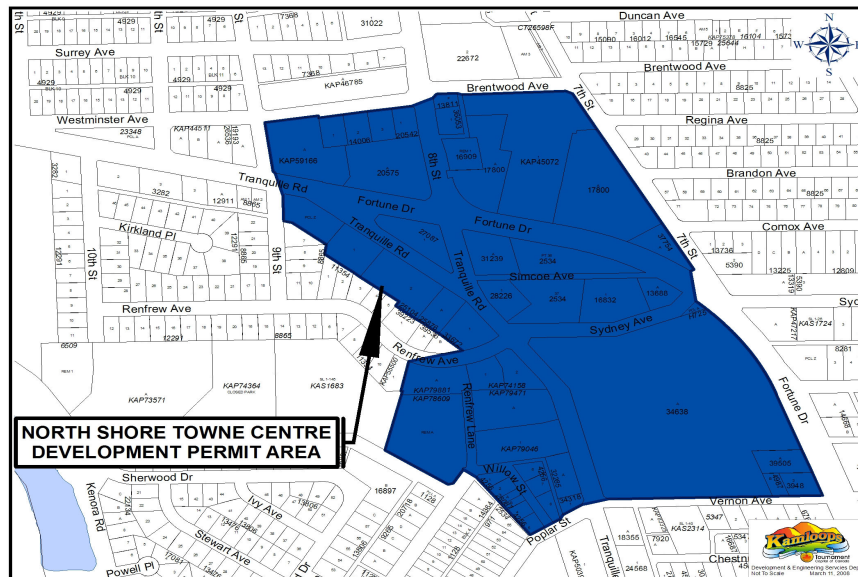
### AREA SPECIFIC GUIDELINES

#### JUSTIFICATION

The Towne Centre is a focal point for the North Shore with concentrations of commercial uses, heavy pedestrian and vehicle traffic and activities centred around the second busiest intersection in the city. Community input during the preparation of the North Shore Neighbourhood Plan identified the North Shore Towne Centre as a significant area that, when developed, should showcase the North Shore with significant buildings designed to make a connection to the high level of pedestrian activity existing throughout the area. Achieving this requires high design standards along with the use of innovative, creative and eco-friendly building and site design concepts that accommodate pedestrians over the vehicle.

#### GUIDELINES

The following Development Permit Guidelines are applicable to the area shown on the Map below and are intended to supplement the North Shore Development Guidelines as well as the city-wide Multiple Family Development Permit Area Guidelines. Development Permits issued in this area shall be in accordance with the following Guidelines:



# "A" - North Shore Development Guidelines

## NORTH SHORE TOWNE CENTRE - SITE LAYOUT AND PARKING

Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Parking shall not be located in any front or side yard or along any street frontage.</li> <li>2. Site layout and building design, for those properties fronting the Fortune Drive, Tranquille Road and 8th Street intersection, shall form or incorporate a landmark character feature and shall have building frontage extend along the street frontage.</li> <li>3. Site layout and building design, for those properties fronting the Simcoe Avenue corridor, shall provide pedestrian-oriented development along both Simcoe Avenue and Fortune Drive in order to create a built form that is consistent with the roles and functions of a Towne Centre.</li> </ol>	<ol style="list-style-type: none"> <li>1. Parking locations within front and side yards and along street frontages can occur where a minimum of a 4 m landscape buffer strip is provided between the parking area and a property line.</li> </ol>

## NORTH SHORE TOWNE CENTRE - BUILDING SETBACKS

Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Front yard setback minimum is 0 m.</li> <li>2. Front yard setback maximum is 1.0 m.</li> <li>3. Side yard and/or side street yard setback minimum is 0 m.</li> <li>4. Side yard and/or side street yard setback maximum is 1.0 m.</li> <li>5. Rear yard setback minimum is 0 m.</li> <li>6. Rear yard setback minimum, where a lot is adjacent to a residential zone with no intervening lane or street, is 2 m.</li> </ol>	<ol style="list-style-type: none"> <li>1. Front yard and side street yard setback minimums and maximums can be increased only with the use of a pedestrian plaza or front entry landscape feature or public area feature at the corner of any property adjacent to the Fortune Drive, Tranquille Road and 8th Street intersection.</li> </ol>



# "A" - North Shore Development Guidelines

## NORTH SHORE TOWNE CENTRE - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY

Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Landscaping shall cover a minimum of five (5) per cent of the total site area, not including the boulevard.</li> <li>2. Auto-oriented, vinyl, back-lit podium or free-standing signs shall not be permitted.</li> </ol>	<ol style="list-style-type: none"> <li>1. Landscaping minimum can be reduced, up to three (3) per cent of the total site area, with the use of eco-roofs and green infrastructure stormwater systems that use landscaped features.</li> </ol>

## NORTH SHORE TOWNE CENTRE - BUILDING DESIGN

Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Building design, for those properties fronting the Tranquille Road, Fortune Drive and 8th Street intersection, and along the Simcoe Avenue corridor, shall be designed and situated so as to create a landmark or focal point building for the North Shore Towne Centre. Landmark and focal point buildings shall incorporate significant architectural styles and features to distinguish them from adjacent properties and to create a positive impact on the landscape and in conjunction with the pedestrian movements and activities in the area.</li> <li>2. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the Towne Centre Area in the North Shore Neighbourhood Plan.</li> <li>3. Building design shall contribute to a lively and useable pedestrian scale over and above auto-oriented designs.</li> </ol>	<ol style="list-style-type: none"> <li>1. Building design should provide a continuous street frontage to encourage a pedestrian-oriented community.</li> <li>2. Building design should provide ground floor commercial uses that are accessible at grade.</li> <li>3. Building design should include a number of separate street front shops with separate entrances.</li> </ol>

## NORTH SHORE TOWNE CENTRE - BUILDING MASSING

Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Building massing shall create focal points and landmark developments at the Fortune Drive, Tranquille Road and 8th Street intersection, and along the Simcoe Avenue corridor, to be consistent with the vision, goals and policies identified in the North Shore Neighbourhood Plan.</li> <li>2. Building massing shall create pedestrian-oriented developments along street fronts and along major pedestrian thoroughfare routes, specifically along the Simcoe Avenue corridor.</li> </ol>	<ol style="list-style-type: none"> <li>1. Building massing shall accent focal points at primary sightlines throughout the Towne Centre.</li> </ol>

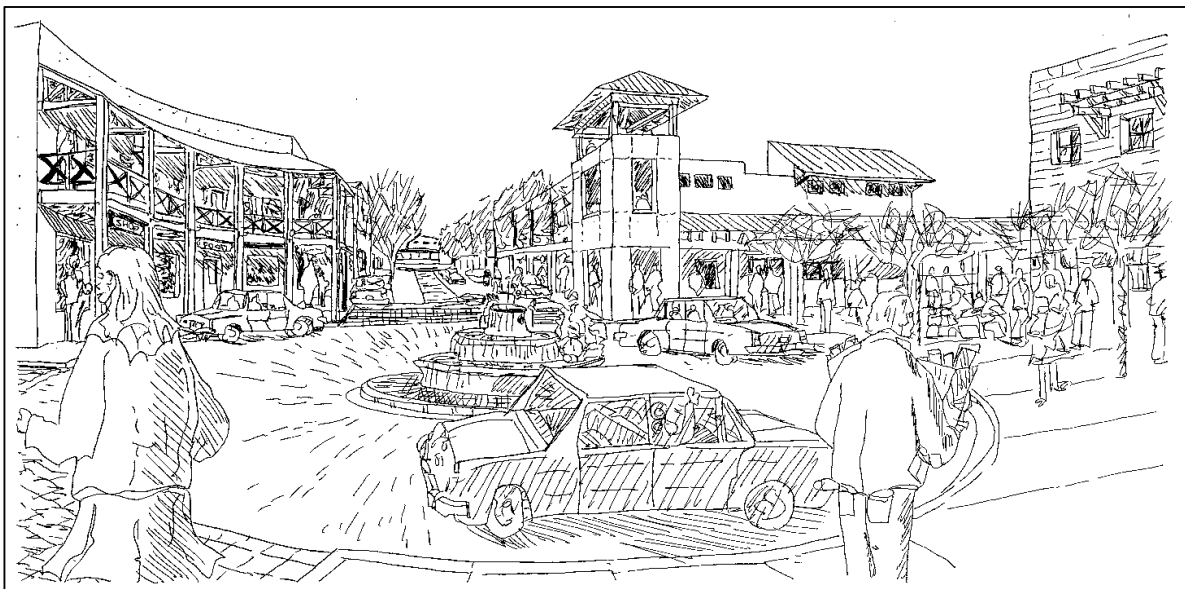
# "A" - North Shore Development Guidelines

## NORTH SHORE TOWNE CENTRE - BUILDING HEIGHT

Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Building height minimum is four (4) storeys.</li> <li>2. Building height maximum is four (4) storeys.</li> </ol>	<ol style="list-style-type: none"> <li>1. Building height minimums can be reduced to two (2) storeys where buildings are constructed as a landmark or focal point building, showcasing the importance of the North Shore Towne Centre and where a combination of public art, landscaped and pedestrian-focused plazas, green building design standards, and eco-friendly stormwater systems, or roofs are used.</li> <li>2. Building height maximum can be increased, up to ten (10) storeys, with the addition of a combination of market housing and non-market housing units, public art, landscaped and pedestrian-focused plazas, green building design standards and eco-friendly stormwater systems and roofs.</li> </ol>



Examples of Infill Development in the North Shore Towne Centre; Source: NDLea Consultants Ltd.



Streetscape Example: Pedestrian Friendly, Visually Appealing; Traffic Calming; Mixed Uses  
Original Image Source: *The Next American Metropolis*  
Drawing: Carla Stewart

# "A" - North Shore Development Guidelines

## TRANQUILLE COMMERCIAL DISTRICT

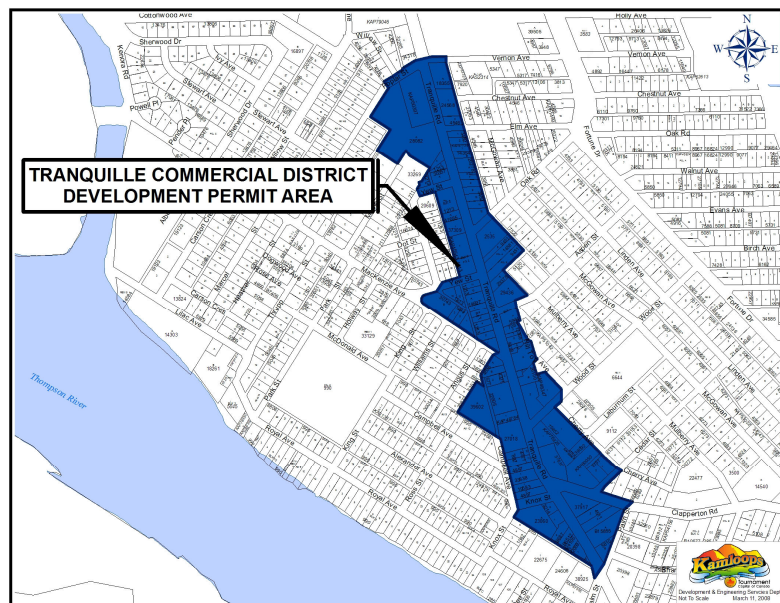
### AREA SPECIFIC GUIDELINES

#### JUSTIFICATION

The Tranquille Commercial District is the original main street for the North Shore and continues to accommodate the largest and most diverse concentration of central functions for the community including retail, office, personal services, entertainment, government resources, institutional uses, residential and community activities. Supporting and expanding these uses is paramount to ensuring the success of this commercial core area. Community input during the preparation of the North Shore Neighbourhood Plan supported increased residential densities along this corridor, maintaining a pedestrian-focused environment, encouraging more office use and creating focal point developments at key intersections along the length of the street. Achieving these goals requires high design standards along with the use of innovative, creative and eco-friendly building and site design concepts that accommodate pedestrians over the vehicle.

#### GUIDELINES

The following Development Permit Guidelines are applicable to the area shown on the Map below and are intended to supplement the North Shore Development Guidelines as well as the city-wide Multiple Family Development Permit Area Guidelines. Development Permits issued in this area shall be in accordance with the following Guidelines:



# "A" - North Shore Development Guidelines

## TRANQUILLE COMMERCIAL DISTRICT - SITE LAYOUT AND PARKING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"><li>1. Parking shall not be located in any front or side yard or along any street frontage.</li><li>2. Site layout and building design shall have building frontage extend along the street frontage and shall provide pedestrian-oriented development along Tranquille Road, consistent with the functions of a main street commercial area.</li></ol>	<ol style="list-style-type: none"><li>1. Parking locations within front and side yards and along street frontages can occur where a minimum of a 3 m landscape buffer strip is provided between the parking area and a property line.</li></ol>

## TRANQUILLE COMMERCIAL DISTRICT - BUILDING SETBACKS

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"><li>1. Front yard setback minimum is 0 m.</li><li>2. Front yard setback maximum is 1.0 m.</li><li>3. Side yard and/or side street yard setback minimum is 0 m.</li><li>4. Side yard and/or side street yard setback maximum is 1.0 m.</li><li>5. Rear yard setback minimum is 0 m.</li><li>6. Rear yard setback minimum, where a lot is adjacent to a residential zone with no intervening lane or street, is 2 m.</li></ol>	<ol style="list-style-type: none"><li>1. Front yard and side street yard setback minimums and maximums can be increased only with the use of a pedestrian plaza or front entry landscape feature or public area feature, particularly where development is located at intersections along the Tranquille Commercial District corridor.</li></ol>

# "A" - North Shore Development Guidelines

## TRANQUILLE COMMERCIAL DISTRICT - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> <li>1. Landscaping shall cover a minimum of five (5) per cent of the total site area, not including the boulevard.</li> <li>2. Auto-oriented, vinyl, back-lit podium or free standing signs shall not be permitted.</li> </ol>	<ol style="list-style-type: none"> <li>1. Landscaping minimum can be reduced, up to three (3) per cent of the total site area, with the use of eco-roofs and green infrastructure stormwater systems.</li> </ol>

## TRANQUILLE COMMERCIAL DISTRICT - BUILDING DESIGN

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> <li>1. Building design, for those properties fronting Tranquille Road at intersections, shall be designed and situated so as to create a focal point building for the Tranquille Commercial District. Focal point buildings shall incorporate significant architectural styles and features to distinguish them from adjacent properties and to create a positive impact on the landscape and in conjunction with the pedestrian movements and activities in the area.</li> <li>2. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the Tranquille Commercial District in the North Shore Neighbourhood Plan.</li> <li>3. Building design shall contribute to a lively and useable pedestrian scale over and above auto-oriented designs.</li> </ol>	<ol style="list-style-type: none"> <li>1. Building design should provide a continuous street frontage to encourage a pedestrian-oriented community.</li> <li>2. Building design should provide ground floor commercial uses that are accessible at grade.</li> <li>3. Building design should include a number of separate street front shops with separate entrances.</li> </ol>

## TRANQUILLE COMMERCIAL DISTRICT - BUILDING MASSING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> <li>1. Building massing shall create focal points and landmark developments along Tranquille Road to be consistent with the vision, goals and policies identified in the North Shore Neighbourhood Plan.</li> </ol>	<ol style="list-style-type: none"> <li>1. Building massing should be consistent with development on adjacent properties. Where this is not the case, architectural elements should be incorporated into building design to offset the difference in massing.</li> </ol>



# "A" - North Shore Development Guidelines

## TRANQUILLE COMMERCIAL DISTRICT - BUILDING HEIGHT

Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Building height maximum is three (3) storeys.</li> <li>2. Building height minimum is two (2) storeys.</li> </ol>	<ol style="list-style-type: none"> <li>1. Buildings at intersections should be constructed as a landmark or focal point building, showcasing the importance of the Tranquille Commercial District, consistent with the vision, goals and policies of the North Shore Neighbourhood Plan.</li> <li>2. Building height maximum can be increased, up to six (6) storeys, with the addition of a combination of market housing and non-market housing units, public art, landscaped and pedestrian-focused plazas, green building design standards and eco-friendly stormwater systems and roofs.</li> </ol>



**BEFORE**



**AFTER**

Examples of Infill Development in the Tranquille Commercial Area; Source: NDLea Consultants Ltd.



Streetscape Example: Pedestrian Friendly, Visually Appealing; Traffic Calming; Mixed Uses  
Original Image Source: *The Next American Metropolis*  
Drawing: Carla Stewart

# "A" - North Shore Development Guidelines

## TRANQUILLE SOUTH

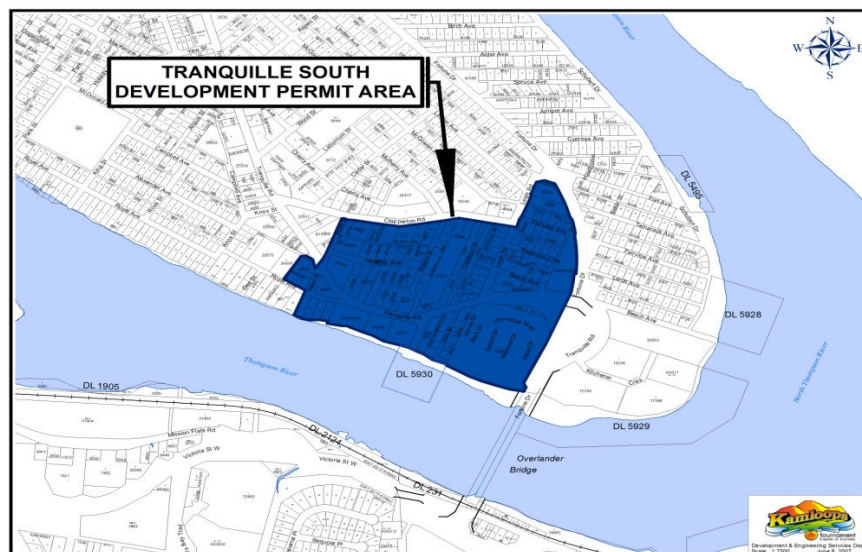
### AREA SPECIFIC GUIDELINES

#### JUSTIFICATION

The Tranquille South area is a key component to the success and function of the Tranquille Commercial District. Representing a southern entry point to Tranquille Road, this area has high levels of both pedestrian and vehicle traffic and is slowly transitioning away from the original auto-oriented uses to more pedestrian-oriented businesses such as bistros, coffee shops, retail stores and restaurants. Community input during the preparation of the North Shore Neighbourhood Plan identified the Tranquille South area as having potential for increased multiple family and office use development in buildings that are significant in nature announcing the entry into a core commercial area. Achieving this goal requires high design standards along with the use of innovative, creative and eco-friendly building and site design concepts that accommodate pedestrians over the vehicle.

#### GUIDELINES

The following Development Permit Guidelines are applicable to the area shown on the Map below and are intended to supplement the North Shore Development Guidelines as well as the city-wide Multiple Family Development Permit Area Guidelines. Development Permits issued in this area shall be in accordance with the following Guidelines:



# "A" - North Shore Development Guidelines

## TRANQUILLE SOUTH - SITE LAYOUT AND PARKING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> <li>1. Parking shall not be located in any front or side yard or along any street frontage that directly abuts Tranquille Road.</li> <li>2. Site layout and building design, for those properties at the entrance to the Tranquille South area and at intersections along Tranquille Road shall form or incorporate a landmark character feature and shall have building frontage extend along the street frontage.</li> <li>3. Site layout and building design, for those properties directly abutting Tranquille Road, shall provide pedestrian-oriented development in order to create a built form that is consistent with the roles and functions of a main street commercial area.</li> <li>4. Drive-thru aisles and idle lanes are not permitted on those properties directly fronting Tranquille Road, where they are permitted, they shall be located at a minimum setback of 10 m from any front and/or side street property line or any property line abutting a street.</li> </ol>	<ol style="list-style-type: none"> <li>1. Parking locations, that directly abut Tranquille Road, within front and side yards and along street frontages can occur where a minimum of a 3 m landscape buffer strip is provided between the parking area and a property line.</li> </ol>

## TRANQUILLE SOUTH - BUILDING SETBACKS

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> <li>1. Front yard setback minimum is 0 m.</li> <li>2. Front yard setback maximum is 1.0 m.</li> <li>3. Side yard and/or side street yard setback maximum is 1.0 m.</li> <li>4. Rear yard setback minimum is 0 m.</li> <li>5. Rear yard setback minimum, where a lot is adjacent to a residential zone with no intervening lane or street, is 2 m.</li> </ol>	<ol style="list-style-type: none"> <li>1. Front yard and side street yard setback minimums and maximums can be increased only with the use of a pedestrian plaza or front entry landscape feature or public area feature at the corner of any property adjacent to intersections at the entrance to the Tranquille South area.</li> </ol>

# "A" - North Shore Development Guidelines

## TRANQUILLE SOUTH - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> <li>1. Landscaping shall cover a minimum of five (5) per cent of the total site area, not including the boulevard.</li> <li>2. Auto-oriented, vinyl, back-lit podium or free standing signs shall not be permitted.</li> </ol>	<ol style="list-style-type: none"> <li>1. Landscaping minimum can be reduced, up to three (3) per cent of the total site area, with the use of eco-roofs and green infrastructure stormwater systems.</li> </ol>

## TRANQUILLE SOUTH - BUILDING DESIGN

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> <li>1. Building design, for those properties located at the entrance to the Tranquille South area, shall be designed and situated so as to create a landmark or focal point building. Landmark and focal point buildings shall incorporate significant architectural styles and features to distinguish them from adjacent properties and to create a positive impact on the landscape and in conjunction with the pedestrian movements and activities in the area.</li> <li>2. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the Tranquille South area in the North Shore Neighbourhood Plan.</li> <li>3. Building design shall contribute to a lively and useable pedestrian scale over and above auto-oriented designs.</li> </ol>	<ol style="list-style-type: none"> <li>1. Building design should provide a continuous street frontage to encourage a pedestrian-oriented community.</li> <li>2. Building design, for buildings fronting Tranquille Road, should provide ground floor commercial uses that are accessible at grade.</li> <li>3. Building design, for buildings fronting Tranquille Road, should include a number of separate street front shops with separate entrances.</li> </ol>

## TRANQUILLE SOUTH - BUILDING MASSING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> <li>1. Building massing shall create focal points and landmark developments at the entrance to the Tranquille South Area, to be consistent with the vision, goals and policies identified in the North Shore Neighbourhood Plan.</li> <li>2. Building massing shall create pedestrian-oriented developments along street fronts and along major pedestrian thoroughfare routes, specifically for those properties fronting Tranquille Road.</li> </ol>	<ol style="list-style-type: none"> <li>1. Accent focal points at primary sightlines throughout the Tranquille South area.</li> </ol>

# "A" - North Shore Development Guidelines

## TRANQUILLE SOUTH - BUILDING HEIGHT

Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Building height minimum is two (2) storeys.</li> <li>2. Building height maximum is two (3) storeys.</li> </ol>	<ol style="list-style-type: none"> <li>1. Building height should be constructed as a focal point building, showcasing the importance of the Tranquille Commercial District consistent with the vision, goals and policies of the North Shore Neighbourhood Plan.</li> <li>2. Building height maximum can be increased, up to six (6) storeys, with the addition of a combination of market housing and non-market housing units, public art, landscaped and pedestrian-focused plazas, mixed-use development, green building design standards and eco-friendly stormwater systems and roofs.</li> </ol>



Examples of Infill Development in the Tranquille South Area; Source: NDLea Consultants Ltd.



Streetscape Example: Pedestrian Friendly, Visually Appealing; Mixed Uses  
Original Image Source: See Sources and References Section  
Drawing: Carla Stewart



# "A" - North Shore Development Guidelines

## **BROCK SHOPPING CENTRE**

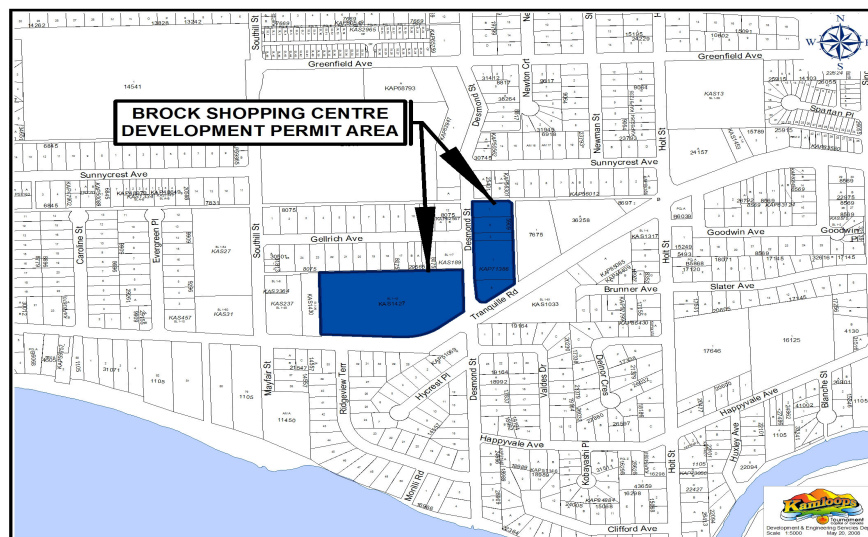
### **AREA SPECIFIC GUIDELINES**

#### **JUSTIFICATION**

The Brock Shopping Centre is the only main commercial focal point for one of the more heavily populated areas of the city. Designed as an auto-oriented shopping mall, the site comprises single-storey buildings sited to discourage pedestrian use with a predominant feature of hard surfaces including concrete and pavement. Community input during the preparation of the North Shore Neighbourhood Plan identified the Brock Shopping Centre as a potential village centre for Brocklehurst where pedestrian-oriented development could be located on site along with an increase in the number of neighbourhood commercial conveniences and with the introduction of multiple family residential uses. Achieving a village centre for Brocklehurst requires high design standards along with the use of innovative, creative and eco-friendly building and site design concepts that accommodate pedestrian activities.

#### **GUIDELINES**

The following Development Permit Guidelines are applicable to the area shown on the Map below and are intended to supplement the North Shore Development Guidelines as well as the city-wide Multiple Family Development Permit Area Guidelines. Development Permits issued in this area shall be in accordance with the following Guidelines:



# "A" - North Shore Development Guidelines

## BROCK SHOPPING CENTRE - SITE LAYOUT AND PARKING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> <li>1. Parking shall not be located in any front or side yard or along any street frontage.</li> <li>2. Site layout and building design, for those properties fronting Tranquille Road and Desmond Street shall have building frontages extend along the street frontage.</li> <li>3. Drive-thru aisles and idle lanes shall be located at a minimum setback of 30 m from any front and/or side street property line or any property line abutting a street.</li> </ol>	<ol style="list-style-type: none"> <li>1. Parking locations within front and side yards and along street frontages can occur where a minimum of a 4 m landscape buffer strip is provided between the parking area and a property line.</li> </ol>

## BROCK SHOPPING CENTRE - BUILDING SETBACKS

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> <li>1. Front yard setback minimum is 0 m.</li> <li>2. Side yard and/or side street yard setback minimum is 0 m.</li> <li>3. Rear yard setback minimum is 0 m.</li> <li>4. Rear yard setback minimum, where a lot is adjacent to a residential zone with no intervening lane or street, is 2 m.</li> </ol>	

## BROCK SHOPPING CENTRE - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> <li>1. Landscaping shall cover a minimum of ten (10) per cent of the total site area, not including the boulevard.</li> </ol>	<ol style="list-style-type: none"> <li>1. Landscaping minimum can be reduced, up to five (5) per cent of the total site area, with the use of eco-roofs and green infrastructure stormwater systems.</li> </ol>

# "A" - North Shore Development Guidelines

## **BROCK SHOPPING CENTRE - BUILDING DESIGN**

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"><li>1. Building design, for those properties fronting Tranquille Road, shall be designed and situated so as to create a landmark or focal point building for the Brock Shopping Centre. Landmark and focal point buildings shall incorporate significant architectural styles and features to distinguish them from adjacent properties and to create a positive impact on the landscape and in conjunction with the pedestrian movements and activities in the area.</li><li>2. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the Brock Shopping Centre in the North Shore Neighbourhood Plan.</li><li>3. Building design shall contribute to a lively and useable pedestrian scale over and above auto-oriented designs.</li></ol>	<ol style="list-style-type: none"><li>1. Building design should provide a continuous street frontage to encourage a pedestrian-oriented community.</li><li>2. Building design should provide ground floor commercial uses that are accessible at grade.</li><li>3. Building design should include a number of separate street front shops with separate entrances.</li></ol>

## **BROCK SHOPPING CENTRE - BUILDING MASSING**

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"><li>1. Building massing shall create pedestrian-oriented developments along street fronts and along major pedestrian thoroughfare routes, specifically along Tranquille Road.</li></ol>	<ol style="list-style-type: none"><li>1. Building massing shall accent focal points at primary sightlines along Tranquille Road.</li></ol>

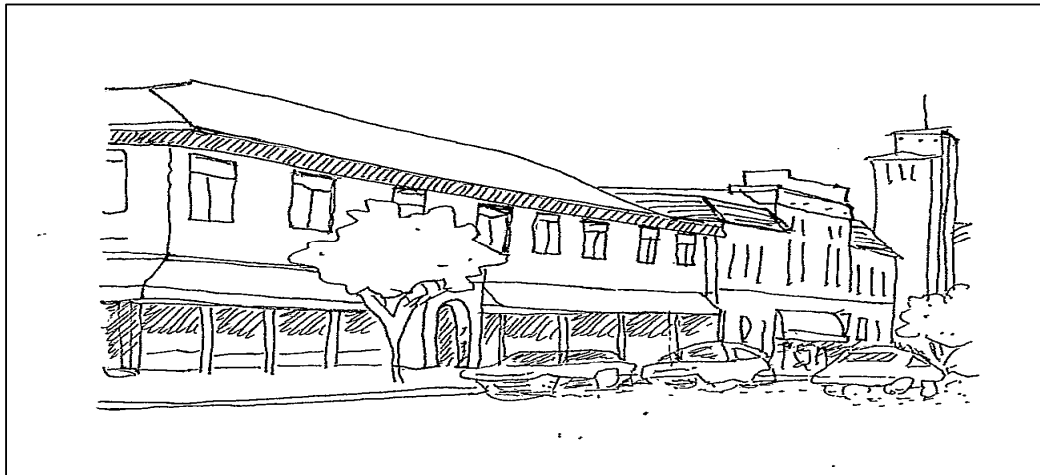
# "A" - North Shore Development Guidelines

## BROCK SHOPPING CENTRE - BUILDING HEIGHT

Required Elements	Recommended Elements
<ol style="list-style-type: none"> <li>1. Building height minimum is two (2) storeys.</li> <li>2. Building height maximum is two (2) storeys.</li> </ol>	<ol style="list-style-type: none"> <li>1. Building height should be constructed as a landmark or focal point building, showcasing the importance of the Brock Shopping Centre, consistent with the vision, goals and policies of the North Shore Neighbourhood Plan.</li> <li>2. Building height maximum can be increased, up to four (4) storeys, with the addition of a combination of market housing and non-market housing units, public art, landscaped and pedestrian-focused plazas, mixed-use development, green building design standards and eco-friendly stormwater systems and roofs.</li> </ol>



Examples of Infill Development in the Brock Shopping Centre; Source: NDLea



Streetscape Example: Pedestrian Friendly, Visually Appealing; Mixed Uses  
Original Image Source: See Sources and References Section  
Drawing: Carla Stewart

# "A" - North Shore Development Guidelines

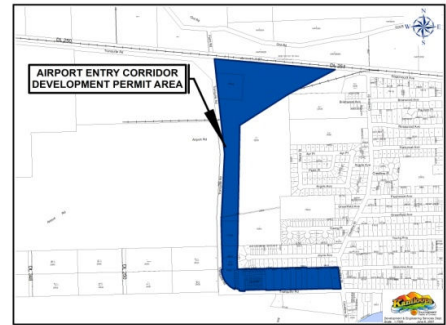
## ADDITIONAL AREA SPECIFIC

### DEVELOPMENT PERMIT GUIDELINES

#### JUSTIFICATION

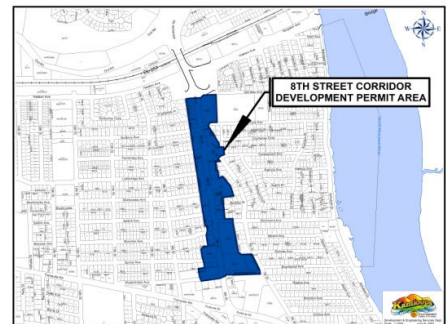
##### **Airport Entry Corridor:**

The Airport Entry Corridor is a significant gateway to the North Shore from the Kamloops Airport and from the agricultural lands and proposed destination resort to the west. Vacant land, remnants of heavy industrial uses and a mix of residential densities are scattered along the corridor. Community input during the preparation of the North Shore Neighbourhood Plan identified the Airport Entry Corridor as one of the most significant entry points to the North Shore that, when developed, should showcase the North Shore with buildings that portray an attractive and positive image and site layout. Achieving this requires design standards and the use of creative building and site design concepts.



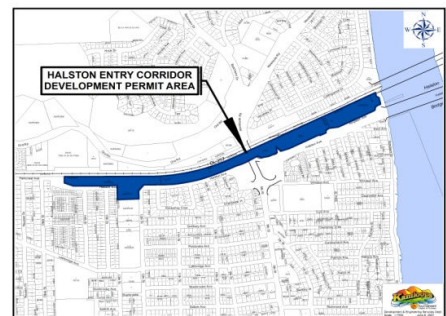
##### **8th Street Corridor:**

The 8th Street Corridor is a significant access point between the North Shore, Batchelor Heights, Westsyde, the Kamloops Indian Band and the nearby Yellowhead Highway. The current function of the 8th Street Corridor was established through an existing residential neighbourhood with a gradual transition toward commercial uses that are auto-oriented. Community input during the preparation of the North Shore Neighbourhood Plan identified the 8th Street Corridor as an area suited to auto-oriented uses but that when developed, also takes into consideration the high pedestrian traffic that exists, by increasing landscape buffer areas and by paying more attention to building design. Achieving this requires design standards and the use of creative site design.



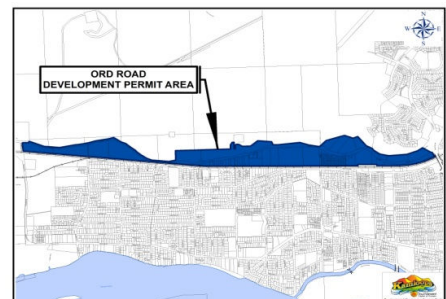
##### **Halston Entry Corridor:**

The Halston Entry Corridor is the eastern entrance point for the North Shore with concentrations of medium industrial uses, varied commercial activities, vacant land and residential development. Community input during the preparation of the North Shore Neighbourhood Plan identified the Halston Entry Corridor as a significant area that, when developed, should project a positive image for the North Shore both in building design and in site layout. Achieving this requires design standards along with the use of creative site design.



##### **Ord Road:**

The Ord Road Corridor is located parallel to a significant railway line and is geographically separated from most of the North Shore. Comprising a variety of land uses such as single family residential, mobile home parks, heavy industrial uses, recycling depots and small auto-oriented uses, Ord Road has long been an area of transition devoid of one cohesive identity. Community input during the preparation of the North Shore Neighbourhood Plan identified the Ord Road Corridor as an area for service commercial and light industrial uses. Achieving this requires design standards to ensure eco-friendly and community appropriate development takes place.





# "A" - North Shore Development Guidelines

## AIRPORT ENTRY CORRIDOR

### AREA SPECIFIC GUIDELINES

#### AIRPORT ENTRY CORRIDOR SPECIFICATIONS - SITE LAYOUT AND PARKING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"><li>1. Drive-thru aisles and idle lanes shall be located at a minimum setback of 30 m from any front and/or side street property line or any property line abutting a street. Drive-thru aisles and idle lanes shall be located a minimum setback of 2 m from any residential area.</li><li>2. Parking areas located within front yards and along street frontages shall be situated with a minimum of a 3 m landscape buffer strip between the parking area and the property line.</li></ol>	<ol style="list-style-type: none"><li>1. Parking areas located within front yards and along street frontages can be situated with a minimum of a 2 m landscape buffer strip between the parking area and the property line where eco-friendly storm drainage systems and/or green roofs are used in the site and building design.</li></ol>

#### AIRPORT ENTRY CORRIDOR SPECIFICATIONS - BUILDING DESIGN

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"><li>1. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the Airport Entry Corridor in the North Shore Neighbourhood Plan.</li><li>2. Building height maximum is two (2) storeys.</li><li>3. Building design shall contribute to a lively and useable pedestrian scale over and above auto-oriented designs.</li><li>4. Those buildings fronting Tranquille Road, shall be designed and situated so as to create a landmark or focal point building to reflect a major entrance to the North Shore and the city. Landmark or focal point buildings shall incorporate significant architectural styles and features to distinguish them from adjacent properties and to create a positive impact landscape and in conjunction with a pedestrian environment.</li></ol>	<ol style="list-style-type: none"><li>1. Building massing should avoid long, continuous and blank vertical surfaces throughout the entire development.</li><li>2. Building design should provide ground floor commercial uses that are accessible at grade.</li></ol>

#### AIRPORT ENTRY CORRIDOR SPECIFICATIONS - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"><li>1. Landscaping shall cover a minimum of ten (10) per cent of the total site area, not including the boulevard.</li><li>2. Auto-oriented, vinyl, back-lit podium or free standing signs shall not be permitted.</li></ol>	<ol style="list-style-type: none"><li>1. Landscaping minimum can be reduced, up to five (5) per cent of the total site area, with the use of eco-roofs and green infrastructure stormwater systems.</li></ol>

# "A" - North Shore Development Guidelines

## 8TH STREET CORRIDOR

### AREA SPECIFIC GUIDELINES

#### **8TH STREET CORRIDOR SPECIFICATIONS - SITE LAYOUT AND PARKING**

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"><li>1. Parking areas located within front yards and along street frontages shall be situated with a minimum of a 3 m landscape buffer strip between the parking area and the property line.</li><li>2. Drive-thru aisles and idle lanes shall be located adjacent to rear alleys and away from any street frontage.</li></ol>	

#### **8TH STREET CORRIDOR SPECIFICATIONS - BUILDING DESIGN**

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"><li>1. Building design shall consider the high level of pedestrian activity along this auto-oriented corridor.</li><li>2. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the 8th Street Corridor area in the North Shore Neighbourhood Plan.</li><li>3. Building height maximum is two (2) storeys.</li></ol>	<ol style="list-style-type: none"><li>1. Podium or free standing signs can occur where height and area are limited to a pedestrian scale and where natural materials and directional lighting is used, in conjunction with landscaping.</li><li>2. Building height maximum can be increased to four (4) storeys with the addition of market and non-market housing, a green building design standard and eco-friendly stormwater systems.</li></ol>

#### **8TH STREET CORRIDOR SPECIFICATIONS - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY**

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"><li>1. Landscaping shall cover a minimum of five (5) per cent of the total site area, not including the boulevard.</li><li>2. Podium and free standing signs can occur where height and area are limited to what is appropriate for a local road standard (50 km/hr) and where natural materials and directional lighting is used, in conjunction with landscaping.</li></ol>	<ol style="list-style-type: none"><li>1. Landscaping minimum can be reduced, up to three (3) per cent of the total site area, with the use of eco-roofs and green infrastructure stormwater systems.</li></ol>

# "A" - North Shore Development Guidelines

## HALSTON ENTRY CORRIDOR

### AREA SPECIFIC GUIDELINES

#### **HALSTON ENTRY CORRIDOR SPECIFICATIONS - SITE LAYOUT AND PARKING**

<u>Required Elements</u>	<u>Recommended Elements</u>
1. Drive-thru aisles and idle lanes shall be located at the rear of the property set back from any front and/or side street property line or any property line abutting a street.	

#### **HALSTON ENTRY CORRIDOR SPECIFICATIONS - BUILDING DESIGN**

<u>Required Elements</u>	<u>Recommended Elements</u>
1. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the Halston Entry Corridor in the North Shore Neighbourhood Plan.	1. Building height maximum can be increased to four (4) storeys with the addition of market and non-market housing, the use of eco-roofs and green infrastructure stormwater systems.
2. Building height maximum is two (2) storeys.	

#### **HALSTON ENTRY CORRIDOR SPECIFICATIONS - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY**

<u>Required Elements</u>	<u>Recommended Elements</u>
1. Landscaping shall cover a minimum of ten (10) per cent of the total site area, not including the boulevard.	1. Landscaping minimum can be reduced, up to five (5) per cent of the total site area, with the use of eco-roofs and green infrastructure stormwater systems.

# "A" - North Shore Development Guidelines

## ORD ROAD CORRIDOR

### AREA SPECIFIC GUIDELINES

#### **ORD ROAD CORRIDOR SPECIFICATIONS - SITE LAYOUT AND PARKING**

<u>Required Elements</u>	<u>Recommended Elements</u>
1. Site layout shall be accompanied by the submission of a geotechnical report and assessment that identifies any potential hazard due to rock fall from the steep slopes along the north side of Ord Road, west of McLean Street, and that also provides recommendations for mitigation of any potential rock fall hazard that may exist because of the steep slopes.	1. Site and parking layout should reflect the auto-oriented nature of the Ord Road Corridor.

#### **ORD ROAD CORRIDOR SPECIFICATIONS - BUILDING DESIGN**

<u>Required Elements</u>	<u>Recommended Elements</u>
1. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the Ord Road Corridor in the North Shore Neighbourhood Plan.	

#### **ORD ROAD CORRIDOR SPECIFICATIONS - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY**

<u>Required Elements</u>	<u>Recommended Elements</u>
1. Landscaping shall cover a minimum of ten (10) per cent of the total site area, not including the boulevard.  2. Podium and free standing signs can occur where height and area are limited to what is appropriate for a local road standard (50 km/hr) and where natural materials and directional lighting is used, in conjunction with landscaping.	1. Landscaping minimum can be reduced, up to five (5) per cent of the total site area, with the use of eco-roofs and green infrastructure stormwater systems.

# "A" - North Shore Development Guidelines



## Appendix "B" - Development Checklist

# "B" - Development Checklist

## INTRODUCTION

The North Shore is a varied, compact and well-established combination of neighbourhoods. The diversity of services, residential accommodation and recreational opportunities compliments a lifestyle where residents can walk, cycle or take transit for their daily needs and where access to leisure and activity space is nearby and readily accessible by all age and income groups. Building on, enhancing and embracing this lifestyle is a key component to the goals of the North Shore Neighbourhood Plan. In order to fulfill that commitment, it is essential that development compliment the values and desires of the community to ensure the identified vision is kept in sight as change takes place.

An essential component of change in any urban environment is the accommodation of new development into an existing fabric of buildings, people and patterns. Those who are involved with new development, whether it be consultants, developers or property owners, are key players in achieving a community's long-term goals. As such, a development checklist has been created to assist property owners, developers and consultants to create the most sustainable, innovative, appropriate and long-lasting projects possible.

The use of this Development Checklist is meant to advance the goals of the North Shore Neighbourhood Plan, as supported by the North Shore community. It is separated into six sections:

**Regulatory Conditions** - includes the policies, by-laws and plans that govern the use and form and character that regulate development within the North Shore.

**Public Health and Safety** - refers to features of the proposed development that affect the public interest in ways that lead to a safe and secure environment as well as an enhanced well-being for the community.

**Urban Design** - pertains to the architectural elements, selected materials and public realm enhancements that work toward creating an environment with visual and functional appeal, actively working toward improving the public realm.

**Social Sustainability** - refers to the ability for the development to contribute to the social well-being of the community through economic enhancement, provision of public services and amenities, supply of affordable housing and other measures that can positively impact the social realm of the community.

**Site Access, Connectivity and Parking** - refers to the movement of people and vehicles, on and off site, as well as to how well the site performs in terms of connection to the adjacent and surrounding road network.

**Environmental Sustainability** - refers to the ability of the development to contribute to an enhanced community environment through the reduction in the consumption of non-renewable resources, sound stewardship of environmental resources, and a reduction in the pressures of infrastructure systems.

# "B" - Development Checklist

**SUBMIT A COMPLETE DEVELOPMENT CHECKLIST, WITH SUPPORTING DOCUMENTATION, WITH YOUR DEVELOPMENT APPLICATION.**

## DEVELOPMENT INFORMATION

Please provide the following information regarding your development proposal:

1. Name of Applicant:	
2. Subject Property Address:	
3. Legal Description:	
4. Date of Application:	

## INSTRUCTIONS

All applications for Zoning By-law Amendments, Development Permits, Development Variance Permits, Official Community Plan Amendments and Temporary Commercial or Industrial Use Permits are required to complete this Development Checklist according to the following steps:

1. Review and complete the Development Checklist (note: partial points are not permitted; items must be completed in order to receive point tallies).
2. Submit Development Checklist with the above noted applications.
3. *Submit any documentation, plans and information that substantiate and coordinates with the Development Checklist items that were identified for the proposed development.*
4. If needed, prepare a supplementary letter explaining, in more detail, how the proposed development incorporates the features identified in the North Shore Neighbourhood Plan Goals Section.
5. Staff will review the Development Checklist, in conjunction with your application information and provide comments on your submitted materials.
6. If additional information or changes to submissions are required, re-submit the changes, addressing comments received from staff.
7. *Your Development Checklist and application material will become your official application and will be forwarded to City Council for review.*

Applicants are encouraged to provide as much information as possible to assist City Council and staff on their review of development proposals. The relevance of the Development Checklist questions will depend on the nature and scope of the project. The intent of the Development Checklist is to assist applicants and the City in working together to develop high quality projects that are a benefit and lasting legacy to the community.

# "B" - Development Checklist

## A. REGULATORY CONDITIONS

The North Shore Neighbourhood Plan is a comprehensive document comprising policies, zoning regulations and Development Permit Guidelines established and supported by the North Shore community, to guide development over the next 5 - 10 years. Ensuring that development proposals meet the policies and by-laws that govern the uses and form and character of the North Shore is paramount to the success of the Plan.

*Regulatory Conditions* - includes the policies, by-laws and plans that govern the use and form and character that regulate development within the North Shore.

Features		Point Value	Points Earned	Supporting Comments and Documentation
<u>Regulatory Support</u>				
a.1	Development meets the policies for development as expressed in the North Shore Neighbourhood Plan.	10		
a.2	Development meets regulatory criteria as expressed in the North Shore Development Permit Guidelines.	10		
a.3	Development meets regulatory criteria as expressed in the North Shore Zoning Code.	10		
<b>Total Points Earned</b> (Minimum Base Points Required = 30)				
Regulatory Conditions Minimum Achieved?			Y / N	

# "B" - Development Checklist

## B. PUBLIC HEALTH AND SAFETY

Design of the urban environment can incorporate elements that reduce crime nuisances. This improves the livability and sustainability of the urban environment through reducing both risk and fear of crime. Preventing crime before it happens saves enormous costs for property owners and the community as a whole. The most effective way of designing for crime prevention is to integrate a comprehensive crime prevention program review, such as CPTED (Crime Prevention Through Environmental Design), at the early stages of the development design process.

*Public Health and Safety* - refers to features of the proposed development that affect the public interest in ways that lead to a safe and secure environment as well as enhanced well-being for the community.

Features		Point Value	Points Earned	Supporting Comments and Documentation
<b>Safety and Security</b>				
b.1	Design generally incorporates CPTED principles.	10		
b.2	Conducted and prepared a CPTED review analysis of the site.	10		
b.3	Design specifically addresses identified travel routes, gathering places and conflict areas in and around the development site.	10		
b.4	Neighbourhood has been contacted to obtain a clear picture of issues that exist within the community in conjunction with the development site.	5		
b.5	Design does not create dark, dead-end spaces and allows for pedestrians to have a clear view of what is ahead of them.	5		
b.6	Landscaping type and placement does not create hiding places or block view sight lines.	10		
<b>Innovation and Design</b>				
b.7	Development adds other unique or innovative features not covered by the above checklist. Provide details.	5		
<b>Total Points Earned</b> (Minimum Base Points Required = 20)				
Public Health and Safety Minimum Achieved?			Y / N	



# "B" - Development Checklist

## C. URBAN DESIGN

Urban Design strives to create urban spaces based on the strength of the existing character and identity of a place. It also strives to create continuity for ease of movement, to establish quality public spaces, to create cities that have the ability to adapt easily over time and to provide choice and variety. New development should examine its impact on existing and adjacent uses to ensure an appropriate fit is being made.

*Urban Design* - pertains to the architectural elements, selected materials and public realm enhancements that work toward creating an environment with visual and functional appeal, actively working toward improving the public realm.

Features		Point Value	Points Earned	Supporting Comments and Documentation
<b>Building Design</b>				
c.1	Quality of the building design and exterior materials is high (e.g. using a combination of timber, stone, brick, concrete, metal and glass ) and creates visual interest.	20		
c.2	Timber, stone, brick and transparent glass are substantial parts of building design.	5		
c.3	Development is compatible with, and complimentary to, existing and/or planned developments, in terms of architecture, character, scale, massing, height, setbacks and open space.	10		
c.4	Residential design incorporates balconies suitable for active use and seating.	10		
c.5	Developments are designed and sited to reduce their intrusion on the privacy of adjacent properties.	5		
<b>Site Context</b>				
c.6	Design provides a landmark or focal point feature consistent with site context.	5		
c.7	Buildings front onto public streets and have 'active' frontages with windows, doorways, decks, etc. which allow interaction between people in the building and people on the street.	10		
c.8	Solid, blank walls at street level are avoided.	10		
c.9	Public and private open spaces are created that have a high (e.g. exceeds the minimum standards established in the zoning regulations, Development Permit Guidelines and Landscape Guidelines) standard of landscaping and include amenities such as benches, waste receptacles, special lighting, water features and public art.	5		
c.10	Ground floor commercial is pedestrian-oriented with separate storefronts opening onto the public right-of-way.	5		

(Cont...)

# "B" - Development Checklist

## C. URBAN DESIGN (cont.)

Features		Point Value	Points Earned	Supporting Comments and Documentation
<b>Public Realm Enhancements</b>				
c.11	Mature trees are retained on site.	2		
c.12	High (e.g. exceeds the minimum standards established in the zoning regulations, Development Permit Guidelines and Landscape Guidelines) quality landscaping is provided.	10		
c.13	Public spaces encourage public life on the street and address sun, shade, wind, safety, and weather protection.	5		
c.14	Lighting systems create visual interest, are pedestrian-oriented and do not create light pollution.	10		
c.15	Buildings do not create shadow or shade conditions that negatively impact on the site or on adjacent properties.	10		
c.16	Utility and service areas and equipment are screened from adjacent streets and properties.	1		
c.17	Utility and service areas are screened with high (e.g. exceeds the minimum standards established in the zoning regulations, Development Permit Guidelines and Landscape Guidelines) quality landscaping.	5		
<b>Signage</b>				
c.18	Signage is incorporated into building design in a coordinated and visually appealing manner, including sign lighting methods.	5		
c.19	Signage is pedestrian-oriented and designed using high-quality material that reflects the materials used on the corresponding development.	5		
<b>Innovation in Design</b>				
c.20	Development adds other unique or innovative features not covered by the above checklist. Provide details.	5		
<b>Total Points Earned</b> (Minimum Base Points Required = 70)				
Urban Design Minimum Achieved?			y / n	

# "B" - Development Checklist

## D. SOCIAL SUSTAINABILITY

The primary purpose of a city is to provide for the well-being of its residents, businesses and visitors. New development should contribute to the health and safety of the North Shore, as well as enhance the range of housing, services and recreational options and commercial opportunities, to meet the community's needs. The design of new development should reflect the local heritage and provide attractive public spaces that encourage social interaction.

*Social Sustainability* - refers to the ability for the development to contribute to the social well-being of the community through economic enhancement, provision of public services and amenities, supply of affordable housing and other measures that can positively impact the social realm of the community.

Features		Point Value	Points Earned	Supporting Comments and Documentation
<u>Economic Benefits</u>				
d.1	Permanent, local employment is created during construction.	1		
d.2	Direct employment to be created after construction.	1		
d.3	Other components of economic sustainability (e.g. flex suites, home-based business adaptable) are created.	1		
d.4	Residential units are considered affordable (based on CMHC definition).	10		
<u>Non-Market Housing</u>				
d.5	Non-market housing is included.	5		
d.6	At least 20% of units are non-market housing.	5		
d.7	At least 50% of units are non-market housing.	7		
d.8	At least 80% of units are non-market housing.	10		
d.9	Non-market housing (minimum 15%) is included with market housing in the same development.	10		

(Cont...)

# "B" - Development Checklist

## D. SOCIAL SUSTAINABILITY (cont.)

Features		Point Value	Points Earned	Supporting Comments and Documentation
<u>Rental Housing</u>				
d.10	Rental housing is included.	5		
d.11	At least 20% of the units are rental units.	1		
d.12	At least 50% of the units are rental units.	2		
d.13	At least 80% of the units are rental units.	3		
d.14	Rent is considered affordable (based on CMHC definition).	1		
d.15	Housing Agreement used to guarantee a minimum of ten years of rent control.	5		
<u>Accessibility</u>				
d.16	Accessibility features (e.g. handicapped-oriented suites, adaptable rental units) are included.	5		
d.17	Development is ground-oriented with entrances off of public right-of way.	5		
<u>Local Identity Reinforcement</u>				
d.18	Development enhances local identity and character (e.g. see Sections 3.3 and 3.6 under General Regulations for details pertaining to local identity and character).	5		
d.19	Development incorporates horizontal and vertical mix of uses.	5		
d.20	Development contributes to heritage revitalization.	1		
d.21	Heritage buildings municipally designated.	1		
<u>Social Amenities</u>				
d.22	Public art (e.g. murals, sculptures, reader boards, etc.) is provided.	1		
d.23	A child care facility is integrated into the development.	1		
d.24	Other public social amenities (such as water features, benches etc.) are provided.	1		
d.25	A Green Roof, to a minimum of 50% of the total roof area, is provided as amenity space.	2		
d.26	Private amenities (such as pools, daycare, recreation rooms) are provided.	1		

(Cont...)

# "B" - Development Checklist

## D. SOCIAL SUSTAINABILITY (cont.)

Features		Point Value	Points Earned	Supporting Comments and Documentation
<u>Planning Process</u>				
d.27	Residents, community stakeholders and end-user groups were involved in the planning, design and development process.	2		
<u>Innovation in Design</u>				
d.28	Development adds other unique or innovative features not covered by the above checklist. Provide details.	5		
<b>Total Points Earned</b> (Minimum Base Points Required = 20)				
Social Sustainability Minimum Achieved?			Y / N	



# "B" - Development Checklist

## E. SITE ACCESS, CONNECTIVITY AND PARKING

The North Shore is one of Kamloops' most walkable and accessible neighbourhoods. Protecting that walkability and ensuring that service and shopping opportunities remain available to pedestrians requires supporting and encouraging residential development to locate within close proximity to community amenities, health facilities and parkland. Development is located close to urban cores and in more dense environments should be considered first above peripheral, auto-dependent sites.

*Site Access, Connectivity and Parking* - refers to the movement of people and vehicles, on and off site, as well as how well the site performs in terms of connection to the adjacent and surrounding road network.

Features		Point Value	Points Earned	Supporting Comments and Documentation
<u>200 m (2 1/2-minute) Connectivity</u>				
e.1	Development located within 200 m of a public transit bus stop.	2		
e.2	Development located within 200 m of recreation trails.	1		
e.3	Development located within 200 m of a clearly defined commercial area.	2		
<u>400 m (5-minute) Connectivity</u>				
e.4	Development located within 400 m of a neighbourhood store.	1		
e.5	Development located within 400 m of a school.	1		
e.6	Development located within 400 m of a community service.	1		
e.7	Development located within 400 m of a child care facility.	1		
e.8	Development located within 400 m of a health service.	1		
e.9	Development located within 400 m of a park or trails.	1		
e.10	Development located within 400 m of a public transit bus stop.	1		
e.11	Development located within 400 m of a minimum of 4 (four) of the above criteria.	10		

(Cont...)

# "B" - Development Checklist

## E. SITE ACCESS, CONNECTIVITY AND PARKING (cont.)

Features		Point Value	Points Earned	Supporting Comments and Documentation
<u>Site Connectivity</u>				
e.12	The pedestrian network is well integrated throughout the site.	1		
e.13	Buildings are oriented along pedestrian routes, or sidewalks, to provide passive surveillance.	1		
e.14	Laneways are provided and optimized for garbage and/or service access to minimize sidewalks being crossed over by driveways.	1		
e.15	The pedestrian network from adjacent sites is incorporated in site design.	1		
<u>Transportation Features</u>				
e.16	Covered and secure bicycle storage is provided.	5		
e.17	Change rooms are provided.	1		
e.18	Designated carpool parking is provided.	1		
e.19	Development incorporated Transportation Demand Management principles.	1		
e.20	Development incorporates an innovative parking and transportation strategy.	2		
<u>Parking</u>				
e.21	Surface parking is located to the side or rear of the building.	5		
e.22	Surface parking areas are landscaped and/or screened.	2		
e.23	A minimum of 80% of residential parking is located underground or in a parking structure incorporated into the design of the building.	5		
e.24	Parking is provided for underground or incorporated into the design of the building with an 'active' (refer to "c.7" in this Checklist) frontage.	5		
<u>Innovation in Design</u>				
e.25	Development adds other unique or innovative features not covered by the above checklist. Provide details.	5		
<b>Total Points Earned</b> (Minimum Base Points Required = 20)				
Site Access, Connectivity and Parking Minimum Achieved?			Y / N	

# "B" - Development Checklist

## F. ENVIRONMENTAL SUSTAINABILITY

Community and building design can significantly influence the resource consumption (e.g. energy and water) and the waste (e.g. vehicle emissions, stormwater run-off) produced in a local community. New development on the North Shore should be designed to minimize negative impacts on the natural environment and should showcase and provide an example of green development for the rest of Kamloops to emulate and implement.

*Environmental Sustainability* - refers to the ability of the development to contribute to an enhanced community environment through the reduction in the consumption of non-renewable resources, sound stewardship of environmental resources, and a reduction in the pressures of infrastructure systems.

Features		Point Value	Points Earned	Supporting Comments and Documentation
<u>Green Building Certification</u>				
f.1	The development is LEED/Built Green certified.	5		
f.2	The development reached LEED/Built Green silver.	15		
f.3	The development reached LEED/Built Green gold.	20		
f.4	The development reached LEED/Built Green platinum.	25		
<u>Environmental Stewardship</u>				
f.5	Greenspace (e.g. landscaped areas that have shrubs, trees and/or grass) for the development is provided.	1		
f.6	Trees are added to the landscaped areas.	1		
f.7	Significant environmental features are maintained and/or enhanced.	1		
<u>On-site Stormwater Management</u>				
f.8	Rain gardens, detention ponds and other on-site stormwater management features are incorporated into landscaped areas.	5		
f.9	Green Roof installed to a minimum of 50% of the total roof area.	5		
f.10	Green Street used in development.	2		
<u>Water Efficiency</u>				
f.11	Water efficient landscaping used.	2		
f.12	Non-potable water system installed and used for irrigation.	2		
f.13	Innovative wastewater technologies used (e.g. low consumption fixtures, grey water systems, stormwater irrigation).	2		

(Cont...)

# "B" - Development Checklist

## F. ENVIRONMENTAL SUSTAINABILITY (cont.)

Features		Point Value	Points Earned	Supporting Comments and Documentation
<u>Good Neighbour Features</u>				
f.14	Noise mitigation building design and features used in development.	1		
f.15	Sign and building light pollution minimized.	1		
<u>Construction Management</u>				
f.16	Majority of materials from regional sources.	2		
f.17	Renewable resources used in construction.	1		
f.18	Durable and long-lasting construction materials used.	1		
<u>Energy Conservation</u>				
f.19	Integration of solar power into building design and construction, including the use of design to orient buildings to maximize interior solar accessibility.	1		
f.20	Power Smart and CFC-reducing HVAC systems used in the building.	1		
<u>Site Development</u>				
f.21	Development connects well with surroundings and integrates into the existing character and function of the neighbourhood.	2		
f.22	Wildlife habitat on or adjacent to the site has been protected, enhanced and/or restored.	1		
f.23	Open space exceeds minimum zoning requirements on site (e.g. use of clustering, reduced lot coverage, etc.)	1		
f.24	Heritage features have been preserved and/or enhanced (in conjunction with the Canadian Historic Places Standards and Guidelines).	2		
<u>Innovation in Design</u>				
f.25	Development adds other unique or innovative features not covered by the above checklist. Provide details.	5		
<b>Total Points Earned</b> (Minimum Base Points Required = 35)				
Environmental Sustainability Minimum Achieved?			Y / N	

# "B" - Development Checklist

## H. INCENTIVE LEVEL WORKSHEET (For Office Use Only)

Development incentives will be determined through negotiations with Development and Engineering Services staff, through the development application process and, depending on the type of incentive received, may require approval from City Council.

This Incentive Level Worksheet will be filled out by City Staff confirming the results of the Development Checklist, the Incentive Level reached and the types and amounts of incentives that the development proposal will be eligible for.

Development Checklist				
<i>Regulatory Conditions</i> Minimum Achieved?	yes	no		
<i>Public Health and Safety</i> Minimum Achieved?	yes	no		
<i>Urban Design</i> Minimum Achieved?	yes	no		
<i>Social Sustainability</i> Minimum Achieved?	yes	no		
<i>Site Access, Connectivity and Parking</i> Minimum Achieved?	yes	no		
<i>Environmental Sustainability</i> Minimum Achieved?	yes	no		
Incentive Level				
Number of Checklist Section Minimums Achieved	Less than 4	4	5	6
Incentive Level Reached for Development Proposal	Base	Level 1	Level 2	Level 3
Incentive Amounts (refer to attached Development Incentive Matrix)				
Tax Reduction Amount (%)				
DCC Reduction Amount - Non-market Housing Residential Unit Amount (%) - Green Development Related Amount (%)				
Parking Relaxation Amount (%)				
Amenity Bonus Increase (%)				
Public Realm Partnership Amount Recommended (%)				
Planning Priority Level				
Checklist Approval				
All Supporting Documentation Submitted?	yes	no		
Checklist Approved by Development Services Department	yes	no		
File Manager:	Date Signed:			
Signature:				



# "B" - Development Checklist

## DEVELOPMENT INCENTIVE MATRIX

The following Development Incentive Matrix is an excerpt from the North Shore Neighbourhood Plan (see page 136) and is to be used in conjunction with the Development Checklist to determine if incentives apply to a development proposal, and if so, to what level. Development Incentive Levels need to be discussed with Development and Engineering Services Department staff and some incentives require approval by City Council.

Development Checklist Section	Base Level	Level 1	Level 2	Level 3
Section Minimums refer to the minimum point level for each Section listed within the Development Checklist.	3 or fewer Section Minimums met	4 out of 6 Section Minimums met	5 out of 6 Section Minimums met	6 out of 6 Section Minimums met
Development Incentive Tool	Reduction Amount	Reduction Amount	Reduction Amount	Reduction Amount
Tax Exemption <sup>1</sup> :	n/a	20%	40%	60%
Within North Shore Towne Centre	n/a	additional 20%	additional 20%	additional 20%
Within Tranquille Commercial District	n/a	additional 15%	additional 15%	additional 15%
Within Tranquille South	n/a	additional 10%	additional 10%	additional 10%
Within Brock Shopping Centre	n/a	additional 10%	additional 10%	additional 10%
Within Halston Entry Corridor or 8th Street	n/a	additional 5%	additional 5%	additional 5%
All Other Areas	n/a	n/a	n/a	n/a
Mixed-use Green Development	n/a	additional 10%	additional 10%	additional 10%
Green Development	n/a	additional 5%	additional 5%	additional 5%
Combined Market and Non-market Housing	n/a	additional 10%	additional 10%	additional 10%
DCC Reduction <sup>2</sup> :				
Social Housing Units	100%	100%	100%	100%
Small Housing Units	TBD	TBD	TBD	TBD
Green Development	n/a	TBD	TBD	TBD
Parking Requirement Relaxation	n/a	5%	10%	25%
Density Bonus <sup>3</sup>	n/a	25%	50%	Maximum Permitted
Public Realm Partnering <sup>4</sup>	n/a	25%	35%	50%
Planning Process Priorities <sup>5</sup>	Normal Process	Normal Process	Expedited Process	Expedited Process

<sup>1</sup>Exemption Periods are for a maximum of 10 years; exemptions calculated on the increase in total assessed value, post-construction; minimum 50 per cent of roof coverage applies to green roof installation; minimum of 50 per cent of units designated as affordable for combined market and non-market housing developments is required;

<sup>2</sup>DCC Reductions pertain to social housing units only; Green Development DCC reductions are "To Be Determined" as part of the Green DCC review taking place 2008-2009; Small Housing Units are self-contained and no larger than 30 m<sup>2</sup>.

<sup>3</sup>Density Bonus is an increase limited to the maximum densities identified within Section Four of this Plan and within the North Shore Development Permit Area Guidelines;

<sup>4</sup>Public Realm Partnering (to a maximum of \$100,000) requires the approval of City Council;

<sup>5</sup>Planning Process Priorities will be expedited for all Level 1 developments that incorporate a minimum of 50 per cent of the total units as non-market housing.

## Appendix "C" - Capital Projects and Action Items

# "C" - Capital Projects and Action Items

## CAPITAL PROJECTS PRIORITIES

There are several capital projects identified to be completed on the North Shore over the next 5 - 10 years. These projects are listed here with estimated costs. Additional projects are also listed here as priorities for the North Shore to take place within the lifespan of the North Shore Neighbourhood Plan. This Capital Projects list is expected to expand and be adjusted over time as additional priorities are identified and as funding becomes available.

Project	Estimated Cost	Priority	Comments
Tranquille Road and Yew Street Roundabout	To Be Determined	Medium	Does not include the cost to purchase any adjacent land; if required.
Fortune and Leigh Street Trees	\$34,800	Medium	Cost estimate obtained by Parks Department in late 2006
Fortune Frontage Road Street Trees	\$58,612	Medium	Cost estimate obtained by Parks Department in late 2006
Green Street Pilot Project	approx \$650,000	High	Fleetwood Avenue is currently being reviewed for this project
McDonald Park Community Garden	\$7,000	Low	Identified in the McDonald Park Neighbourhood Plan
MacKenzie, McArthur Island and Kenora Traffic Calming	\$87,000	High	Identified in the McDonald Park Neighbourhood Plan
McDonald Park Neighbourhood Garden Bulges	To Be Determined	High	Identified in the McDonald Park Neighbourhood Plan
Tranquille Road/Airport Street Trees	\$62,400	Medium	Cost estimate obtained by Parks Department in late 2006
Singh Street Green Street Upgrades	To Be Determined	Medium	
Simcoe Avenue Redevelopment	To Be Determined	Medium	Dependent on private investment and redevelopment proposals; only includes upgrades to public areas
Ord Road Upgrading	To Be Determined	High	
Yew and Mackenzie Spirit Square	up to \$500,000	High	In conjunction with Provincial Funding
Transit Exchange	\$900,000	High	Federal Funding
12th Street, Kenora and McArthur Island Traffic Calming	\$116,000	High	
Airport Entry Corridor Beautification	To Be Determined	Medium	
Tranquille Commercial District Lighting Upgrades	To Be Determined	High	

# "C" - Capital Projects and Action Items

## ACTION ITEMS

Page	Action Item	Section	Priority
39	Prepare a strategy plan with the North Shore Business Improvement Association, with input from Venture Kamloops, to help increase awareness of the North Shore's business opportunities and to promote the benefits of locating businesses on the North Shore.	General Policies: Economic Growth	High
39	Prepare a revitalization strategy for the North Shore Towne Centre, Tranquille Commercial District and Tranquille South area, that acts as a showcase for the North Shore's main commercial areas.	General Policies: Economic Growth	High
39	Prepare a land inventory identifying development opportunities within the commercial and industrial areas of the North Shore.	General Policies: Economic Growth	High
42	Develop a program to encourage, educate and provide incentives for property maintenance and beautification for both residential and commercial properties.	General Policies: Community Improvements	High
42	Develop a program for the installation and maintenance of landscaping and beautification treatments within the Tranquille Commercial District, the North Shore Towne Centre and the Tranquille South area as a cost share initiative between the City, the North Shore Business Improvement Association and local North Shore businesses.	General Policies: Community Improvements	High
47	Incorporate a CPTED review into the City's development application review process.	General Policies: Community Form and Identity	High
47	Incorporate RCMP input into the City's development application review process.	General Policies: Community Form and Identity	High
47	Conduct a public lighting system feasibility study for the North Shore Towne Centre, Tranquille Commercial District and Tranquille South area to identify improvements and implementation of upgrades.	General Policies: Community Form and Identity	High
47	Develop programs to educate City Council, City staff, architects, developers and designers on CPTED and/or crime reduction principles.	General Policies: Community Form and Identity	High
54	Create a Green Infrastructure Best Management Practices Guide for the North Shore providing specifics on green streets designs, eco-stormwater systems and green roof systems.	General Policies: Alternative Development Standards	High
54	Examine the feasibility of preparing an integrated stormwater management system that takes into account sustainable forms of infrastructure development, establishing a connected and comprehensive system for the North Shore.	General Policies: Alternative Development Standards	High
60, 63	Incorporate the Development Checklist into the processing of development applications.	General Policies: Urban Design and Development; Sustainability	High
62	Examine the use of the Housing Reserve Fund to directly support and encourage the construction of non-market housing units.	General Policies: Non-Market Housing	High
68, 88	Identify the feasibility of establishing additional transit coverage throughout the North Shore.	General Policies: Transportation and Mobility; and Land Use Policies: North Shore Towne Centre	High
70	Prepare a by-law and covenant, to be used for the properties falling within the Airplane Noise Exposure Contours area, requiring additional soundproofing for residential construction.	General Policies: Transportation and Mobility	High
74	Evaluate the use of water conservation methods for residential and commercial development.	General Policies: Servicing and Utilities	High
83	Develop a streetscape enhancement program for Tranquille Road and for main intersections within the Tranquille Commercial District.	Land Use Policies: Tranquille Commercial District	High

(cont...)

# "C" - Capital Projects and Action Items

## ACTION ITEMS (cont.)

Page	Action Item	Section	Priority
103	Undertake a comprehensive development review of the Henry Grube property to determine appropriate future uses for the site.	Land Use Policies: Schubert Drive Neighbourhood	High
113	Identify required road upgrades needed to accommodate further commercial and industrial traffic along Ord Road.	Land Use Policies: Ord Road District	High
118	Dedicate an east/west commuter cycle route from Brocklehurst to Schubert Drive and/or to McArthur Island to create a formalized cycle route to the Overlanders Bridge.	Land Use Policies: Brocklehurst	High
132	Prepare a new Development Cost Charge By-law incorporating reductions for development that uses sustainable building and site construction and design methods.	Implementation: Implementation Barriers	High
137	Prepare new sign regulations for the North Shore, consistent with the North Shore Development Permit Guidelines.	Implementation: Sign Regulations	High
38, 49	Develop density transfer opportunities for residential and mixed-use developments within the Tranquille Commercial District and the North Shore Towne Centre.	General Policies: Growth Management; Accessibility	Medium
42	Acquire property for strategic purposes, involving such matters as parkland, transportation improvements, urban beautification efforts and off-street parking.	General Policies: Community Improvements	Medium
44	Establish a "Neighbourhood Pride" program, in conjunction with stakeholders and businesses, to foster a sense of neighbourhood identity and pride and to conduct programs such as Neighbourhood Watch, Adopt-a-Street and other community activities.	General Policies: Community Form and Identity	Medium
44	Partner with the Kamloops Food Policy Council to explore developing Community Gardens on the North Shore.	General Policies: Community Form and Identity	Medium
49	Prepare a priorities list of infrastructure upgrades to work toward improving accessibility between existing neighbourhoods and between residential and commercial areas along major pedestrian traffic routes.	General Policies: Accessibility	Medium
52	Include heritage conservation policies for the North Shore within the City of Kamloops Heritage Strategic Management Plan.	General Policies: Heritage Conservation	Medium
54	Develop a program to help landowners retrofit existing buildings to become sustainable by using green infrastructure systems.	General Policies: Alternative Development Standards	Medium
59	Examine creating a stewardship program and trail connectivity strategy with BC Parks and other appropriate Ministries, for the Lac Du Bois grasslands, to promote the protection, maintenance and enhancement of the existing grasslands ecosystem.	General Policies: Environmentally Sensitive Areas/Grasslands	Medium
63	Create a Sustainability Officer, to act as a coordinator between City departments, to ensure the principles and concepts of sustainable development are instituted into the Best Management Practices of the City of Kamloops.	General Policies: Sustainability	Medium
63	Create a 'Replace-the-Turf' Program which offers financial and informational assistance to homeowners wishing to remove turf and replace it with xeriscape landscaping treatments.	General Policies: Sustainability	Medium
63	Coordinate preparation of the proposed Greenways Plan, Sustainability Plan and the revision of the Parks Master Plan with the policies of the North Shore Neighbourhood Plan.	General Policies: Sustainability	Medium
68	Identify the top five commuter streets for pedestrians and cyclists to upgrade for accessibility by all citizens of Kamloops.	General Policies: Transportation	Medium
68	Create Transit-Oriented Development Guidelines for the Subdivision By-law.	General Policies: Transportation	Medium
68, 88	Prepare a Pedestrian Management Plan to identify areas for improvement to increase safety, visibility and accessibility for pedestrians within the North Shore Towne Centre, Tranquille Commercial District and Tranquille South area.	General Policies: Transportation; and Land Use Policies: North Shore Towne Centre	Medium

(cont...)



# "C" - Capital Projects and Action Items

## ACTION ITEMS (cont.)

Page	Action Item	Section	Priority
78	Work with the North Shore Business Improvement Association to install information and identification signage about the North Shore within the North Shore Entry Corridor.	Land Use Policies: North Shore Entry Corridor; Halston Entry Corridor	Medium
83	Maintain and enhance a vibrant and mixed-use Commercial District by establishing a long-term program of community improvement projects and a strategy for implementation and improvements in a planned and coordinated manner.	Land Use Policies: Tranquille Commercial District	Medium
83	Develop a comprehensive parking strategy for the Tranquille Commercial District.	Land Use Policies: Tranquille Commercial District	Medium
101	Identify appropriate Tranquille Road and Fortune Drive access points from the John Tod Neighbourhood to both the McDonald Park Neighbourhood and Schubert Drive Neighbourhood, to ensure those access points provide full mobility function for all citizens, are well marked and lit and function in an efficient manner for pedestrian and cycle movements.	Land Use Policies: John Tod Neighbourhood	Medium
109	Undertake a transportation and mobility study for the 8th Street Corridor to determine additional east/west connections between North Kamloops West and East neighbourhoods to create alternatives for transportation modes and to create an efficient and safe community.	Land Use Policies: 8th Street Corridor	Medium
111	Work with the North Shore Business Improvement Association to install information and identification signage about the North Shore within the Halston Entry Corridor.	Land Use Policies: Halston Entry Corridor	Medium
111	Develop a beautification plan for the Halston Entry Corridor, including the Halston Avenue/8th Street intersection.	Land Use Policies: Halston Entry Corridor	Medium
118	Identify and work toward eliminating any physical or functional barriers that exist that would prevent efficient and convenient cycle travel within and to and from the North Shore.	Land Use Policies: Brocklehurst	Medium
38	Establish a new Lac Du Bois Grasslands Park boundary, in conjunction with identifying an urban growth boundary, for the northwestern limits of the City of Kamloops.	General Policies: Growth Management	Low
52	Engage in a planning process to create, where appropriate, Heritage Conservation Areas, for the McDonald Park, John Tod, North Kamloops West and Schubert Drive Neighbourhoods.	General Policies: Heritage Conservation	Low
54, 71	Investigate establishing an 'Adopt-a-Trail' Program to engage community involvement in the development and maintenance of current and future trails.	General Policies: Alternative Development Standards; Recreation and Open Space	Low
59	Prepare a comprehensive development strategy for the Lac Du Bois grasslands north of Ord Road.	General Policies: Environmentally Sensitive Areas/Grasslands	Low
83	Examine the feasibility of installing a traffic circle at the Yew Street and Tranquille Road intersection.	Land Use Policies: Tranquille Commercial District	Low
88	Examine the feasibility of reducing traffic volumes throughout the North Shore Towne Centre to encourage a safer pedestrian environment.	Land Use Policies: North Shore Towne Centre	Low
97	Pursue the completion of an 'On the Ground' marking system for the Rivers Trail, to clearly identify pedestrian and cycle routes, to and from and within, the McDonald Park neighbourhood.	Land Use Policies: McDonald Park Neighbourhood	Low
105	Work with School District #73 to coordinate with the development and maintenance of neighbourhood park space.	Land Use Policies: North Kamloops East	Low
125	Conduct a transportation study to determine if there are any safety gaps for cyclists, pedestrians and farm machinery along the main access road through the Agricultural Lands district.	Land Use Policies: Agricultural Lands	Low

## "C" - Capital Projects and Action Items

# Appendix "D" - North Shore Zoning Code

## "D" - North Shore Zoning Code

The North Shore Zoning Code has been established to directly represent the policy statements and directions provided within the North Shore Neighbourhood Plan. Reference to specific sections of the North Shore Zoning Code are made throughout the North Shore Neighbourhood Plan and are meant to be used in conjunction with the new Development Guidelines also established for the North Shore (see Appendix "A").

Although included as an Appendix of the North Shore Neighbourhood Plan, the North Shore Zoning Code is available as a separate document from this Plan. Copies can be obtained at either the Legislative Services Department or the Development and Engineering Services Department. The North Shore Zoning Code is also available on the City of Kamloops website ([www.kamloops.ca](http://www.kamloops.ca)).

## Appendix "E" - CPTED Principles



# "E" - CPTED Principles

All new developments on the North Shore should implement the principles and components of CPTED as listed below. *What to Look For* hints are provided to aid in the implementation of CPTED principles.

## a. Principles

**Territoriality and Defensible Space** - Design the physical environment to extend a perceived sphere of influence or territory. By creating a sense of ownership, people become more connected with their surroundings and this is interpreted as a warning by potential offenders. Fencing, paving, signage and landscaping are often used to show ownership of space.

**Hierarchy of Space** - Design the built environment to show clear transitions between public, semi-public, semi-private and private space. In this sense, territory, as well as ownership is also defined thereby reducing the risk of undesirable uses.

**Natural Surveillance** - Design to maximum visibility and ensure legitimate users can observe the space around them, reducing their fear of crime. This strategy increases the likelihood that intruders will be seen and apprehended or discouraged from loitering. The siting of developments and buildings in relation to each other and the street, lighting, landscaping and window placement are key factors in providing appropriate visibility and sightlines.

**Access Control** - Design entry and exit points to naturally maximize control, surveillance and ultimately ownership, by desirable users. Access to a crime target is denied, creating a perception of risk to potential offenders. This can be achieved by real and perceived barriers.

**Image and Maintenance** - Design by enhancing and maintaining physical appearance to encourage users of the area to respect their surroundings. The more dilapidated an area, the more vulnerable it is to further abuse (e.g. litter, graffiti).



Source: Unknown Internet Site

# "E" - CPTED Principles

## b. Components

**Movement Predictors** - Design features that create predictable or unchangeable routes or paths that offer no choice to pedestrians (e.g. pedestrian tunnels, narrow passageways, pedestrian bridges, escalators).

### **What to Look For in Design:**

1. Elimination of movement predictors - can a movement predictor be eliminated rather than attempting costly improvements?
2. Sightlines - can a pedestrian see what is in a tunnel and what is at the end of it?
3. Lighting - is lighting adequate and consistent, avoiding pools of shadow? Is natural light possible? Do wall and ceiling materials help to reflect light?
4. Hardware - can emergency telephones, intercoms or video cameras be added? Who is viewing them? Is the means to summon help well signed?
5. Alternative night route - is there an alternative well-lit and frequently travelled route that can be indicated at the entrance?
6. Juxtaposition of movement predictor and entrapment spot - is there an entrapment spot or isolated area within 50-100 m of the end of the movement predictor? If so, can it be modified?

**Entrapment Areas** - Small, confined areas, adjacent to or near a well-travelled route, that are shielded on three sides by some barrier (e.g. elevator, storeroom, fire stairs, far recessed entrances, gaps in tall shrubs).

### **What to Look For in Design:**

1. Is there an entrapment spot(s) adjacent to a main pedestrian route (e.g. storage area, hidden area below or above grade, private dead-end alley)? Can it be eliminated?
2. Can the area be closed off or locked in off-hours? Can, for example, the stairwell to a locked building be locked as well?
3. Are there deadlocks for storage areas off pedestrian routes? Is there limited access to loading docks or other restricted areas?
4. If an entrapment area is unavoidable, is it well lit and are there aids to visibility such as convex mirrors?

**Activity Generators** - Include everything from increasing recreational facilities in a park to placing housing in a previously commercial area; places that attract both activities and people.

### **What to Look For in Design:**

1. Complimentary Uses - are there complimentary uses, especially in potentially isolated areas?
2. Complimentary Users - are users complementary? Avoid forcing users to pass through an area controlled by people considered to be threatening.
3. Reinforcing activity generators - are activity generators located along an active edge or along one or two pedestrian paths in large parks or on the boundary of large developments? An active edge creates a boundary of space that is inviting rather than threatening to passers-by.
4. Design for programming activity mix - do planning and design provide opportunities for enhanced programming, such as cultural and recreational programs in parks to encourage people to use and own the public space?
5. Grade-level activity - are uses at grade in high and medium density districts?



Dark passageway and low visibility to the other side;  
Source: Unknown Internet Site

# "E" - CPTED Principles

## b. Components (cont.)

**Lighting** - Design features that provide sufficient and appropriate lighting day and night is essential to preventing crime and creating safe environments.

### **What to Look For in Design:**

1. Minimum Standards - does the lighting allow for visibility if a night time use is desired? Are pedestrian pathways, laneways and access routes in outdoor public spaces lit to the minimum recommended levels?
2. Consistency of lighting - is lighting consistent, in order to reduce contrast between shadows and illuminated areas?
3. Proper placement of lighting - does street lighting shine on pedestrian pathways and possible entrapment spaces rather than on the road or in people's windows? Does lighting take into account vegetation, including mature trees and other potential blocks?
4. Improper lighting - can paths or spaces not intended for night time use remain unlit to avoid giving a false impression of use?
5. Protection of lighting - are the light fixtures protected from casual vandalism by means such as wired glass or a lantern-style holder?
6. Maintenance - are lighting fixtures maintained in a clean condition and promptly replaced if burnt out or broken?
7. Planning for night time use - do development drawings or any other material produced by the developer take into account how spaces will be used at night?



Stairwell visible from the outside and to the outside;  
Source: Unknown Internet Site

**Sightlines** - Design features that create an inability to see what is ahead and that create a serious impediment to feeling and being safe (e.g. sharp corners, walls, earth berms, fences, bushes or pillars).

### **What to Look For in Design:**

1. Sharp corners - are sharp, blind corners avoided, especially on stairs or in corridors where movement can be predicted? Can sudden changes of grade on pathways that reduce sightlines be avoided or ameliorated?
2. Permeable barriers - are barriers along paths visually permeable when possible?
3. Especially problematic places - is special care for visibility taken into account in spaces where risk to personal safety is perceived to be high, such as stairwells or parking garages, lobby entrances to buildings, laundry rooms?
4. Improving sightlines - in spaces or paths where sightlines are impeded, can hardware be added to make it easier to see, such as security mirrors?
5. Future sightline impediments - are landscaping features which could, in their maturity, serve as screens or barriers to an unimpeded view along pathways be avoided?

**Isolation (Ear and Eye)** - Designs that provide the feeling that there is natural surveillance occurring thereby creating the feeling of safety.

### **What to Look For in Design:**

1. Informal surveillance of routes - do building windows overlook pedestrian routes? Are blank facades avoided at street level?
2. Especially problematic routes - are routes to and from parking lots or garages overlooked?
3. Hardware to ameliorate isolation - is there a telephone, emergency telephone or alarm and is it adequately signed?

# "E" - CPTED Principles

## **b. Components (cont.)**

**Land Use Mix** - Design and land use mixes need to create public streets that are used throughout the day and that are not intended for just one type of use. Land use mixes cannot be considered without taking scale into account and uses must be compatible with one another and with what the community needs.

### **What to Look For in Design:**

1. Compatible mixed uses - are there compatible mixed uses that encourage activity, informal surveillance and contact among people during the day and evening? Is the first purpose in mixing uses to provide adequate and appropriate services to the primary users of a development?
2. Scale - is the scale of development consistent with its neighbours?
3. Balancing "negative" land uses - How might nearby land uses considered negative be balanced by positive measures? Can other land uses be added or modified to help address the discomfort?

**Human Scale** - Design features that clearly orient to human scale activities and that create spaces that are cared for and are monitored frequently.

### **What to Look For in Design:**

1. Pedestrian-oriented open spaces such as courtyards, patios or other unified landscaped areas; porches or covered entryways, pedestrian weather protection in the form of canopies, awnings, arcades or other features.
2. Small design features that allow for residents and building users to show ownership and a human presence such as flower pots, hooks to hang outdoor decorative items and covered stoops big enough to accommodate at least one chair.



Low visibility fence separates public and private space;  
Source: Henrico County, USA.



Flower pots create semi-public space in this outdoor area;  
Source: The Planning Center, *Center Views Newsletter*

# "E" - CPTED Principles



# Appendix "F" - North Shore Procedures Guide

# "F" - North Shore Procedures Guide

## NORTH SHORE PROCEDURES GUIDE

The North Shore Neighbourhood Plan is a land use plan with policies that incorporate elements of urban design, sustainable community development practices and ecologically-responsible construction methods. The combination of policies and implementation methods in the North Shore Neighbourhood Plan have made it the new benchmark for high quality, neighbourhood-appropriate, self-sustaining development for the City with the use of Development Permit Guidelines, a Development Checklist and a Development Incentives Matrix.

This Procedures Guide is intended to provide additional information on how to engage in development on the North Shore in a manner that is consistent with the vision, goals and principles of the Plan and of the neighbourhood and ultimately, to create a healthy, viable and resilient community, one development at a time.

The following steps should be followed when developing on the North Shore:

### STEP ONE

Review North Shore Neighbourhood Plan Policies

### STEP TWO

Review North Shore Zoning Code Regulations

### STEP THREE

Review North Shore Development Permit Guidelines  
Prepare Development Permit Application

### STEP FOUR

Complete Development Checklist

### STEP FIVE

Submit all Development Applications, including Development Permits, Building Permits,  
and any other necessary Development Applications

### STEP SIX

Determine Incentive Level  
Submit all Documentation Supporting Incentive Level Reached

### STEP SEVEN

Obtain Approvals  
Begin Project

### STEP EIGHT

Complete Project  
Obtain Incentives